

PLANNING COMMITTEE

Wednesday, 20 June 2018

5.30 pm

Committee Rooms 1-2, City Hall

Membership:	Councillors Jim Hanrahan (Chair), Naomi Tweddle (Vice-Chair), Biff Bean, Bill Bilton, Alan Briggs, Kathleen Brothwell, Chris Burke, Bob Bushell, Gary Hewson, Ronald Hills and Edmund Strengiel
Substitute members:	Councillors Jackie Kirk and Neil Murray
Officers attending:	Emma Brownless, Simon Cousins, Democratic Services, Kieron Manning and Paul Thompson

The Planning Committee comprises democratically elected members who will be presented with a recommendation from the professional officers for each application on the agenda. After each application has been presented, those interested parties who have registered to speak will then be given 5 minutes to verbally present their views, and, following this, the committee will debate each proposal and make the decision, having considered all relevant information.

Clearly the process of making a decision will inevitably cause some people to feel aggrieved, but it is hoped that all interested parties will feel that their views have been considered as part of the process.

Please ensure that your mobile phones are switched off or set to silent throughout the meeting and please refrain from attempting to speak from the public gallery unless you have formally registered to speak on an application, in which case the Chair will call you to the table at the relevant time.

A G E N D A

SECTION A	Page(s)
1. Confirmation of Minutes - 23 May 2018	5 - 28
2. Declarations of Interest	
Please note that, in accordance with the Members' Code of Conduct, when declaring interests members must disclose the existence and nature of the interest, and whether it is a disclosable pecuniary interest (DPI) or personal and/or pecuniary.	
3. Applications for Development	
(a) Land Adjacent to A46 Ring Road and North of Queen Elizabeth	29 - 100

Road , Lincoln,

(b)	Land at Westbrooke Road, Lincoln.(Phase 4)	101 - 178
(c)	Land at 94 and 96 Newland (Taste Of Marrakesh) and No. 100, 102 and 104 Newland, Lincoln	179 - 218
(d)	Grantham Street Car Park, Grantham Street, Lincoln	219 - 258
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THE LOCAL GOVERNMENT (ACCESS TO INFORMATION) ACT 1985 (AS AMENDED)

LIST OF BACKGROUND PAPERS FOR PLANNING, LISTED BUILDING, CONSERVATION AREA AND ADVERTISEMENT APPLICATIONS ON THE AGENDA OF THE PLANNING COMMITTEE

The Background Papers for the Planning, Listed Building, Conservation Area and Advertisement Applications are:

1. The Planning Application File. This is a file with the same reference number as that shown on the Agenda for the Application. Information from the planning application file is available online at <https://development.lincoln.gov.uk/online-applications/>

The application files contain the following documents:

- a. the application forms;
 - b. plans of the proposed development;
 - c. site plans;
 - d. certificate relating to ownership of the site;
 - e. consultation letters and replies to and from statutory consultees and bodies;
 - f. letters and documents from interested parties;
 - g. memoranda of consultation and replies to and from Departments of the Council.
2. Any previous Planning Applications referred to in the Reports on the Agenda for the particular application or in the Planning Application specified above.
 3. Central Lincolnshire Local Plan – Adopted April 2017
 4. National Planning Policy Framework - March 2012
 5. Applications which have Background Papers additional to those specified in 1 to 5 above set out in the following table. These documents may be inspected at the Planning Reception, City Hall, Beaumont Fee, Lincoln.

APPLICATIONS WITH ADDITIONAL BACKGROUND PAPERS (See 5 above.)

Application No.: Additional Background Papers

CRITERIA FOR PLANNING COMMITTEE SITE VISITS (AGREED BY DC COMMITTEE ON 21 JUNE 2006 AND APPROVED BY FULL COUNCIL ON 15 AUGUST 2006)

Criteria:

- Applications which raise issues which are likely to require detailed first hand knowledge of the site and its surroundings to enable a well-informed decision to be taken **and** the presentational material at Committee would not provide the necessary detail or level of information.
- Major proposals which are contrary to Local Plan policies and proposals but which have significant potential benefit such as job creation or retention, environmental enhancement, removal of non-confirming uses, etc.
- Proposals which could significantly affect the city centre or a neighbourhood by reason of economic or environmental impact.
- Proposals which would significantly affect the volume or characteristics of road traffic in the area of a site.
- Significant proposals outside the urban area.
- Proposals which relate to new or novel forms of development.
- Developments which have been undertaken and which, if refused permission, would normally require enforcement action to remedy the breach of planning control.
- Development which could create significant hazards or pollution.

So that the targets for determining planning applications are not adversely affected by the carrying out of site visits by the Committee, the request for a site visit needs to be made as early as possible and site visits should be restricted to those matters where it appears essential.

A proforma is available for all Members. This will need to be completed to request a site visit and will require details of the application reference and the reason for the request for the site visit. It is intended that Members would use the proforma well in advance of the consideration of a planning application at Committee. It should also be used to request further or additional information to be presented to Committee to assist in considering the application.

Present: Councillor Jim Hanrahan (*in the Chair*),
Councillor Naomi Twedde, Councillor Biff Bean,
Councillor Bill Bilton, Councillor Alan Briggs, Councillor
Kathleen Brothwell, Councillor Chris Burke, Councillor
Bob Bushell, Councillor Gary Hewson and Councillor
Ronald Hills and Councillor Hilton Spratt

Apologies for Absence: Councillor Edmund Strengiel

1. Confirmation of Minutes - 22 March 2018

RESOLVED that the minutes of the meeting held on 22 March 2018 be confirmed.

2. Declarations of Interest

The Solicitor to Planning Committee advised that one of the objectors to item number 5(c) Garage, Rosebery Avenue may be known to labour members through links with their political party. She gave legal advice stating that for members to meet him at party events did not necessarily mean that person was a close associate. If a member considered his/her relationship with the person concerned to be more than an acquaintance this would be sufficient grounds for him/her to declare a personal and pecuniary interest in the matter to be discussed.

Councillor Biff Bean declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'.
Reason: He knew one of the objectors, but not as a close associate.

Councillor Bill Bilton declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'.
Reason: He knew one of the objectors, but not as a close associate.

Councillor Bob Bushell declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'.
Reason: He knew one of the objectors, but not as a close associate.

Councillor Chris Burke declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'.
Reason: He knew one of the objectors, but not as a close associate.

Councillor Gary Hewson declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'.
Reason: He knew one of the objectors, but not as a close associate.

Councillor Jim Hanrahan declared a Personal and Pecuniary Interest with regard to the agenda item titled 'Application for Development: Land At Westbrooke Road, Lincoln'.
Reason: His son owned a property on Westbrooke Road and rented garage space to the developer as the show home office.

He left the room during the discussion of this item and took no part in the vote on the matter to be determined.

Councillor Jim Hanrahan declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'. Reason: He knew one of the objectors, but not as a close associate.

Councillor Kathleen Brothwell declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'. Reason: She knew one of the objectors, but not as a close associate.

Councillor Naomi Tweddle declared a Personal and Pecuniary Interest with regard to the agenda item titled 'Application for Development: Land At Westbrooke Road, Lincoln'.

Reason: Her close friend owned a house on the LN6 development.

She left the room during the discussion of this item and took no part in the vote on the matter to be determined.

Councillor Naomi Tweddle declared a Personal Interest with regard to the agenda item titled 'Application for Development: Garage, Rosebery Avenue, Lincoln'. Reason: She knew one of the objectors, but not as a close associate.

3. Work to Trees in City Council Ownership

The Arboricultural Officer:

- a. advised members of the reasons for proposed works to tree's in City Council ownership and sought consent to progress the works identified at Appendix A of his report
- b. explained that Ward Councillors had been notified of the proposed works
- c. stated that in some cases it was not possible to plant a tree in the exact location and in these cases a replacement would be replanted in the vicinity.

Members queried why the felling of a monkey puzzle tree in the front garden of 97 Holly Street was not to be replaced with a 'like for like' species.

The Arboricultural Officer advised that a monkey puzzle tree was not an appropriate replacement species for the area in terms of the local street scene/amenity value, therefore a Maple tree would be replanted in a suitable location.

RESOLVED that the works set out in the schedule at Appendix A attached to the report be approved.

4. Confirmation of Tree Preservation Order 156

The Planning Team Leader:

- a. advised members of the reasons why a tree preservation order should be confirmed at the following site:

- Tree Preservation Order 156: Group of trees comprising 49 mixed species located adjacent to the car park at the Lincolnshire Poacher, Bunkers Hill, Lincoln
- b. provided details of the individual trees to be covered by the order and the contribution they made to the area
 - c. advised that following the statutory 28 day consultation period, one supporting letter had been received to the making of the order from the occupant of 20 Sympson Close, requesting some changes to the trees included in the tree preservation order in relation to his property; following consideration by the City Arboriculturist the plan had been revised to incorporate some of the suggestions made in that letter amending the number of trees included from 49-47
 - d. added that 3 additional letters of support had been received from residents of Sympson Close, and no objections
 - e. reported that confirmation of the tree preservation order here would ensure that the trees could not be removed or worked on without the express permission of the council.

RESOLVED that tree preservation order no 156 be confirmed with modifications as shown on the revised plan to include a group of 47 mixed species trees and that delegated authority be granted to the Planning Manager to carry out the requisite procedures for confirmation.

5. Change to Order of Business

RESOLVED that the order of business be amended to allow the report on the Garage, Rosebery Avenue, Lincoln to be considered before the remaining agenda items and the report on 1 St Paul's Lane to be considered after the item on 98 Newland (Viking House) and 100, 102 and 104 Newland Lincoln.

6. Application for Development: Garage, Rosebery Avenue, Lincoln

The Planning Team Leader:

- a. advised that planning permission was sought for conversion of a single storey garage to a 3 bedroom residential dwelling within Use Class 3
- b. described the location of the application property located to the west side of Rosebery Avenue
- c. stated that the property was located within the West Parade and Brayford No. 6 Conservation Area
- d. provided details of the policies pertaining to the application as follows:
 - National Planning Policy Framework
 - Central Lincolnshire Local Plan, adopted April 2017
 - Policy LP1 A Presumption in Favour of Sustainable Development
 - Policy LP21 Biodiversity and Geodiversity
 - Policy LP26 Design and Amenity

- e. outlined the responses made to the consultation exercise
- f. advised members of the main issues to be considered as part of the application as follows:
 - Accordance with national and local planning policy
 - Impact on residential amenity
 - Impact on visual amenity
 - Highway safety, access and parking
 - Communal space, bin storage and other factors
 - Ecology and the protection of habitats and species
 - Other matters
- g. concluded that:
 - The proposed conversion to a residential dwelling would not have a harmful impact on the amenities of neighbouring properties and would enhance the character and appearance of the conservation area.
 - The application facilitated the conversion of a an existing building into a more sustainable use through the addition of a new dwelling, in accordance with policies LP1 A, LP21 & LP26 of the Central Lincolnshire Local Plan and the National Planning Policy Framework.

Mr Richard Mair, local resident, addressed Planning Committee in opposition to the proposed development, covering the following main points:

- He resided at 290 West Parade to the south of the proposed development.
- He represented all 21 objectors to the proposals, living both north and south of the garage.
- The developer was trying to squeeze a 3 bedroomed house into an area which was already full to capacity.
- The Planning Officer seemed to be taking the view that one additional house did not matter, however, it was even more important in a closely knit area and the proposals were not welcomed.
- The developers came from the London area where tighter relationships between properties would be more acceptable.
- Following concerns raised, the window to one of the bedrooms had been removed from the plans, leaving a double bedroom with no window at all. The three storey adjacent properties would look down on the garage and roof lights of its bedroom windows and vice-versa they would also be able to see into our property.
- The roof lights to the north side of the roof would be even closer to the occupant of 1 Rosebery Avenue.
- Local residents would look down on sixty feet of shiny black roof slates. Planning officers should have insisted on the use of traditional planning materials.
- The applicant had no right to use the passageway currently located to the side of the proposed development. Residents had confirmed this with the Land Registry. The passageway was for emergencies only such as in the event of fire. Allowing the applicant to use the passageway for access/egress would result in loss of security and privacy to existing residents.

- Officers stated that the matter of the fire escape was not a material planning consideration, however, would Planning Committee be happy to grant permission for a development which would propose a fire risk?
- This proposal was overdevelopment in a small area.
- The development would damage the amenity of existing residents living there.
- Due to scale/density and layout, the planning application should be refused.

Councillor Neil Murray addressed Planning Committee as Ward Advocate in respect of the proposed development, covering the following main points:

- He was surprised to see officers quoting National Planning Policy in support of the planning application.
- This house would be 'crammed in' with some rooms even without windows.
- The proposed development would be detrimental to amenity being so close to existing residents.
- Lincoln Civic Trust had stated the proposals represented overdevelopment – this type of build was not sustainable in the West End of Lincoln.
- The proposed dwelling was of a curious design with 3 bedrooms and a tiny living area, one bedroom without a window.
- The applicant was trying to maximise monetary value with no respect to the area/local residents.
- This was a tightly built up area with small gardens.
- This was a negative development which would damage the amenity of local residents.
- The passageway did not benefit from shared access arrangements. If the applicant was not being honest about the details of ownership on the plan this was misleading.
- There was no emergency exit. The existing residents owned the land as listed in their deeds.
- He would like to see proof from the applicant of shared ownership of the passageway.
- This was an overdevelopment causing loss of residential amenity.

Miss Elly Krisson, Applicant, addressed Planning Committee in support of the proposed development, covering the following main points:

- She thanked members of Planning Committee for allowing her the opportunity to speak.
- She was no stranger to Lincoln. Her husband's parents had lived here for 40 years and they also wished to settle here.
- Her husband's father had owned the garage property for over 20 years.
- She understood some people did not like change, however, the plans involved very little change in appearance and there was no alterations to the height/footprint of the building.
- They had received an offer to buy, however, she wanted to live there with her husband
- The architect had suggested a larger footprint than the design they had opted for.
- The development would be an improvement to the area.
- High level/unobtrusive opaque windows had been removed from the original plans following neighbour concerns.

- They loved the area, it was a lovely spot, peaceful and calm.
- They hoped planning permission would be granted similar to other dilapidated garages in the area having been turned into homes.
- Her solicitor had advised that the passageway was unregistered land and was not owned by the residents. The right of way had been established from a door in the garage.
- They didn't want a right of way to the passage, just to use it as an emergency exit in case of fire.
- They hoped they could all live together amicably and happily with local residents in this beautiful spot.

Members discussed the content of the report in further detail, raising the following main points:

- Members of Planning Committee should not get involved in the detail of land ownership, it was not their remit. It was assumed that the building regulations officer was satisfied with regard to fire regulations.
- The application should not be passed until the fire risk issue had been resolved.
- Concern was raised as to whether the S106 agreement was time limited to prevent occupation by students and if not whether any request for a variance would need to come back to committee for approval.
- It was the owner's choice as to whether or not they wished to live in a home without a bedroom window.
- Any grant of planning permission should require a bat box to be installed, together with the S106 agreement to prevent occupation by students and suitable materials used.

The Planning Team Leader offered the following points of clarification to members:

- A condition regarding samples of materials was already included within the officer recommendation.
- The third bedroom had two roof lights similar to those used in an attic room.
- Planning Committee was here to consider material planning considerations. The interior of the property had not been viewed by building regulations as yet, officers in this area dealt with fire risk issues. Should the applicant be unable to utilise the fire exit the interior of the building would need to be redesigned and a new planning application submitted.
- Should members be so minded, the inclusion of a bat box within the conditions for grant of the planning application would be appropriate.

RESOLVED that authority to grant planning permission be delegated to the Planning Manager subject to:

- The receipt of a bat survey and the introduction of any necessary mitigation measures
- The signing of a section 106 agreement to ensure no student occupation of the property
- The conditions listed below.

Standard Conditions

- 01) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990.

- 02) With the exception of the detailed matters referred to by the conditions of this consent, the development hereby approved shall be carried out in accordance with the drawings listed within Table A below.
The works shall be carried out in accordance with the details shown on the approved plans and in any other approved documents forming part of the application.

Reason: To ensure the development proceeds in accordance with the approved plans.

Conditions to be discharged before commencement of works

- 03) Samples of all external materials to be used in the development shall be submitted to and approved by the Local Planning Authority before the development commences. The approved materials shall not be substituted without the written consent of the City Council as Local Planning Authority.

Reason: In the interests of visual amenity.

Conditions to be discharged before use is implemented

None.

Conditions to be adhered to at all times

- 04) The construction of the development hereby permitted shall only be undertaken between the hours of 08:00 to 18:00 Monday to Friday (inclusive) and 08:00 to 13:00 on Saturdays and shall not be permitted at any other time, except in relation to internal plastering, decorating, floor covering, fitting of plumbing and electrics and the installation of kitchens and bathrooms; and

Any deliveries associated with the construction of the development hereby permitted shall only be received or despatched at the site between the hours of 08:00 to 18:00 Monday to Friday (inclusive) and 08:00 to 13:00 on Saturdays and shall not be permitted at any other time.

Reason. In the interests of the amenity of neighbouring properties.

- 05) In the event that contamination is found at any time when carrying out the approved development it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval, in writing, of the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the

approval of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

- 06) The dwelling hereby granted shall be used as a residential dwelling (Use Class C3) and for no other purpose within the Schedule of the Town and Country Planning (Use Classes) Order 2015 or any subsequent amendment or re-enactment thereof).

Reason: In order to protect amenity.

- 07) Notwithstanding the provisions of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 (or any subsequent re-enactment or revocation thereof) the dwelling hereby approved shall not be enlarged, improved or otherwise altered without the prior consent of the City Council as Local Planning Authority.

Reason: In the interests of the privacy and amenity of neighbouring residents.

Table A

The above recommendation has been made in accordance with the submitted drawings identified below:

Drawing No.	Version	Drawing Type	Date Received
ST-267/03	B	Floor plans	15th April 2018
ST-267/04	B	Elevations - Proposed	15th April 2018

7. Application for Development: Land At Westbrooke Road, Lincoln

(Councillors Hanrahan and Tweddle left the room during the discussion of this item, having declared a personal and pecuniary interest in the matter to be discussed.)

It was proposed, seconded, put to the vote and:

RESOLVED that Councillor Bushell be appointed as chair for this agenda item.

Councillor Bushell took the chair.

The Planning Team Leader:

- a. advised that full planning permission was sought by Chestnut Homes for the erection of 29 dwellings for Phase 3 of the Westbrooke Road development known as LN6, with vehicular access through the existing access created for Phases 1 and 2 off the western end of Westbrooke Road
- b. described the location of the site at the rear of properties on St Helen's

Avenue between the former school site to the south and Phase 2 to the North, presently nearing completion, with Tritton Road beyond the western boundary

- c. advised that the site was currently owned by the City of Lincoln Council with certificate B having been served by the applicant
- d. reported that negotiations ongoing throughout the course of the application had resulted in layout revisions to the plans having been submitted, followed by further consultation with those neighbours immediately adjacent to the proposals
- e. described the relevant planning history to the application site as detailed within the officer's report
- f. provided details of the policies pertaining to the application as follows:
 - National Planning Policy Framework
 - Policy LP1: A Presumption in Favour of Sustainable Development
 - Policy LP9: Health and Well Being
 - Policy LP11: Affordable Housing
 - Policy LP12: Infrastructure to Support Growth
 - Policy LP14: Managing Water Resources and Flood Risk
 - Policy L26: Design and Amenity
- g. outlined the responses made to the consultation exercise
- h. referred to the update sheet which contained revised site layout plans, revised ground level plans, a revised boundary treatment plan, together with comments received from the Highways Authority regarding the submitted Transport Statement, further comments received in respect of the application from Lincolnshire County Council as the Flood Risk Authority requesting additional conditions on any grant of planning permission for the development, and comments also from Historic England
- i. advised members of the main issues to be considered as part of the application as follows:
 - Local and National Planning Policy
 - Effect on Residential Amenity
 - Effect on Visual Amenity
 - Highway Safety
 - Flood Risk
 - Ecology
 - Affordable Housing
 - Land Levels
 - Land Contamination
 - CIL and other Contributions
 -
- j. concluded that:
 - The site had an allocation for housing in the CLLP and was located in a sustainable location close to existing services and amenities, with good transport links.

- Negotiations had secured revisions to the proposals including the removal of the proposed footpath link to Skellingthorpe Road (phase 4) and revisions to the layout and position of proposed dwellings, in response to concerns regarding residential amenity from the occupiers of existing properties.
- The development would contribute to the housing supply within the city and provide affordable housing in accordance with national and local planning policy.

Rebecca Archer, representing Chestnut Homes, addressed Planning Committee in support of the proposed development, covering the following main points:

- She was employed by Chestnut Homes as Land Development Manager.
- Phase 3 was a continuation of the LN6 development containing 80 dwellings so far.
- Phase 3 proposed an additional 29 dwellings.
- The development currently offered 16 affordable dwellings and 60 market houses with only 1 unit remaining left to sell.
- This land was designated for residential development in the local Plan for this area.
- The design of Phase 3 was similar to that of Phases 1 and 2.
- The tree belt along Tritton Road had been maintained with a footpath link through the development to Tritton Road.
- Separation between adjacent dwellings had been maximised.
- Land levels had been reviewed and lowered to reduce the effect on residential amenity.
- A foul drainage pumping station would be provided.
- French drains would be continued around the boundary.
- New boundary treatment was planned to prevent overlook.
- Further construction would commence in 1 week if planning permission was granted.

Members discussed the content of the report in further detail, raising the following main points:

- Members raised concern that continual expansion of the development would reach a point where traffic considerations would become a big problem. Further development must consider traffic congestion on the Western Avenue/Boultham Park Road junction.
- This quality development had won an award from the City of Lincoln Council and should be highly commended.
- Would the payment for Community Infrastructure Levy (CIL) and S106 have been different if a single planning application had been submitted rather than in phases?
- Would there be a designated drive through for lorries to transport soil to infill the development?
- Would wildlife/deer in the area be disturbed?
- What would be the impact on residents from the operation of the pumping station?
- Would the developer be happy to install stop signs/road markings to slow motorists down at the junction with Westbrooke Road/Western Crescent/Western Avenue?

The Planning Team Leader offered the following points of clarification to members in response to queries raised:

- The applicant was willing to erect traffic signage at the traffic junction with Westbrooke Road/Western Crescent/Western Avenue, however, the Highways Authority as responsible body for making the final decision had previously said this was not necessary. Officers would ask again.
- In addition to the Highways Authority report, the Highways Authority had visited the site several times and raised no objections.
- The requirement for payment of CIL didn't come into force until this year, otherwise, each phase of the development paid S106 monies separately so there would have been no difference in contributions made had a single planning application been submitted.
- An ecological assessment had established that there was no evidence of deer on the site, it may be they were coming from the other side of Tritton Road.
- Tests carried out by Environmental Health officers indicated that the pumping station would not cause issues of noise/smells.
- The County Council as Lead Flood Authority had stated that the surface water drainage system proposed by the applicant was considered to be suitable.
- With regards to infill transport, not a great deal of soil would be brought in as excavation work for drainage would create sufficient soil to move around the existing site.

RESOLVED that planning permission be granted conditionally subject to the signing of a unilateral agreement to cover the provision of affordable housing and a contribution towards Local Green Infrastructure (Playing Field, Play Space and Amenity Space) and subject to the following conditions:

Standard Conditions

1. 3 years
2. Drawing numbers
3. Materials
4. Landscaping scheme
5. In accordance with FRA and finished floor levels
6. Tree protection measures during construction.
7. Any removal of scrub, hedgerows or trees between March to late August to be supervised by an ecologist and mitigation measures applied if required.
8. Sensitive lighting plan.
9. Installation of bat boxes.
10. Noise impact assessment for the pumping station.
11. Archaeology- development to proceed in accordance with the submitted WSI. Fieldwork report to be submitted within 6 months of completion
12. Electric charging points to be installed in accordance with drawing no WLR3 01 Rev E
13. Land contamination-1) Implementation of approved remediation scheme, 2) Reporting of unexpected contamination
14. Removal of pd for plots adjacent to existing residential development
15. Fencing to plots 54,55,56 not to be altered without the prior consent of the Council
16. Standard highway conditions

(Councillor Hills requested that his vote against this planning application be recorded.)

8. Application for Development: Land Including 98 Newland (Viking House) And 100, 102 And 104 Newland, Lincoln.

(Councillors Hanrahan and Tweddle returned to the room for the remainder of the meeting. Councillor Hanrahan re took his seat as Chair.)

The Principal Planning Officer:

- a. advised that full planning permission was sought for change of use of Viking House to student accommodation (Sui Generis) alongside external alterations; erection of partial subterranean building to provide four storeys of student accommodation (Sui Generis) between No. 96 and No. 100 Newland, including glazed link to No. 100 Newland; change of use of No. 100, 102 and 104 Newland to student accommodation (Sui Generis); and associated hard and soft landscaping works including creation of an internal courtyard
- b. described the location of the site occupied by two buildings, No. 98 Newland (Viking House) and the terrace at Nos. 100-104 Newland as detailed within the officers report
- c. advised that access to the buildings was currently a mixture of pedestrian access from Newland with vehicular access from Carholme Road and the traffic lit junction at the foot of the Brayford Way flyover where it met Carholme Road, the ground floor of Viking House currently served as a car park, accessed through the OTB car park and the rear courtyard of 100-104 was also car parking.
- d. described the relevant planning history to the application site as detailed within the officer's report
- e. provided details of the policies pertaining to the application as follows:
 - Policy LP1: A Presumption in Favour of Sustainable Development
 - Policy LP2: The Spatial Strategy and Settlement Hierarchy
 - Policy LP3: Level and Distribution of Growth
 - Policy LP5: Delivering Prosperity and Jobs
 - Policy LP13: Accessibility and Transport
 - Policy LP14: Managing Water Resources and Flood Risk
 - Policy LP16: Development on Land affected by Contamination
 - Policy LP17: Landscape, Townscape and Views
 - Policy LP25: The Historic Environment
 - Policy LP26: Design and Amenity
 - Policy LP29: Protecting Lincoln's Setting and Character
 - Policy LP31: Lincoln's Economy
 - Policy LP33: Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area
 - Policy LP36: Access and Movement within the Lincoln Area
 - Policy LP37: Sub-Division and Multi-Occupation of Dwellings within Lincoln
 - National Planning Policy Framework

- f. outlined the responses made to the consultation exercise
- g. referred to the update sheet which contained further comments received in relation to the application from Lincoln Civic Trust together with an officer response to these comments, and further neighbour correspondence
- h. advised members of the main issues to be considered as part of the application as follows:
 - The Principle of the Development;
 - The Impact of the Design of the Proposals;
 - The Implications of the Proposals upon Amenity;
 - Sustainable Access, Highway Safety and Air Quality
 - Other Matters; and
 - The Planning Balance.
- i. concluded that:
 - The presumption in favour of sustainable development required by the National Planning Policy Framework would apply to the proposals as there would not be conflict with the Framework in respect of sustainability that would apply to development, as set out in the planning balance.
 - It was the conclusion of officers and therefore the recommendation to Members that there would not be harm caused by approving the development so the application should benefit from planning permission for the reasons identified in the report and subject to the conditions outlined below.
 - However, if any new material planning considerations were raised within correspondence received following the writing of this report which would lead to a different conclusion being reached or which would require further consideration and/or planning conditions, officers would provide members with a detailed response on the Update Sheet.
 - This would have regard to any further consultation responses received in the timeframe from the agenda being published and the date of the Planning Committee, or these would be reported directly at the Planning Committee if appropriate.

Chris Spendlove representing the University of Lincoln, addressed Planning Committee in support of the proposed development, covering the following main points:

- He worked as Registrar at the University of Lincoln.
- This development proposed 173 purpose built student bed spaces to satisfy the demand in current and future years.
- The University had received several awards in recognition of its high standards of education.
- The University was currently collaborating with the University of Nottingham to establish a medical school.
- The University had been extremely successful in the recruitment of students both nationally and internationally, numbers being significantly higher than last year.

- There was a demand for an additional 2,800 bed spaces over future years.
- The University must be able to recruit 'to target' to be effective and sustainable.
- There was a deficit of 500 bed spaces forecast for the year 2018/19.
- A strategy had been arranged with developers to build purpose built student accommodation over the next few years.
- The University was in support of the City Council's Article 4 Direction policy.
- Viking House was ideally situated for students being 5 minutes from the City and University.
- There would be no internal link from Viking House to No 1 Brayford or external thoroughfare.
- These proposals were the 1st phase of a wider regeneration scheme.
- CCTV screens would monitor the unit from the main University campus.
- The proposal would 'knit back' the street scene along Newland.

Members discussed the content of the report in further detail, raising the following main points:

- A mix of accommodation would be better rather than only student accommodation.
- Why was there a need to consider this application this evening when the consultation period did not finish until 24 May 2018?
- Why was it safe for proposed residents to live below footpath level, whilst in other areas of the city ground floor bedrooms were not permitted?
- This type of purpose built accommodation was not always filled.
- It was reassuring to hear the University of Lincoln's support for Article 4 Direction.
- This was excellent use of a 'run down' area.
- Officers should be commended on negotiations resulting in an impressive scheme.

The Planning Officer offered the following points of clarification to members:

- The wall along the pavement edge of the new development would envelope the subterranean areas as a flood risk mitigation measure agreed with the Lead Local Flood Authority and was an improvement on the existing situation.
- In relation to the future sustainability of the use of the building, there was no requirement for an applicant to demonstrate a specific need for student accommodation. A mixed use policy supported the wider use of areas of the city including student accommodation.

Members asked whether mitigation measures would be used by developers in the future to allow flood risk areas to accommodate habitable sleeping accommodation on the ground floor.

The Planning Manager clarified that not all flood risk areas were equal dependant on the different levels of flood risk across the city. It may be possible for such mitigation measures to be used in future development if appropriate.

RESOLVED that:

1. As the overall public consultation period for the application did not expire

until 24 May 2018 (as a result of the press and site notices published), authority be delegated to the Planning Manager to issue planning permission subject to the planning conditions listed below.

2. However, should there be any further material planning considerations raised (within correspondence received following the Planning Committee agenda being published) that had not already been considered in this report or that could not be addressed by existing or additional planning conditions, the application would be referred back to the next available Planning Committee for the consideration of Members.

Standard Conditions

- Timeframe of the planning permission
- Approved Plans

Conditions to be discharged before commencement of works

- Materials Schedule and Detailed Plans (Windows etc.)
- Contaminated Land Remediation
- Archaeology
- Site Drainage
- Air Quality and Noise Mitigation

Conditions to be discharged before use is implemented

- Building-wide Management Plan
- Scheme of Landscaping
- Refuse Storage

Conditions to be adhered to at all times

- Construction Working Hours and Deliveries
- Scheme of External Site Lighting

9. Application for Development: 1 St Pauls Lane, Lincoln

The Principal Planning Officer:

- a. advised that retrospective planning permission was requested for installation of perspex dining igloos on the roof terrace of Tap House Bar and Kitchen, situated at the corner of St. Pauls Lane and Westgate
- b. advised that the proposals were to retain two structures described by the applicant as 'dining igloos' in a dome format, however, there were two further unauthorised structures that had been erected prior to and alongside these structures, i.e. a building which had the appearance of a shed and a roof-top bar, all three types of structure having a differing appearance
- c. clarified that the application building was not listed but located within the Cathedral and City Centre Conservation Area (No.1) and adjacent to the ramparts of the Lincoln Castle, a Grade I Listed Building and Scheduled Monument

- d. reported that there was no relevant planning history for the site but it was important to note that the current application was retrospective for the two dome structures shown in the plans attached to this report, moreover, these were erected at different points in time alongside the 'shed' structure and external bar, all without planning permission, although only the domes were shown within this application
- e. provided details of the policies pertaining to the application as follows:
- Policy LP1 A Presumption in Favour of Sustainable Development
 - Policy LP2 The Spatial Strategy and Settlement Hierarchy
 - Policy LP3 Level and Distribution of Growth
 - Policy LP5 Delivering Prosperity and Jobs
 - Policy LP7 A Sustainable Visitor Economy
 - Policy LP13 Accessibility and Transport
 - Policy LP17 Landscape, Townscape and Views
 - Policy LP25 The Historic Environment
 - Policy LP26 Design and Amenity
 - Policy LP29 Protecting Lincoln's Setting and Character
 - Policy LP31 Lincoln's Economy
 - National Planning Policy Framework
- f. outlined the responses made to the consultation exercise
- g. advised members of the main issues to be considered as part of the application as follows:
- The Principle of the Development;
 - The Impact of the Proposals upon Heritage Assets;
 - Other Matters; and
 - The Planning Balance.
- h. concluded that:
- The presumption in favour of sustainable development required by the National Planning Policy Framework would not apply to the proposals as there would be conflict with the three strands of sustainability that would apply to development as set out in the planning balance.
 - Therefore, there would be harm caused by approving the development. As such, it was considered that the application should not benefit from planning permission for the reasons identified in the report and within the officer's recommendation.

Barbara Griffin, representing the Applicant, addressed Planning Committee in support of the proposed development, covering the following main points:

- She spoke on behalf of the owners of 1 St Paul's Lane
- This was a small project.
- The dome structures were temporary.
- She read out a statement from the owner:
 - In the winter, the premises needed sustainable business and these pods contributed to help it survive.
 - The pods were popular over the winter months.

- They provided kudos to the Lincoln area.
- The restaurant took bookings from as far away as Malta, China and the United States with worldwide recognition.
- The 2.1 metre wide temporary structures were used in the winter months to help keep the business afloat.
- The use of umbrellas did obstruct the view of the Cathedral when up or down, whilst the domes did not.
- There were other temporary structures around the Castle/Cathedral walls with greater impact, for example the Big Wheel at the Christmas Market.
- These domes had been used in London without issue.
- Please support this local business to encourage more people to visit our historic City.

A motion was moved that the pods be granted planning permission but the other structures demolished.

The Chair advised that this motion would not be possible to implement as the other structures did not have consent and would be dealt with under separate legislation.

Members discussed the content of the report in further detail, making the following individual comments:

- Whether or not these domes were acceptable was a subjective view.
- The pods looked exotic and romantic.
- It was surprising that the owner thought planning permission wasn't needed.
- Perhaps further negotiation with officers could come up with an alternative scheme acceptable to both parties.
- The speaker had said the business would fold if the pods weren't allowed, then afterwards had boasted it received worldwide recognition.
- An event held over 3 days such as the Christmas Market was different to structures that had been erected for quite a few months.
- Would any ordinary person think planning permission for this type of structure was required? Probably not.

The Planning Manager offered the following points of clarification to members:

- The applicant had been advised at pre application stage that the proposals wouldn't be supported, however, he still continued.

RESOLVED that the application be refused permission for the reasons outlined below:-

- By reason of their form, external appearance (including materials of construction) and ancillary lighting, the proposals would be incongruous additions to the roof terrace of the building, which introduced visual clutter and impeded and diminished the quality of the views towards the Scheduled and Grade I Listed Lincoln Castle, thereby causing harm to its setting and significance.
- What is more, the impact upon the character and appearance of the Cathedral and City Centre Conservation Area was not preserved by the proposal and diminished the character of the area in a harmful way.
- The proposals would therefore be contrary to the requirements of Policies

LP5, LP17, LP25, LP26, LP29 and LP31 of the Central Lincolnshire Local Plan, as well as the requirements of Sections 7 and 12 and the social and environmental sustainability principles referred to in the National Planning Policy Framework.

(Councillor Brothwell left the meeting at this point to attend a prior engagement.)

10. Application for Development: 27-29 Clasketgate, Lincoln

The Planning Manager:

- a. advised that planning permission was sought for the erection of a 63no. bedroom Hotel (Use Class C1) and Restaurant (Use Class A3) following demolition of existing building (Revised Description)
- b. described the location of the application site sloping upwards heading north along Flaxengate
- c. provided details of the policies pertaining to the application as follows:
 - Policy LP1: A Presumption in Favour of Sustainable Development
 - Policy LP2: The Spatial Strategy and Settlement Hierarchy
 - Policy LP7: A Sustainable Visitor Economy
 - Policy LP12: Infrastructure to Support Growth
 - Policy LP13: Accessibility and Transport
 - Policy LP14: Managing Water Resources and Flood Risk
 - Policy LP16: Development on Land affected by Contamination
 - Policy LP18: Climate Change and Low Carbon Living
 - Policy LP25: The Historic Environment
 - Policy LP26: Design and Amenity
 - Policy LP29: Protecting Lincoln's Setting and Character
 - Policy LP31: Lincoln's Economy
 - Policy LP33: Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area
 - National Planning Policy Framework
- d. outlined the responses made to the consultation exercise
- e. advised members of the main issues to be considered as part of the application as follows:
 - National and local planning policy
 - Impact on designated and non-designated heritage assets
 - Impact on visual amenity
 - Impact on residential amenity
 - Highway safety, access and parking
 - Foul and surface water drainage
 - Potential land contamination and other environmental impacts
- f. concluded that:
 - The application site was sustainably located within Lincoln's Central Mixed Use Area, where Hotel (C1) and Restaurant (A3) uses were supported in principle, and was considered an appropriate location for visitor accommodation; in accordance with Policy LP2 'The

Spatial Strategy and Settlement Hierarchy' and LP33 'Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area' of the Central Lincolnshire Local Plan (2017).

- The proposed development would contribute positively towards the sustainable growth of Lincoln's visitor economy, and supported and enhanced the City's role as a key destination for tourism and leisure; in accordance with Policy LP7 'A Sustainable Visitor Economy' and Policy LP31 'Lincoln's Economy' of the Central Lincolnshire Local Plan (2017).
- It would deliver an attractive building that would reinforce local distinctiveness and make a positive contribution to the character and appearance of Conservation Area No. 1 'Cathedral and City Centre' and not cause undue harm to the setting of identified listed buildings; in accordance with the duties contained with Section 66(1) and Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990.
- Notwithstanding the above, further work was needed to address the following elements of national planning policy;
 - 1) Potential indirect impacts of the development on the setting and context of the nearby Scheduled Monument, and what level of harm may result (paragraphs 132-134 of the NPPF); and
 - 2) Whether the Roman remains thought to be present on the site were demonstrably of equivalent significance to the nearby Scheduled remains, and therefore themselves subject to the relevant NPPF policies protecting designated heritage assets in accordance with NPPF paragraph 139.
- However, it was considered these requirements could be adequately addressed through the application of appropriately worded conditions that would enable a staged approach to delivering the proposed development.

Members discussed the content of the report in further detail, offering general support to the scheme to bring about improvements to the local area, providing a commercially viable property, with reassurance that archaeological conditions had been imposed.

One member voiced concern about the scale and massing of the proposed development and the continuation of tall buildings along Clasketgate, although the boutique style design was welcomed and he supported the proposals apart from accepting the reservations made by Historic England.

RESOLVED that planning permission be granted subject to the following conditions:

- 3 year time limit
- In accordance with approved plans and documents
- No demolition of existing building until redevelopment contract in place
- Internal assessment of existing building prior to demolition
- Detailed Written Scheme of Investigation for mitigation of impacts of shallow foundations
- Further intrusive evaluation to establish nature and significance of Roman remains
- Final Mitigation Strategy to address the impacts of piled foundations upon Roman remains

- Final Site Report and deposition of site archive
- Samples of materials
- Window specification and measures to reduce the passage of sound into hotel rooms
- Details of delivery vehicles and times
- Surface water management strategy
- Standard contaminated land conditions
- Scheme for the extraction, filtration and abatement of cooking odours
- Noise impact assessment prior to the installation of any stationary external plant or machinery

11. Application for Development: 62 Baker Crescent, Lincoln

The Planning Manager:

- a. described the location of the application site to the east of Fulmar Road in the Birchwood area of Lincoln, at the northern end of Baker Crescent, a two storey semi-detached property with a detached single garage
- b. advised that permission was sought for a two storey extension to the side of the property which would provide additional bedrooms and living accommodation
- c. reported that this application was being presented to members because the applicant was related to an employee of the City of Lincoln Council
- d. provided details of the policies pertaining to the application as follows:
 - National Planning Policy
 - Central Lincolnshire Local Plan:
 - Policy LP26 - Design and Amenity
- e. outlined the responses made to the consultation exercise
- f. advised members of the main issues to be considered as part of the application as follows:
 - Visual amenity and design
 - Residential amenity and impact on neighbours
 - Highways
- g. concluded that:
 - It was considered that the proposed development would be in accordance with local plan policy LP26.
 - The height and scale of the extension was in keeping with the existing and neighbouring properties and would be constructed of materials to match.
 - The amenities which all existing and future occupants of neighbouring properties may reasonably expect to enjoy would not be unduly harmed by the proposed development and as such was considered acceptable subject to the conditions set out below.

Members discussed the content of the report in further detail.

RESOLVED that planning permission be granted subject to the following conditions:

- Development to commence within 3 years
- Development to be carried out in accordance with the plans
- Samples of bricks to be approved prior to commencement of development

12. Application for Development: Skellingthorpe Road Playing Field , Skellingthorpe Road, Lincoln

The Planning Manager:

- a. reported that retrospective planning permission was sought for the siting of 1no. storage container relocated from Boultham Park, following the sale of land for development
- b. advised that the application related to Skellingthorpe Road Playing Fields, located on the north side of Skellingthorpe Road, opposite the main entrance to Hartsholme Country Park.
- c. confirmed that the application site fell within the boundary of the Western Growth Corridor Sustainable Urban Extension (CL819) as allocated within the Central Lincolnshire Local Plan (2017), however, indicative site layouts suggested that the playing fields would remain undeveloped open space
- d. provided details of the policies pertaining to the application as follows:
 - Policy LP1: A Presumption in Favour of Sustainable Development
 - Policy LP23: Local Green Space and other Important Open Space
 - National Planning Policy Framework
- e. outlined the responses made to the consultation exercise
- f. advised members of the main issues to be considered as part of the application as to whether the development was in accordance with Policy LP23 'Local Green Space and other Important Open Space' of the Central Lincolnshire Local Plan (2017)
- g. concluded that the development had not affected the provision of open space nor resulted in detrimental impacts on the character and appearance of the surrounding area, ecology and any heritage assets; in accordance with Policy LP23 'Local Green Space and other Important Open Space' of the Central Lincolnshire Local Plan (2017) and relevant guidance within the National Planning Policy Framework (2012).

Members discussed the content of the report in further detail, receiving confirmation of the legitimate reason for submission of a retrospective planning application.

RESOLVED that planning permission be granted subject to the following conditions:

Standard Conditions

- 01) With the exception of the detailed matters referred to by the conditions of this consent, the development hereby approved shall be carried out in accordance with the drawings listed within Table A below.
The works shall be carried out in accordance with the details shown on the approved plans and in any other approved documents forming part of the application.

Reason: To ensure the development proceeds in accordance with the approved plans.

Conditions to be discharged before commencement of works

None.

Conditions to be discharged before use is implemented

None.

Conditions to be adhered to at all times

None.

Table A

The above recommendation has been made in accordance with the submitted drawings identified below:

Drawing No.	Version	Drawing Type	Date Received
		Location Plan	19th February 2018
		Block plans	19th February 2018

13. Application for Development: Boultham Park Allotments, Hall Drive, Lincoln

The Planning Manager:

- a. reported that retrospective planning permission was sought for the siting of 1no. storage container relocated from Boultham Park, following the sale of land for development
- b. advised that the application site related to Boultham Park Allotments, located on the east side of Hall Drive, opposite Lincoln Indoor Bowls Club
- c. confirmed that the application site was identified as Important Open Space (IOS) within the Central Lincolnshire Local Plan (2017), and fell within Flood Zone 3 as shown on the Environment Agency's Flood Map for Planning (Rivers and Sea)
- d. provided details of the policies pertaining to the application as follows:
 - Policy LP1: A Presumption in Favour of Sustainable Development
 - Policy LP23: Local Green Space and other Important Open Space
 - National Planning Policy Framework
- e. outlined the responses made to the consultation exercise

- f. advised members of the main issues to be considered as part of the application as to whether the development was in accordance with Policy LP23 'Local Green Space and other Important Open Space' of the Central Lincolnshire Local Plan (2017)
- g. concluded that the development had not affected the provision of open space nor resulted in detrimental impacts on the character and appearance of the surrounding area, ecology and any heritage assets; in accordance with Policy LP23 'Local Green Space and other Important Open Space' of the Central Lincolnshire Local Plan (2017) and relevant guidance within the National Planning Policy Framework (2012).

Members discussed the content of the report in further detail.

RESOLVED that planning permission be granted subject to the following conditions:

Standard Conditions

- 01) With the exception of the detailed matters referred to by the conditions of this consent, the development hereby approved shall be carried out in accordance with the drawings listed within Table A below.
The works shall be carried out in accordance with the details shown on the approved plans and in any other approved documents forming part of the application.

Reason: To ensure the development proceeds in accordance with the approved plans.

Conditions to be discharged before commencement of works

None.

Conditions to be discharged before use is implemented

None.

Conditions to be adhered to at all times

None.

Table A

The above recommendation has been made in accordance with the submitted drawings identified below:

Drawing No.	Version	Drawing Type	Date Received
		Location Plan	9th February 2018
		Block plans	9th February 2018

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Application Number:	2017/1393/RG3
Site Address:	Land Adjacent To A46 Ring Road And North Of Queen Elizabeth Road, Lincoln
Target Date:	29 June 2018
Agent Name:	Halsall Lloyd Partnership
Applicant Name:	City of Lincoln Council - Mr Clive Pridgeon
Proposal:	Erection of 325 dwellinghouses, including 8 flats, facilitated by the demolition of existing flats known as Garfield View and Woodburn View. Associated infrastructure and external works including new footpath link to Clarendon Gardens, the provision of new parking bays to Garfield Close and Woodburn Close and hard and soft landscaping and children's play area (REVISED PLANS).

Background - Site Location and Description

Site Location and Description

The application site is situated within Ermine West to the north of the city and adjoins development to the south within the residential streets which run perpendicular to Queen Elizabeth Road and between the main north-south routes of Burton Road (to the west) and Riseholme Road (to the east). Meanwhile, to the north is the first section of the duelled southbound A46, which begins at Riseholme Roundabout. The site is shown as being allocated for housing in the Central Lincolnshire Local Plan.

As outlined in the Lincoln Townscape Assessment, “the majority of the current townscape of the Ermine West Character Area dates from the building of the Ermine West Estate by the City of Lincoln Council in the Post-War Period [1946-1966 AD].”

Description of Development

The application effectively brings together two parcels of land in separate ownership as shown below:



The two parcels incidentally closely align with the administrative boundaries of the City of Lincoln with West Lindsey, as such, West Lindsey are also considering an application for the development but the City of Lincoln are the lead authority with the application.

The development is for a significant number of dwellings, all of which would be in two-storey format, including a number of flats. The access into the site would be from either end of Queen Elizabeth Road from existing cul-de-sacs known as Garfield Close, to the western, Burton Road end; and Woodburn Close, to the eastern, Riseholme Road end. This would necessitate the demolition of two of the existing three-storey blocks of housing known as Garfield View and Woodburn View. The application also includes some access works to Garfield Close and Woodburn Close, including road widening.

Beyond these access points, the principle circulations routes into the site therefore run parallel to Queen Elizabeth Road but it is new housing development that primarily backs up to existing accommodation, not the routes themselves. The dwellings would be arranged predominantly in a number cul-de-sacs to the north and eastern end of the site and, as mentioned, a linear arrangement to the southern side, which adjoins existing development in Ermine West.

The green spaces within the site will be through a mix of the retention of the existing tree belt which separates the two portions of land but also to either end, incorporating SUDs and other landscape features.

The dwellings proposed are of a modern appearance and do not copy the form and detailing of the simple dwellings which characterise the Ermine West area. The closest comparable to the appearance of the development proposed are recent schemes involving the same architect team for sites at Blankney Crescent; Ingleby Crescent / Welton Gardens; and Thoresway Drive / Hatcliffe Gardens, all in Ermine East.

EIA Development

Officers have carried out a screening opinion for the development and does not consider that the proposed development is Environmental Impact Assessment development. Therefore the submission of an environmental statement is not required under the terms of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017.

Site History

No relevant site history.

Case Officer Site Visit

Undertaken on 15 February 2018.

Policies Referred to

- Policy LP1 A Presumption in Favour of Sustainable Development
- Policy LP2 The Spatial Strategy and Settlement Hierarchy
- Policy LP3 Level and Distribution of Growth
- Policy LP9 Health and Wellbeing
- Policy LP10 Meeting Accommodation Needs
- Policy LP11 Affordable Housing
- Policy LP12 Infrastructure to Support Growth
- Policy LP13 Accessibility and Transport
- Policy LP14 Managing Water Resources and Flood Risk
- Policy LP16 Development on Land affected by Contamination

- Policy LP17 Landscape, Townscape and Views
- Policy LP18 Climate Change and Low Carbon Living
- Policy LP20 Green Infrastructure Network
- Policy LP21 Biodiversity and Geodiversity
- Policy LP24 Creation of New Open Space, Sports and Recreation Facilities
- Policy LP26 Design and Amenity
- Policy LP36 Access and Movement within the Lincoln Area
- Policy LP49 Residential Allocations - Lincoln
- National Planning Policy Framework

Issues

In this instance the main issues to consider are as follows:-

- 1) The Principle of the Development;
- 2) Provision of Affordable Housing and Contributions to Services;
- 3) The Design of the Proposals and their Visual Impact;
- 4) Implications of the Proposals upon Amenity;
- 5) Sustainable Access, Highway Safety and Air Quality;
- 6) Other Matters; and
- 7) Planning Balance.

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Public Consultation Responses

Name	Address
C.R.C.Greenwold	2 Edendale View Edendale Gardens Lincoln LN1 3RW
Mrs Janice Farrar	1 Riverton View Riverton Close Lincoln LN1 3RA
Mr Michael Stafford	38 Clarendon Gardens Lincoln LN1 3RQ
Mr M Foster	35 Garfield Close Lincoln LN1 3QL
Mr Nigel Williams	375 Burton Road Lincoln LN1 3XE
Mrs Julie Dolby	12 Ridge View Garfield Close Lincoln LN1 3QW

In addition, officers have also been provided with copies of the responses that have been received by West Lindsey District Council. These include correspondence from one resident, which is attached to this agenda.

Statutory Consultation Responses

Consultee	Comment
Highways & Planning	Comments Received
Environmental Health	Comments Received
Shane Harrison	Comments Received
Lincoln Civic Trust	Comments Received
Education Planning Manager, Lincolnshire County Council	Comments Received in relation to impact upon Primary and Secondary Education
County Council Planning	No Response Received
Environment Agency	No objections
Lincolnshire Police	Comments Received
Anglian Water	No Response Received
Bat Conservation Trust	Comments Received
Natural England	Comments Received
Lincolnshire Wildlife Trust	Comments
NHS England	Comments received in relation to Health Impacts of the Development

In addition, officers have also been provided with copies of the responses that have been received by West Lindsey District Council. These include correspondence from Anglian

Water, Burton and Riseholme Parish Councils and Lincolnshire Fire and Rescue. All of which are attached to this agenda.

Consideration

1) The Principle of the Development

a) *Relevant Planning Policies*

Section 38(6) of the Planning and Compulsory Purchase Act 2004 and S70(2) of the Town and Country Planning Act 1990 requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The National Planning Policy Framework (the Framework) is a material consideration in determining planning applications. Framework paragraph 215 indicates that due weight should be given to relevant policies in the development plan according to their consistency with the Framework i.e. the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given.

The development plan comprises the adopted Central Lincolnshire Local Plan (the Plan). During its examination the policies therein were tested for their compliance with the Framework.

In terms of sustainable development, Paragraph 7 of the Framework suggests that there are three dimensions: economic, social and environmental. “These dimensions give rise to the need for the planning system to perform a number of roles:

- **an economic role** – contributing to building a strong, responsive and competitive economy, by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth and innovation; and by identifying and coordinating development requirements, including the provision of infrastructure;
- **a social role** – supporting strong, vibrant and healthy communities, by providing the supply of housing required to meet the needs of present and future generations; and by creating a high quality built environment, with accessible local services that reflect the community’s needs and support its health, social and cultural well-being; and
- **an environmental role** – contributing to protecting and enhancing our natural, built and historic environment; and, as part of this, helping to improve biodiversity, use natural resources prudently, minimise waste and pollution, and mitigate and adapt to climate change including moving to a low carbon economy.”

Meanwhile, at the heart of the Core Planning Principles within the Framework (Paragraph 17) is the expectation that planning should:-

“proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs. Every effort should be made objectively to identify and then meet the housing, business and other development needs of an area, and respond positively to wider opportunities for growth”

Turning to Local Plan Policy, Policy LP1 of the Plan supports this approach and advocates that proposals that accord with the Plan should be approved, unless material considerations indicate otherwise

In terms of the spatial dimension of sustainability, proposals need to demonstrate that they contribute to the creation of a strong, cohesive and inclusive community, making use of previously developed land and enable larger numbers of people to access jobs, services and facilities locally, whilst not affecting the delivery of allocated sites and strengthening the role of Lincoln (Policy LP2). Meanwhile, Policy LP3 sets out how growth would be prioritised and Lincoln is the main focus.

The relatively recent adoption of the Local Plan ensures that there is a very clear picture of the options for growth in Central Lincolnshire.

The Framework expects LPAs to have a 5 year supply of deliverable sites (para. 49) that provide for a full range of market and affordable housing, with an additional buffer of 5%. The buffer should be increased to 20% for authorities who have persistently under delivered against their targets and, although there is some debate regarding what constitutes "persistent under delivery", the view is taken that the Council does not fall within this category. Sites with planning permission contribute towards this supply but Councils must also identify a supply of specific, developable sites or broad locations for growth, for years 6-10 and, where possible, for years 11-15 (para. 47). The supply can contain an allowance for windfall sites under certain criteria (para. 48). Moreover, Policy LP49: Residential Allocations – Lincoln identifies the sites that are allocated for residential use.

b) Location and Supply of Housing

The Council's current housing supply was considered as part of the preparation of the Central Lincolnshire Local Plan and includes those sites allocated for residential development. The application site is one of those that is allocated.

Evidence currently available to officers indicates that the Council is able to demonstrate a five-year supply and local development plan policies can be considered up to date. There is therefore not pressure for the Council to approve development which may not otherwise satisfy the three strands of the Framework as referred to above. This will be referred to in detail throughout the remainder of this report.

Notwithstanding this, the site is located within a sustainable position for the proposed dwellings to meet local demand. Moreover, the location would offer the opportunity to promote sustainable transport choices (due to accessibility by bus, cycle and walking routes) and connections to existing areas of employment, schools and other services and facilities. However, the accessibility to existing services is only one of the issues relevant to the consideration of sustainability.

Officers recognise that the development would deliver economic and social sustainability directly through the construction of the development and indirectly through the occupation of the apartments, spend in the City and retention/creation of other jobs due to the location of the development within the Lincoln Urban Area. In addition, the erection of development in this location would not in itself undermine sustainable principles of development subject to other matters as set out below.

2) Provision of Affordable Housing and Contributions to Services

a) *Relevant Planning Policies*

i) Provision of Affordable Housing

The Framework maintains the principle of creating sustainable, inclusive and mixed communities and calls for local planning authorities to set policies for meeting identified affordable housing needs on site unless offsite provision or a financial contribution of broadly equivalent value can be robustly justified (para. 50). The Council's current policy for affordable housing dictates that 25% of all units should be affordable homes (Policy LP11) for all schemes incorporating 11 or more residential properties.

ii) Other Community Infrastructure and Services

The Framework highlights that planning should be a creative exercise in finding ways of enhancing and improving the places in which people live (para.17). Perhaps most crucially however, is Paragraph 70 which refers to new development and states:

"To deliver the social, recreational and cultural facilities and services the community needs, planning policies and decisions should:

- Plan positively for the provision and use of shared space, community facilities (such as local shops, meeting places, sports venues, cultural buildings, public houses, and places of worship) and other local services to enhance the sustainability of communities and residential environments; and
- Ensure an integrated approach to considering the location of housing, economic uses and community facilities and services."

Paragraph 72 of the Framework refers to the importance of ensuring "that a sufficient choice of school places is available to meet the needs of existing and new communities." The Framework therefore advocates that LPAs should "give great weight to the need to create, expand or alter schools".

In addition, developments which would result in an increase in the number of households within the locality are expected to contribute to improvements to existing playing facilities or provide play and amenity and open space that could be utilised by the development (Policy LP24 of the Plan).

This also aligns with the requirements of Policy LP9 of the Local Plan, which requires that developments of 25 or more dwellings demonstrate how they have taken into account health impacts have been designed into the development. Furthermore, developments should also contribute towards health provision where there is evidence that a development will impact upon current provision.

b) *Affordable Housing*

The development being sizeable and above the threshold for affordable housing would be expected to provide dwellings for affordable purposes onsite. In this instance, this would equate to 81 properties; and the applicant has indicated that it would be their preference that the majority of these are for affordable rent.

c) *Impact upon Education and of the Community Infrastructure Levy*

The County Council as Education Authority would receive contributions to secondary education Provision by virtue of the Community Infrastructure Levy. However, in their consultation response (attached to this report), the County has also referred to the impacts of the development upon primary education. They have requested that the applicant is required to provide a contribution in order to mitigate the impact of the development upon primary education. This would need to be secured through a S106 agreement.

d) *Local Green Infrastructure and Strategic Playing Fields*

The size of the development site would not be sufficient to meet the requirements of policy in respect of on-site provision of strategic playing fields and would be expected to contribute to the development of local green infrastructure projects. As with education provision, this matter can be secured through a S106 agreement.

e) *Impact upon Health*

i) *Health Impact Assessment*

The application is supported by a Health Impact Assessment and it is considered that the outcomes of the checklist and conclusions of the document are reasonable in the context of the scale of development, site context and other matters, i.e. the contributions that will be made to facilities in the city. Officers therefore agree that no further assessment is required to demonstrate compliance with Policy LP9 of the Central Lincolnshire Local Plan.

ii) *Mitigating the Impact on Health Provision*

Notwithstanding the above, including reference in the HIA to GP Services, NHS England has identified that the development would have an impact upon the provision of primary care in the community. Their response highlights the direct action that would need to be taken to address this matter. This includes a contribution to the provision of additional facilities locally. A scheme to mitigate this impact can also be addressed through the signing of a S106 agreement.

f) *Mitigating the Direct Impact of the Development*

All of the matters raised in subsections (b) to (e) above appear to be reasonable and based upon a solid rationale, as such officers are satisfied that these requests would meet the tests relevant to planning obligations referred to in the Framework. Consequently, the provision of onsite affordable housing and schemes to deal with the impact upon other facilities and services can be secured by virtue of a S106 agreement.

The applicant has committed to meeting these requirements and to the signing of a S106 agreement. However, should the applicant subsequently fail to meet these requirements, it could undermine the principles of sustainable development outlined in the Framework. As such, if the S106 agreement has not been signed within six months of the date of Planning Committee, and there is no reasonable prospect of doing so, the Planning Manager will refer the application back to the Planning Committee for further consideration by Members.

3) The Design of the Proposals and their Visual Impact

a) *Relevant Planning Policy*

So far as this issue is concerned, as alluded to above, the proposals must achieve sustainable development and it is the social dimension of sustainability that relates to design. Paragraph 7 of the Framework requires the creation of high quality built environment. In addition, the policy principles outlined in Paragraphs 17, 58, 60, 61 and 64 of the Framework also apply. Moreover, the Framework states that good design is a key aspect of sustainable development and is indivisible from good planning. Design is to contribute positively to making places better for people (para. 56). To accomplish this development is to establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live and responding to local character and history (para. 58). It is also proper to seek to promote or reinforce local distinctiveness (para. 60).

Policy LP26 refers to design in wider terms and requires that “all development, including extensions and alterations to existing buildings, must achieve high quality sustainable design that contributes positively to local character, landscape and townscape, and supports diversity, equality and access for all.” The policy includes 12 detailed and diverse principles which should be assessed.

b) *Assessment of the Implications of the Proposals*

i) *The Site Context*

The application site does not contain any nationally designated (protected) heritage assets, such as scheduled monuments, listed buildings or registered parks and gardens. However, the location of the site at the edge of the city where it adjoins the A46 is sensitive; and the site is currently dominated by an established tree belt to its centre, along the administrative boundary of the City where it meets West Lindsey.

The site adjoins established housing, the largest of which is predominantly outside but along the southern perimeter of the site, in the form of three storey buildings incorporating maisonette properties. The majority of the wider mix of properties in the locality tend to be two storeys in height and set within soft landscape dominated streets.

ii) *The Submission*

The visual implications of the proposals for the site are key to the assimilation of development into its context and the creation of high quality built environment. In this instance the applicant has proposed a mixed approach to the character of the development involving streets, woodland edges and mews character areas. Indicative visuals of these are included below:



Above and below are Street Character Area Concept Images





Above and below are Mews Character Area Concept Images





Woodland Edge Character Area Concept Image

In terms of design and layout of the development, the proposals will reinforce and provide connectivity into surrounding areas and the grain of development is similar to established patterns of development close-by, including road arrangements. However, development engages with the tree belt and takes advantage of it as an ecological and natural amenity resource and utilises it as a transition between the two elements of the site, the south being predominantly more spacious and the north being more closely aligned with the existing grain of development. Overall, the layout, is positive in its appreciation of the positive characteristics of the Ermine West area of the city, particularly the form of the 'village green' spaces that typify a number of the streets that lead from Queen Elizabeth Road. These are with mature trees and housing around the edges.

Meanwhile, the scale of the proposed dwellings would be commensurate with that of other buildings in their context, which would allow for a successful integration with the surrounding townscape. Furthermore, officers are satisfied that the detailed design, including façade treatment, materials and roof form would be modern and provide visual interest.

Consequently, officers would advise Members that the development would not be harmful to the character of the area or to the social sustainability of the locality, as required by the Framework. Notwithstanding this, it would be necessary to control the final appearance of the dwellings through the materials of construction and other fine details in relation to site layout through the imposition of planning conditions.

c) *Summary on this Issue*

The visual implications of the proposals for the site are key to the assimilation of development into its context and the creation of high quality built environment. Officers are satisfied that the application demonstrates that the proposals could be accommodated within the site in the context of the established grain of development, including in terms of the number and spacing of dwellings, so would not be harmful to the character of the area.

The proposals would therefore not be harmful to the character of the area for such reasons and the provision of a high quality built development would improve the social sustainability of the locality as required by the Framework. As such, officers would advise Members that there would not appear to be grounds to resist the development in this regard.

4) Implications of the Proposals upon Amenity

a) *Relevant Planning Policy*

In terms of national policy, the NPPF suggests that development that results in poor design and/or impacts upon the quality of peoples' lives would not amount to sustainable development. Consequently, the implications of both are key to the consideration of the acceptability of the principle of development within a given site. Moreover, the Framework (Paragraph 9) sees "seeking positive improvements in the quality of the built, natural and historic environment, as well as in people's quality of life" as being important to the delivery of sustainable development, through "replacing poor design with better design" and "improving the conditions in which people live" amongst others. Furthermore, the core principles of the Framework (Paragraph 17) indicate that "planning should...always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings".

Policy LP26 of the Plan deals with design and amenity. The latter refers to the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy and suggests that these must not be unduly harmed by, or as a result of, the development. There are nine specific criteria which must be considered. The policy is in line with the policy principles outlined in Paragraphs 17, 59 and 123 of the NPPF. Indeed, Paragraph 123 of the Framework suggests that "decisions should aim to...avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development".

b) *Assessment of the Implications of the Proposals*

i) *Impacts of the Scale of the Buildings in the Development*

Upon the basis of the orientation of the site and the proximity of existing and proposed dwellings, it is considered that the scale and massing of the proposed buildings would not result in an overbearing effect resulting from the development. As such, there would not be conflict with the planning policies relevant to this aspect of residential amenity.

ii) *Overlooking and Loss of Privacy*

The development would inevitably introduce overlooking of shared spaces and the properties that adjoin the site that have not experienced this previously, however, this would be no more harmful than the window to window relationship within the immediate context. Moreover, the distances between the existing and proposed properties would be spaced sufficiently to ensure that there would not be harm caused through overlooking or loss of privacy that would be harmful to the amenities that the existing or future occupiers of those dwellings would expect to enjoy.

iii) *Noise and Disturbance from the Development*

Upon the basis of the nature of the proposed use, i.e. residential accommodation, it is inevitable that there will be a certain element of noise associated with vehicle movements to and from the proposed dwellings and by virtue of the future occupants. Moreover, the principal impacts associated with the development will be the comings and goings of people and vehicles through the proposed entry points into the development.

It is clear that the changes would result in a significant change in terms of the manner in which Garfield Close and Woodburn Close are currently accessed, both presently being cul-de-sacs; and the a new pedestrian link from the development would be formed into Clarendon Gardens. However, due to the separation distance of the existing roads to properties, officers are satisfied that the proposals would not adversely affect the amenities of the occupants of those properties as the noise and disturbance would not be significantly different from that previously experienced and should not be any different to a normal residential context elsewhere within Ermine West. What is more, as the scheme is effectively split into two separate elements, the loading of traffic and any noise created by traffic would be split between these two streets.

Similarly, the properties adjacent to the proposed footpath link currently face an extensive footpath that runs along the frontages of all the properties located adjacent to the southern boundary of the site. As such, it is considered that the impacts of this new link would not be harmful to amenity. Furthermore, in terms of the general activity associated with people utilising their gardens and their comings and goings would also not be of sufficient detriment to the enjoyment of occupants of other properties to warrant the refusal of the application.

Given the proximity of the site to neighbouring properties, there is potential for the impacts of construction to disturb residents. As such, officers agree with the Council's Pollution Control Officer that it would be appropriate to ensure that adequate control measures are put in place. As such, it is recommended that details of the construction management for the site are agreed, including a demolition strategy for the development; the location(s) of compound(s); parking for construction staff; wheel washing facilities; and working and delivery hours.

iv) Impacts of Noise from the A46

The principal properties that would be effected by the location of the proposals would be those that are adjacent to the A46 in terms of the noise and pollution created by traffic utilising this route.

The application is supported by an Acoustic Report, which seeks to establish the existing noise climate affecting the proposed development site using a combination of noise monitoring and modelling. Upon the basis of the proximity of the site in relation to the A46, the noise climate is heavily dominated by road traffic, as such this forms the principal focus of the report.

Officers have been provided with an assessment of the applicant's supporting Acoustic Report by the Council's Pollution Control Officer. He has returned comments in two separate areas, firstly in relation to internal noise and, secondly, in relation noise in external areas.

In terms of the first issue, it is clear that the PCO and the applicant differ in their approach to the interpretation of the British Standards. Moreover, the applicant suggests that the noise levels experienced internally should be 35dB (A) but the PCO suggests that this should be 30dB (A). Having considered his advice to officers, it is considered that there would not be a justifiable reason to avoid providing the best mitigation available for residents of the worst affected properties. As such, were Members minded to grant approval for the development, this would be with the expectation that the applicant/developer incorporates a scheme of mitigation to those properties that would need to be agreed by planning condition.

Members will note that a similar approach was taken for the developments at Ingleby Crescent and Blankney Crescent, where reasonable internal noise levels were achieved across the site when windows were not open and an alternative means of acoustically treated ventilation was to be provided. Furthermore, whilst the Lincoln Civic Trust has raised concern regarding whether it would be appropriate to incorporate acoustic fences as part of the development, given the steep incline of the banks the officers understand that this would funnel any noise up and over a fence atop the bank. For a fence to be effective, it needs to be located immediately alongside a noise source between the noise and the effected property. However, as properties are significantly higher this approach would not be appropriate.

The submitted Design and Access Statement, along with the supporting Design Process statement, provides some additional discussion on mitigation for the wider site, including the revised orientation/layout of the development. In brief, it is argued that the layout has been developed to utilise the buildings fronting the A46 to shield the garden/amenity areas. However, in terms of this second issue, the PCO has highlighted that there will still be many garden areas where the upper guideline levels detailed in British Standards will be exceeded and, therefore, there is likely to be a significant observed adverse effect due to noise.

Officers note the concerns of the PCO and whilst the noise in the gardens of the properties may not be ideal, with conditions there would be sufficient mitigation measures in place to protect occupants of the properties indoors but the outdoor impact could only reasonably be addressed by significant alterations to the layout of the site. This would include significantly setting the dwellings back from the northern boundary of the site but also potentially providing a solid band of development with no breaks in the frontage, even roadways leading north-south would enable noise to penetrate further into the site. Such, significant alterations would undoubtedly affect the viability of the development and potentially jeopardise the delivery of a large number of dwellings on an allocated site, which would otherwise make a significant contribution to housing supply in Central Lincolnshire. This would also include a significant portion of affordable homes, which cannot be ignored. Furthermore, the development would also result in the enhancement of green infrastructure and strategic playing fields nearby, which would help to mitigate the impact of the harm caused by noise in individual gardens. In light of this, it is considered that these benefits would weigh in favour of the proposals and would outweigh any harm resulting from the development.

v) Crime, Anti-Social Behaviour and Safety

A consultation response has been received as part of the application from Lincolnshire Police whom offer pertinent advice in relation to the design of the dwellings and boundaries to design in crime reduction measures. However, the provision of these facilities is not an obligation of planning, rather it is for the applicant to consider. Notwithstanding this, officers are satisfied that there would not be unacceptable risks to residents resulting from the development and that the layout of the site would promote sufficient legibility to promote safe environments.

In light of comments received from a resident of Burton Road, the applicant has revised the position of the SUDs pond adjacent to the properties at the west of the site and omitted a footpath link that would duplicate the one shown through the tree belt to the centre of the site. Nonetheless, concerns expressed by a resident in relation to the safety of residents (particularly children), in terms of the SUDs ponds are not new concerns with such proposals. Depending on the adopting body, it is possible that they would insist on safety

fences. However, Members will note that this has not always been necessary, as shown at the LN6 Development adjacent to Tritton Road.

Similarly, the security of neighbouring land would be ensured through an appropriate scheme of boundary treatments for the site controlled by condition.

c) *The Planning Balance*

Taking all the above in to account, it is considered that the application would demonstrate that the proposed development could be accommodated within the site in the manner suggested with mitigation. Moreover, upon the basis of orientation and position of the buildings, their proximity to existing development and the scale of the buildings and location of windows, officers are satisfied that there would not be unacceptable harm caused to the occupants of nearby properties in terms of the amenities that they would expect to enjoy.

In addition, the harm that would be caused by noise to properties within the development would not be ideal but the benefits of the scheme would outweigh this harm. There would therefore be compliance with Local Plan policies, as well as the amenity principles in the Framework. However, it would be necessary to agree the details suggested by conditions above, including those matters relevant to construction and the mitigation to properties. Consequently, as these matters can all be resolved by suitable planning conditions Members are advised that this matter would not be sufficient to outweigh the presumption in favour of sustainable development or tip the planning balance against the application.

5) Sustainable Access and Highway Safety

a) *Relevant Planning Policy*

The impacts of growth are enshrined in the Core Planning Principles of the Framework (Paragraph 17), which expects planning to actively manage this growth “to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable”. As such, Paragraph 35 requires that: “developments should be located and designed where practical to [amongst other things] give priority to pedestrian and cycle movements, and have access to high quality public transport facilities; and should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones”.

A number of Local Plan Policies are relevant to the access, parking and highway design of proposals. In particular, the key points of Policy LP13 are that “all developments should demonstrate, where appropriate, that they have had regard to the following criteria:

- a) Located where travel can be minimised and the use of sustainable transport modes maximised;
- b) Minimise additional travel demand through the use of measures such as travel planning, safe and convenient public transport, walking and cycling links and integration with existing infrastructure;
- c) Should provide well designed, safe and convenient access for all, giving priority to the needs of pedestrians, cyclists, people with impaired mobility and users of public transport by providing a network of pedestrian and cycle routes and green corridors, linking to existing routes where opportunities exist, that give easy access and permeability to adjacent areas”

There are also transport measures referred to in Policy LP36, which more specifically refers to development in the 'Lincoln Area', the key measures add to and reinforce the criteria within Policy LP13. As such, they are intended to reduce the impact upon the local highway network and improve opportunities for modal shift away from the private car. In particular, development should support the East West Link in order to reduce congestion, improve air quality and encourage regeneration; and improve connectivity by means of transport other than the car.

Paragraph 32 of the Framework suggests that the residual cumulative impacts of the development would need to be severe for proposals to warrant refusal. This is reinforced by Policy LP13 of the Local Plan which suggests that only proposals that would have "severe transport implications will not be granted planning permission unless deliverable mitigation measures have been identified, and arrangements secured for their implementation, which will make the development acceptable in transport terms."

b) Access and Highway Safety of the Proposals

i) Concerns Outlined in Responses Received

The Lincoln Civic Trust have raised concerns regarding the implications of the traffic resulting from the development upon the Queen Elizabeth Road / Riseholme Road and the Queen Elizabeth Road / Burton Road junctions and the appropriateness of the road network to serve the development. They have therefore questioned the response received from the County Council, which does not raise any concerns in relation to the traffic impact of the proposals. A resident has also raised concerns regarding traffic in the context of the development.

ii) Access

As noted above, concerns have been expressed regarding additional traffic resulting from the development, particularly in relation to the junctions at either end of Queen Elizabeth Road.

There would clearly be an increase in the number of vehicles accessing and egressing either road serving the development and entering Burton Road and Riseholme Road. The application is supported by various assessments and appraisals and the Highway Authority is satisfied that these junctions as well as others further afield would not be severely affected as a result of the development. They are also satisfied that Monies to be secured through CIL would help to mitigate any impact as this would be utilised for the construction of the Lincoln Eastern Bypass.

Nonetheless, it is important to consider that the site is accessible by various means of transport and it is by no means certain that all residents will either own or make use of a car within peak traffic flows. However, the Travel Plan submitted with the application should be amended either prior to the determination of the application or the implementation of the development in order to ensure that it reflects the comments of the County Council which seek to encourage usage of other forms of transport (there would also be a small payment to the County Council to monitor this). Furthermore, as the Highway Authority has not raised any concerns regarding issues of visibility from the site or noted any incidences of accidents involving the use of the accesses, officers consider that it would be difficult to raise concerns regarding the intensification vehicle movements in a similar position.

Notwithstanding this, officers have engaged with the applicant in order to ensure that the road widths within the development reflect the comments made by the County Council, particularly in respect of shared surfaces, which need to be wider for pedestrian safety.

iii) Car and Cycle Parking

The proposed properties are catered for, primarily with two spaces per property but the Highway Authority considers that this is not sufficient for some larger properties. However, they do not have any specific guidance that they can call upon to support their request. As such, it would not be reasonable to insist that the developer provides further parking, particularly as the reconfiguration of the areas around the dwellings could reduce separation of dwellings from the road, lead to more cars being visible in the street and lead to there being less landscaping to soften the appearance of streets. Moreover, providing additional parking could be more harmful than beneficial, particularly in light of the fact that the roads would be sufficiently wide in many places to enable further vehicles to be parked in the streets created by the development.

c) Summary on this Issue

Consequently, in the absence of evidence from the relevant authority to suggest matters of congestion or safety are so severe that refusal is warranted in relation to traffic generation or road safety, there would not appear to be justification to refuse the application upon such grounds.

6) Other Matters

a) Site Drainage

i) Relevant Planning Policy

The Framework sets out a strategy for dealing with flood risk in paragraphs 93-108 inc. which involves the assessment of site specific risks with proposals aiming to place the most vulnerable development in areas of lowest risk and ensuring appropriate flood resilience and resistance; including the use of SUDs drainage systems. Meanwhile, Policy LP14 of the Plan is also relevant as it reinforces the approach to appropriate risk averse location of development and drainage of sites, including the impact upon water environments.

ii) Assessment of the Implications of the Proposals

The application is supported by a Drainage Strategy and the County Council, as Lead Local Flood Authority, has been in dialogue with the applicant but it has not been possible for both parties to come to an agreement in terms of the overall scheme of drainage for the site. Moreover, there is not consensus regarding the impact of the geology of the site, which the applicant argues would not be conducive overall to a fully SUDs compliant scheme.

The scheme does incorporate two fairly large SUDs ponds, one at either end of the site but the County Council argue that the scheme should incorporate swales to convey water to the ponds. However, the applicant argues that these would not be appropriate in the context of the geology and that they would have a significant impact upon the number of dwellings that could be accommodated within the site which would adversely affect the viability of the development. This is a fundamental element of the sustainability of the development as a fully viable scheme would deliver crucial infrastructure to serve the residents of the

development. Furthermore, the County Council has not objected to the drainage scheme proposed as this has been proven to be proportionate to the development and would not lead to surface water flooding elsewhere; and they have also accepted similar drainage schemes for developments at Blankney Crescent and Ingleby Crescent elsewhere in Lincoln.

Similarly, Members will note that officers have been provided with a consultation response from Anglian Water for West Lindsey District Council, this suggests that the scheme for foul water drainage would need to be agreed by planning condition.

Finally, as the development is large scale and could require raising of levels for drainage or other purposes, it would be necessary for the developer to provide full details of the existing and proposed land levels within the development. This would ensure that there would not be harmful impacts upon neighbouring properties from the perspective of run-off but also ensure that established trees and other vegetation are not adversely affected by changes in levels.

In light of the above, officers are satisfied that the final details of the surface water and foul water drainage schemes for the site could be secured by condition. Subject to the development being constructed in accordance with these details, there would not appear to be grounds to resist the application in relation to these matters.

b) Archaeological Implications of the Development of the Site

i) Relevant Planning Policy

Paragraph 128 of the Framework states that “in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.”

Paragraph 141 of the Framework states that LPAs should ‘require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible.’

Policy LP25 in the Central Lincolnshire Local Plan requires that development does lead to significant detrimental impacts on heritage assets. This issue is directed in relation archaeology that could be non-designated heritage assets.

ii) Assessment of the Implications of the Proposals

The application has been the subject of a search of the Lincoln Archaeological Resource Assessment (LARA). This indicated that the location of the proposed development may have potential to contain archaeological remains associated with prehistoric settlement. An initial appraisal of the Lincoln Heritage Database revealed that a number of undated burials had been discovered in the immediate area. As a result, the applicant was advised to undertake a number of evaluation trenches in order to establish the likelihood of archaeological remains being present on the site.

The evaluation of the site demonstrated that there is a very low likelihood of archaeological remains being present on the site, and that the development is therefore unlikely to have a negative impact on the archaeological resource.

This information has been considered by the City Archaeologist and it is his advice to officers that no further work is required in mitigation of this development. Officers are satisfied with this approach and consider that there would not be conflict with Policy LP25 of the Local Plan or Section 12 of the Framework.

c) Air Quality

i) Relevant Planning Policy

Paragraph 109 of the NPPF introduces the section in relation to the conservation and enhancement of the natural environment. Given that the site is located adjacent within the Air Quality Management Areas (declared by the Council due to the likely exceedance of the national air quality objectives for nitrogen dioxide and particulate matter), this section of the NPPF should be given great weight. It states that “the planning system should contribute to and enhance the natural and local environment by...preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of soil, air, water or noise pollution or land instability”.

Paragraph 120 sets the scene and refers to development being “appropriate for its location”. It goes on to say that “the effects (including cumulative effects) of pollution on health, the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account.” Paragraph 124 refers in more detail to the implications of the location of development within an Air Quality Management Area and requires that “planning decisions should ensure that any new development in Air Quality Management Areas is consistent with the local air quality action plan”.

The Framework also seeks to promote and enable sustainable transport choices and, in doing so, aims to protect and enhance air quality. Paragraph 35 states “developments should be located and designed where practical to....incorporate facilities for charging plug-in and other ultra-low emission vehicles”.

ii) Assessment of the Implications of the Proposals

Officers concur with the Council’s Pollution Control Officer that the proposed development, when considered in isolation, is unlikely to have any significant impact on air quality. However, cumulatively the numerous minor and medium scale developments within the city will have a significant impact if reasonable mitigation measures are not adopted.

Given that the site is at the outer edge of the city and there are air quality issues in the city, this seems entirely reasonable and proportionate to the scale of development that the significant amount of off-street parking to be provided would incorporate electric vehicle recharge points. Officers would advise Members that this matter can be addressed by the planning condition referred to in the response received from colleagues.

d) *Ecology, Biodiversity and Arboriculture*

i) *Relevant Planning Policy*

Paragraph 118 of the NPPF requires LPAs to conserve and enhance biodiversity by refusing planning permission where significant harm resulting from a development cannot be avoided, mitigated or compensated for. Meanwhile, Policy LP21 refers to biodiversity and requires development proposals to “protect, manage and enhance the network of habitats, species and sites of international, national and local importance (statutory and non-statutory), including sites that meet the criteria for selection as a Local Site; minimise impacts on biodiversity and geodiversity; and seek to deliver a net gain in biodiversity and geodiversity.” The policy then goes on to consider the implications of any harm associated with development and how this should be mitigated.

ii) *Assessment of the Implications of the Proposals*

The application is supported by a number of independent and complimentary surveys and documents which help to provide a clearer picture upon any implications associated with the development. These have been assessed in detail by the Lincolnshire Wildlife Trust and they have offered a number of supportive comments in relation to the proposals. However, they have also referred to a number of further enhancements or revisions to the mitigation suggested for the site. The combined mitigation required would not necessarily be restrictive to development and would improve the current situation overall. Subject to these matters, alongside ecological enhancement, being satisfied by condition, it is considered that ecology would not represent an insurmountable constraint to development in the context of Policy LP21 of the Local Plan.

e) *Land Contamination*

i) *Relevant Planning Policy*

As with air quality, Paragraph 109 of the Framework also refers to contamination. Paragraph 120 expands upon this and suggests that “to prevent unacceptable risks from pollution and land instability, planning policies and decisions should ensure that new development is appropriate for its location. The effects (including cumulative effects) of pollution on health, the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account. Where a site is affected by contamination or land stability issues, responsibility for securing a safe development rests with the developer and/or landowner.”

In addition Paragraph 121 states that planning decisions “should also ensure that:

- the site is suitable for its new use taking account of ground conditions and land instability, including from natural hazards or former activities such as mining, pollution arising from previous uses and any proposals for mitigation including land

- remediation or impacts on the natural environment arising from that remediation;
- after remediation, as a minimum, land should not be capable of being determined as contaminated land under Part IIA of the Environmental Protection Act 1990; and
- adequate site investigation information, prepared by a competent person, is presented.”

In terms of Local Plan policies, given the location of the site, Policy LP16 directly refers to the requirements of development in relation to contaminated land.

ii) Assessment of the Implications of the Proposals

The applicant’s consultant is still currently in discussions with the Council’s Scientific Officer who has requested further information. Although this has not been provided to date, it is not essential that the outstanding matters are closed out before the grant of planning permission. Moreover, further detailed information can be provided before built development is undertaken and ultimately the proposals would result in the redevelopment of the site which would lead to remediation of any contamination. In light of this, officers consider that planning conditions could be imposed to deal with land contamination if necessary.

7) Planning Balance

Paragraph 14 of the Framework sets out a presumption in favour of sustainable development which for decision taking means that where relevant policies of the development plan are out-of-date planning permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against policies in the Framework, taken as a whole; or specific Framework policies indicate development should be restricted. There are no restrictive policies that would lead to the proposals not being sustainable. However, a conclusion whether a development is sustainable is a decision that has to be taken in the round having regard to all of the dimensions that go to constitute sustainable development.

In this case, officers recognise that the development would deliver economic and social sustainability directly through the construction of the development and indirectly through the occupation of the dwellings, spend in the City and retention/creation of other jobs due to the location of the development within the City. Whilst the Council currently has a five-year supply of housing, the application site is a housing allocation in the Central Lincolnshire Local Plan. In addition, the location of further residential development in a sustainable location would not undermine the housing supply position, rather it would provide additional choice.

In addition, the benefits of providing the proposed dwellings in a sustainable location would commute to the local community as they would result in the provision of affordable housing, infrastructure and facilities, which would benefit the health and social wellbeing of those living nearby. In particular, the enhancement of public spaces nearby, which would help to mitigate the impact of the harm caused by noise in individual gardens.

Furthermore, as this is a suitably designed development, the implications upon the character of the area and the residential amenities of near neighbours would not have negative sustainability implications for the local community, as they would lead to a development that would be socially sustainable. In addition, with suitable schemes to deal with ecology, drainage, contamination, noise and air quality, the development would be environmentally sustainable.

Thus, assessing the development as a whole, officers are satisfied that all of the strands would be positively reinforced by the proposals. As such, assessing the development as a whole in relation to its economic, social and environmental dimensions and benefits, it is considered that, in the round, this proposal could be considered as sustainable development and would accord with the Local Plan and Framework.

Application Negotiated either at Pre-Application or during Process of Application

Yes, additional information sought in respect of numerous matters as referred to in the application.

Financial Implications

The proposals would offer benefits to economic and social sustainability through spend by new and existing residents and visitors, jobs created/sustained through construction and the operation of the development respectively. In addition, there would be residential properties that would be subject to council tax payments. What is more, the Council would receive monies towards the upgrade of strategic playing fields and local green infrastructure; the NHS towards upgrade of facilities; the County Council receive payment towards infrastructure for primary education and for the monitoring of the Travel Plan; and the proposals will contribute to affordable housing.

Legal Implications

The S106 agreement will require legal input in relation to schemes proposed to deal with affordable housing, NHS and primary education services and strategic playing fields/green infrastructure.

Equality Implications

None.

Conclusion

The presumption in favour of sustainable development required by the National Planning Policy Framework would apply to the proposals as there would not be conflict any of the three strands of sustainability that would apply to development as set out in the planning balance. There would not be harm caused by approving the development so it is considered that the application should benefit from planning permission for the reasons identified in the report and subject to the planning conditions outlined below.

Application Determined within Target Date

Yes, subject to extension of time.

Recommendation

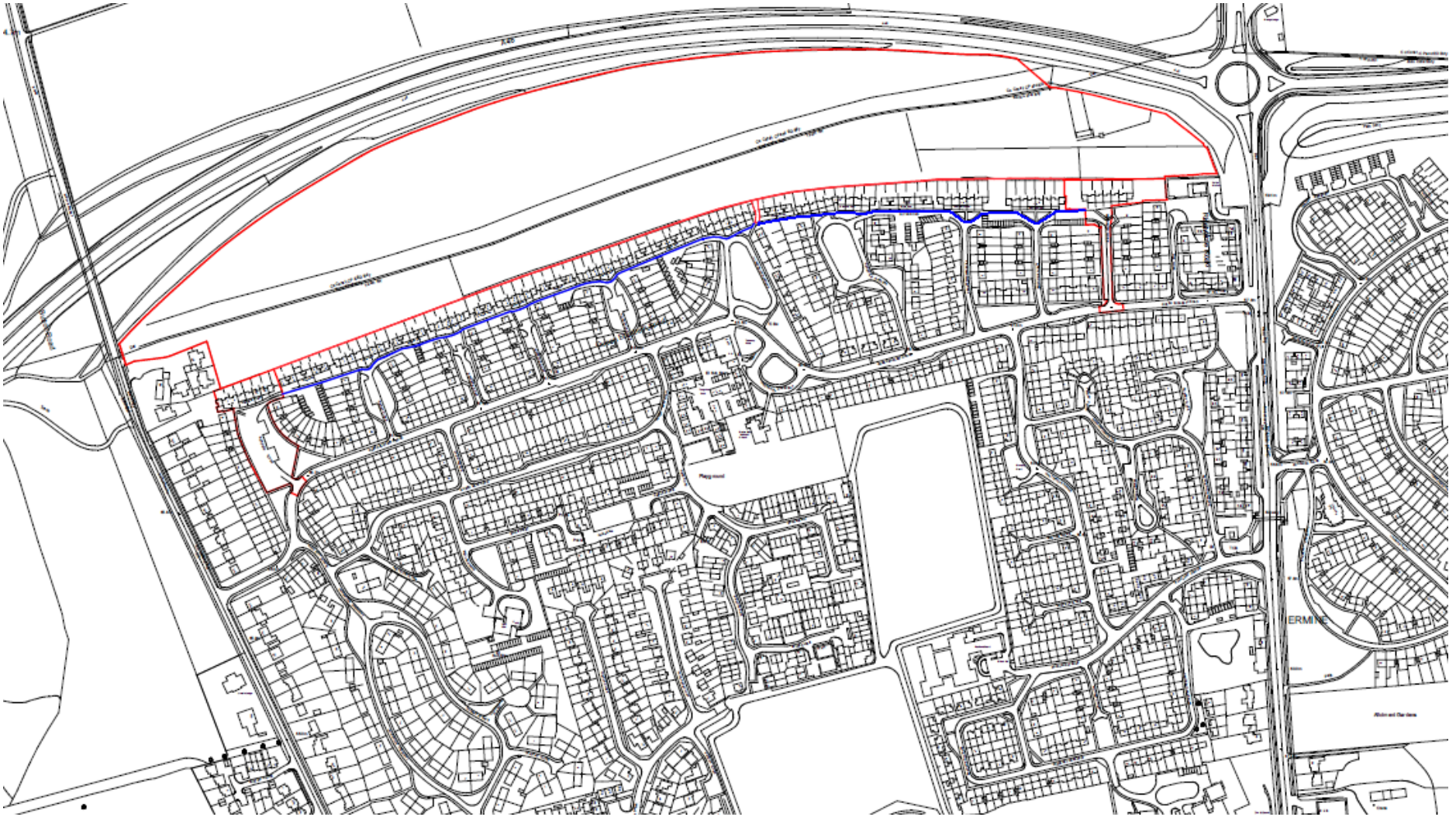
That the application is granted subject to the following conditions:

- Timeframe of Permission (3 Years);
- Approved Plans;

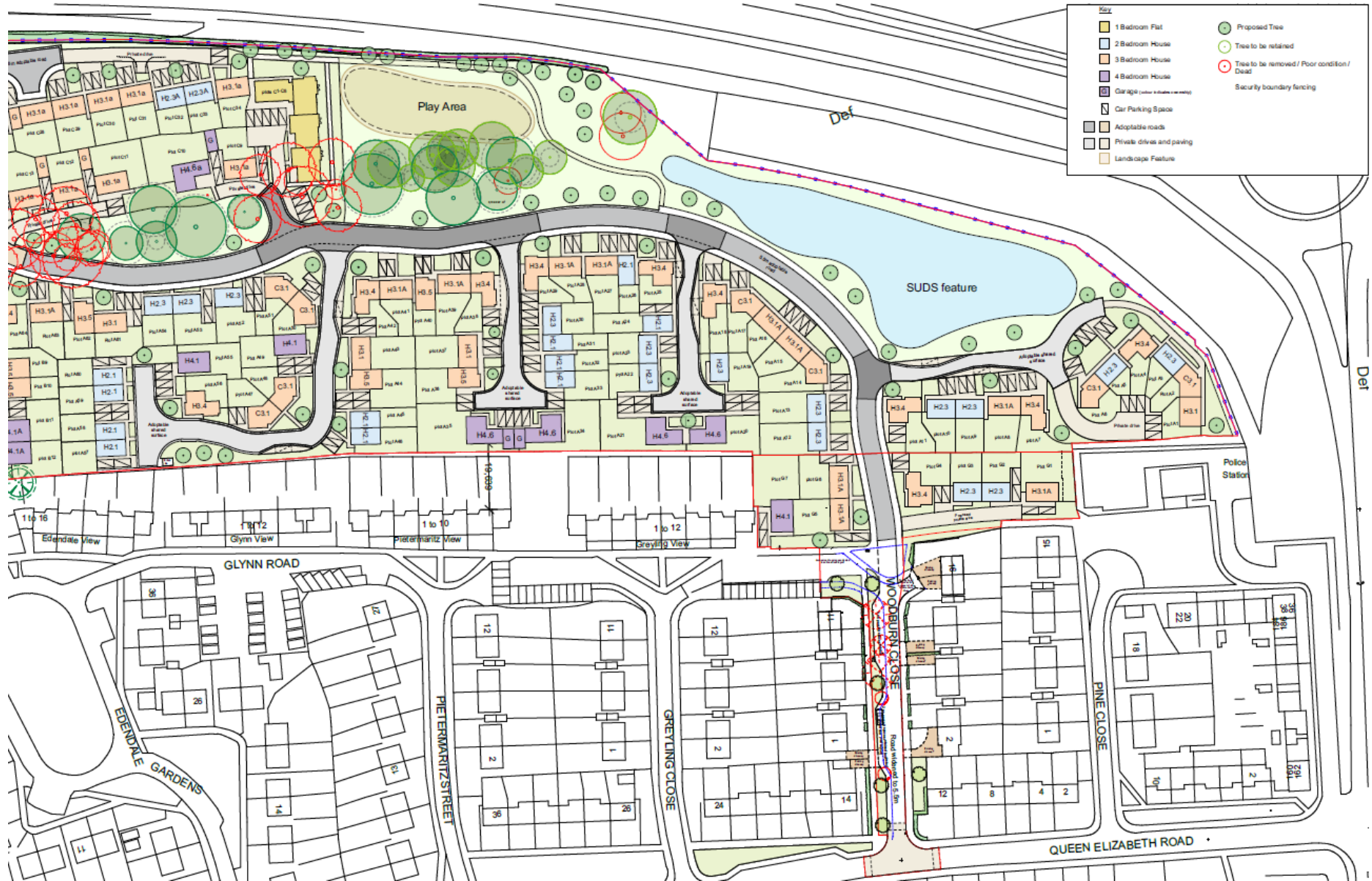
- Schemes to provide Affordable Housing and deal with Impact upon NHS Services and Playing Fields / Play Space;
- Materials of Construction (including surfacing);
- Scheme of Landscaping and Boundary Treatments;
- Scheme of Foul Drainage;
- Contaminated Land Remediation;
- Controls over Scheme for Site Surface Water Drainage;
- Highway Access and Parking;
- Revised Travel Plan before Implementation of the Development (if not amended);
- Strategy for Site Lighting;
- Finished Site Levels;
- Scheme of Noise Mitigation;
- Scheme for Ecological Enhancement and Mitigation including tree protection;
- Scheme for Electric Vehicle Recharging Points;
- Hours of Construction Working and Deliveries; and
- Construction Management (compounds, parking, wheel washing and working/delivery hours).

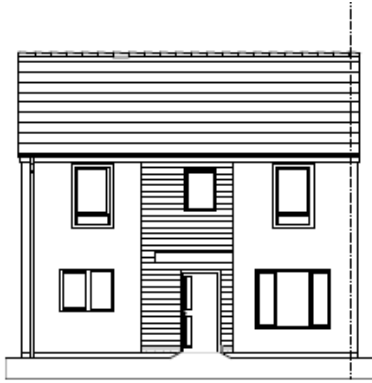
Report by Planning Manager

Plans

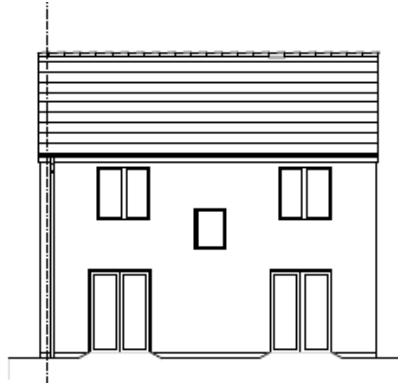




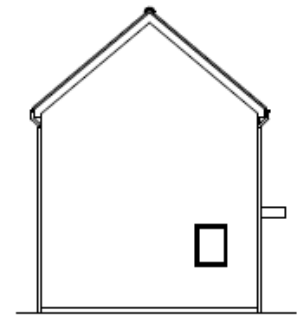




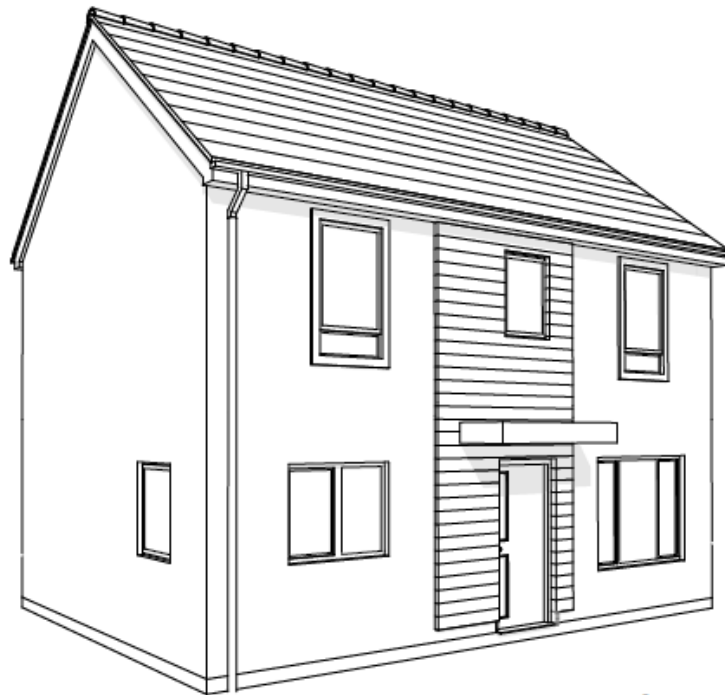
Front elevation
1:100



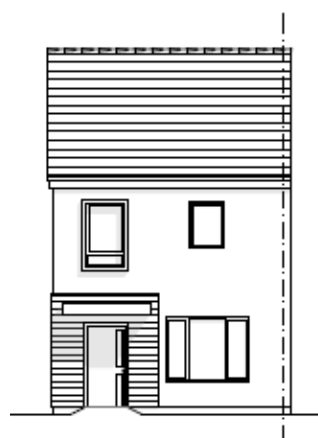
Rear elevation
1:100



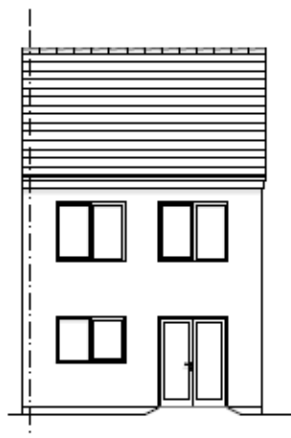
Side elevation
1:100



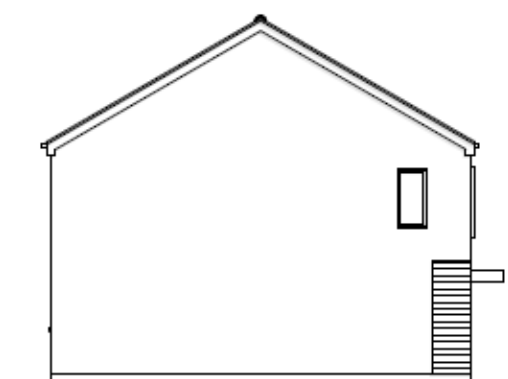
Perspective view



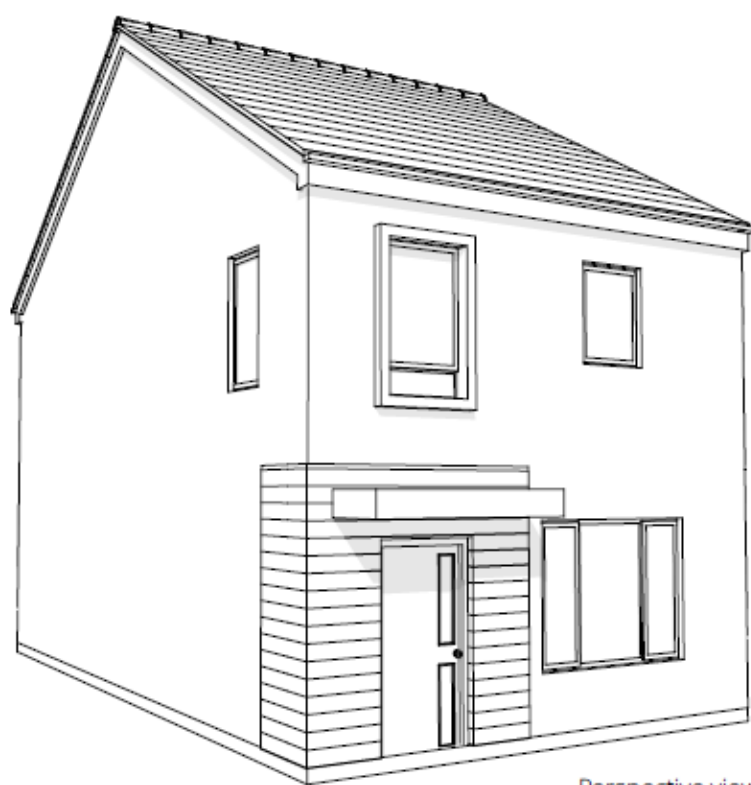
Front elevation
1:100



Rear elevation
1:100



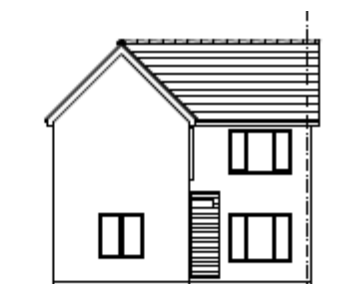
Side elevation
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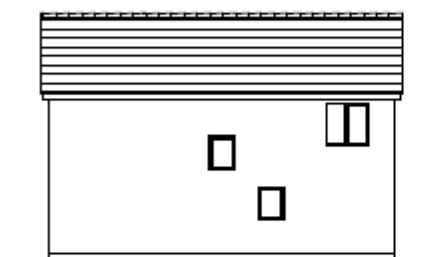
Perspective view



Front elevation
1:100



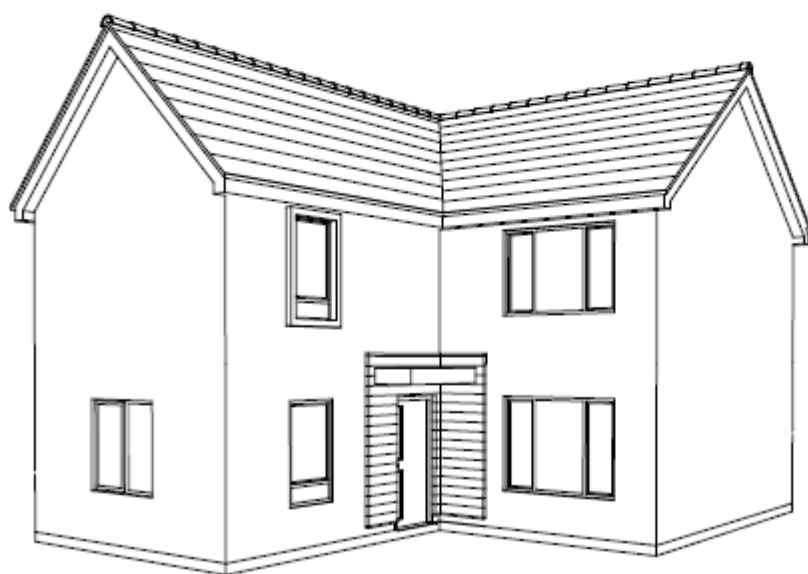
Front elevation
1:100



Side elevation
1:100



Rear elevation
1:100



Perspective view

Site Photos

Views of Road Junctions



The views above and below are in either direction from the junction of Burton Road with Queen Elizabeth Road





The views above and below are in either direction from the junction of Garfield Close with Queen Elizabeth Road





The views above and below are towards the end of Garfield Close





The views above and below are of Clarendon Gardens which will be used as a pedestrian/cycle access/egress for the development





The views above and below are in either direction from the junction of Woodburn Close with Queen Elizabeth Road





The view above is towards the end of Woodburn Close



The views above and below are in either direction from the junction of Queen Elizabeth Road with Riseholme Road



Typical Views within the Site or at its Perimeter



The Woodland Walk



View along the northern edge of the tree belt towards the west of the site



Above and below are views from the tree belt towards the three-storey buildings at the southern edge of the site



Above and below are typical views along the southern perimeter of the site



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Consultation Responses Received by City of Lincoln Council

Lincoln Civic Trust

Our original objections remain valid as the revised plans do not address any of our reason for objecting to the proposals.

Our original Objections:

1. Only access to existing estate and the current development is from Queen Elizabeth Road via the junctions with Burton Road and Riseholme Road. The formation and control of both of these junctions needs to be reviewed and radically changed if the congestion on Queen Elizabeth Road is not to become grid locked.
2. Only two accesses from the existing estate to the new development being Woodburn Close and Garfield Close which in themselves were never designed as busy access roads and are not fit for purpose.
3. No provision for any increase in service provision e.g. Education Medical Retail etc.
4. The noise and pollution levels particularly for the houses nearest to the A46 Lincoln By Pass.

OBJECTION to the revised plans:

1. Do not address any of our previous objections.
2. We question the decision to remove the separate internal footpaths given that the internal roads are to be of "shared" usage.
3. We question the revised noise assessment provided particularly the comments that the Acoustic Screens have little or no value. It would appear that our European neighbours have a totally different view of screens alongside busy arteries in that many of the major roads in particularly Germany and the Netherlands have some very substantial structures to deflect the noise and I would suggest mitigate the pollution levels.

FURTHER COMMENT: We noted the welcome submission by the Lincolnshire County Council Highways Department. We totally agree with their assessment of the number of parking spaces required by each property and suggest that their recommendations should be adopted for all residential developments. We agree with their recommendation concerning the road width particularly when shared access is proposed, but we do not agree with their assessment that the increase in volume created by the development will not create major congestion on Queen Elizabeth Road and create gridlock at the junctions with Burton Road and Riseholme Road as vehicles try to turn out of Queen Elizabeth Road and attempt to make right turns into the estate.

Lincolnshire County Council (as Education Authority)

Thank you for your notification of 05 December 2017, concerning the proposed development at the above site. I have now had the opportunity to consider the impact on the local schools reasonably accessible from the development. Please see below overview in relation to the impact, and details for primary, secondary and sixth-form that follow.

Overview

Please see below table in relation to the number of places required and available in local schools from/for the proposed development:

Type	Children produced by scheme	Sufficient places available 2019/20 (Y/N/Partial)	Places to be mitigated	Contribution sought
Primary	49	N	49	£552,545
Secondary	46	N	46	CIL
Sixth-form	9	N	9	CIL 165,848
			Total	£563,822

Please note, where an application is outline a formulaic approach will be taken in a section 106 agreement, this may result in a higher contribution if a high proportion of large houses are built. This would be finalised at the reserved matters stage. All section 106 agreements should include indexation using the Tender Price Index of the Royal Town Planning Institute Building Cost Information Services (RICS BCIS TPI).

The above contributions would be spent on the following:

Type	Amount	Scheme
Primary	£552,545	To be confirmed following discussions with local primary schools
Secondary	£781,608	N/A – CIL (amount indicated would have been due under s.106)
Sixth-form	£165,848	N/A – CIL (amount indicated would have been due under s.106)

I can confirm that the County Council will ensure that no more than five s.106 agreements are signed towards a specific piece of infrastructure, as detailed above (where known), which will be specific within the s.106 agreement.

Detail

The below table indicates the number of pupils generated by the proposed development. This is on the basis of research by Lincolnshire Research Observatory utilised to calculate Pupil Production Ratio (PPR) multiplied by the number of homes proposed.

House Type (if known)	No of Properties	PPR Primary	Primary Pupils	PPR Secondary	Secondary Pupils	PPR Sixth Form	Sixth Form Pupils
2 Bedroom	124	0.09	11.16	0.09	11.16	0.018	2.232
3 Bedroom	144	0.17	24.48	0.17	24.48	0.034	4.896
4+ Bedroom	41	0.33	13.53	0.27	11.07	0.054	2.214
Unknown		0.2		0.19		0.038	
Total (rounded down)	309	-	49	-	46	-	9

Numbers above are total development less 1 bedroom dwellings that generate no children and 2 bedroom dwellings to be demolished

Capacity is assessed using the County Council's projected capacity levels at 2020/21, this is the point when it is reasonable to presume that the development would be complete or well on the way.

Type	Local School/School Planning Area	Pupils generated	Sufficient places available 2019/20 (Y/N/Partial)	Places to be mitigated
Primary	Lincoln North Primary planning area	49	N	49
Secondary	Lincoln North Secondary planning area	46	N	46
Sixth-form	Lincoln North Secondary planning area	9	N	9

As the development would result in a direct impact on local schools, a contribution is therefore requested to mitigate the impact of the development at local level. This is a recognisable and legitimate means of addressing an impact on infrastructure, accords with the NPPF (2012) and fully complies with CIL regulations; we feel it is necessary, directly related, and fairly and reasonably related in scale and kind to the development proposed in this application.

The level of contribution sought in this case is in line with the below table.

Type	Places to be mitigated	Contribution per place*	Sub-total	Local multiplier**	Total contribution requested
Primary	49	£12,257	£600,593	0.92	£552,545
Secondary	46	£18,469	£849,574	0.92	£781,608
Sixth-form	9	£20,030	£180,270	0.92	£165,848
Total	-	-	£1,630,437	-	£1,500,001

*current cost multiplier per pupil place based on National Cost Survey

**to reduce cost and to reflect Lincolnshire's lower than average build cost compared to national average

We would suggest the s.106 monies are paid at the halfway point in the development to allow timely investment by the County Council whilst not adversely affecting the developer's viability.

Please note the County Council retains the statutory duty to ensure sufficiency of school places and this includes capital funding provision of sufficient places at maintained schools, academies and free schools. We would invest the funding at the most appropriate local school(s) regardless of their status, but ensure the s.106 funding is used only to add capacity as this is the only purpose for which it is requested.

I look forward to hearing from you, thank you for your notification of the application and thank City of Lincoln Council for your continued cooperation and support.

Yours sincerely

Simon Challis
Strategic Development Officer
Corporate Property Service

Lincolnshire County Council (as Highway and Lead Local Flood Authority)

Environment & Economy
Lancaster House
36 Orchard Street
Lincoln
LN1 1XX
Tel: (01522) 782070
E-Mail: Highwayssupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2017/1393/RG3

With reference to this application dated 23 November 2017 relating to the following proposed development:

Address or location

Land adjacent to A46 Ring Road and north of Queen Elizabeth Road , Lincoln

Date application referred:
5 December 2017

Type of application: Outline/Full/RM/
FUL

Description of development

Erection of 325no. dwellinghouses, including 8no. flats, facilitated by the demolition of existing flats known as Garfield View and Woodburn View. Associated infrastructure and external works including new footpath link to Clarendon Gardens, the provision of new parking bays to Garfield Close and Woodburn Close and hard and soft landscaping and children's play area

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Requests that the Local Planning Authority request the applicants to provide additional information as set out below.

ADDITIONAL INFORMATION REQUIRED

Highways and Lead Local Flood Authority response dated 28th February 2018:

Travel Plan comments:

The Travel Plan has been submitted to support a planning application for c314 dwellings off Queen Elizabeth Road, Lincoln. Overall the travel plan as submitted contains the relevant information that would be expected in a residential travel plan; however, there are some areas where additional detail or clarity is required. The measures suggested are mainly promotion based. The DfT publication 'Making Travel Plans Work: Lessons from UK Case Studies' states that Travel Plans containing only marketing and promotional works will not achieve modal shift. Most relatively basic travel plans can achieve a 3-5% shift, providing the package of measures included is robust. Such measures should include promotions, car

sharing and walking and cycling measures. However, in order to achieve a higher target and ensure the delivery of mode shift, the development requires additional measures such as discounts on public transport. In order to achieve mode shift, the measures suggested will need to be supported by incentives for change.

Ref	Travel Plan Reference	LCC Comment
1	Policy Background	The primary reader of a Travel Plan is the Travel Plan Co-ordinator [guidance] and/or resident [information] informing the developer's commitment and intentions to reduce single occupancy car travel and promote sustainable transport to and from the site. Whilst planning policy is important, a summary would be sufficient within the TP.
2	Existing Site and Highway Conditions	<p>Informative with useful photographs, however, whilst the isochrones maps give an overview of distance it is difficult for the reader to interpret the relationship between where they are and want to be. A table showing local facilities that the resident may use along with distance and walking/cycling times would be more informative and easier for the reader to assimilate. Either an appendix showing the cycle map or a link in this section would make the information provided on National Cycle Routes more understandable.</p> <p>Public Transport – Bus Services: The Lincoln Central bus station, now behind Sincil Street, is now within the Lincoln Transport Hub with direct connectivity to the Railway Station.</p> <p>Rail Services – the Lincoln Railway Station has secure cycle parking facilities for up to 196 bicycles.</p>
3	Development Proposals	It is a positive to note that secure cycle parking will be provided at each dwelling at the site.
4	Travel Plan Measures	<p>Travel Plan Co-ordinator will also be responsible for undertaking and/or commissioning surveys. The TPC should be appointed in sufficient time to implement pre-occupation measures, such as the Welcome Pack. Prior to circulation of the Welcome packs it is requested that the developer liaise with the County Council to ensure that the contents are comprehensive and up to date.</p> <p>As the post requires the appropriate authority to make decisions and approve budget spend, the TPC should not be a member of the sales staff unless supported by a more senior member of staff. The TPC needs to be in post for the duration of the build out and 1 year post final occupation – this could be for five years or may be longer depending on built out rate. The establishment of a Local Residents Group is a positive measure.</p> <p>As mentioned above, the proposed measures listed are mainly promotion based. There should be at the least, for a site of this size, a firm commitment to the provision of a discounted travel/complimentary bus taster ticket.</p>

		<p>To increase incentive choice a voucher within the pack for either a bus ticket, hi-visibility wear /D Locks or cycle safety checks for cyclists or pedometers for walkers could be used. Residents can then opt for the incentive most beneficial to them.</p> <p>Other measures that could be used to incentivise change in travel habits and maintain the profile of the Travel Plan could include sustainable travel workshops on or around the survey and Dr. Bike sessions alongside the workshops.</p>
5	Targets, Monitoring and Review	<p>LCC Guidance for the Development of Travel Plans states, 6.2.6 "In the majority of cases it is expected that targets will be challenging but achievable. All new site developments should commit to a minimum reduction of 10% in single occupancy car journeys in rural areas and 15% in urban areas. A lower target must be justified." The targets suggested provide for a reduction of 9% (A 6% modal shift). Some assurance that this will be discussed with LCC following the first survey should be provided.</p> <p>The first survey should be completed following 25% occupation and annually thereafter for five years or one year post final occupation whichever is greater. It is further recommended that a %response rate be set to ensure sufficient responses are received to provide meaningful data. If insufficient data is collated then there should be a commitment to undertaking a multi modal travel survey to provide accurate progress of the travel plan the following year alongside the survey.</p> <p>The County Council uses an online travel plan management tool to assist in monitoring of travel plans. This tool provides online survey facilities. Where residents do not have email access, paper versions of the survey can be used. It is recommended that the developer consider encouraging the TPC to use the system. Further information can be found at https://starsfor.org/.</p> <p>Some indication as to the types of remedial action will be taken should the target not be achieved during the monitoring period should also be included. This might be providing additional bus tickets, further promotional work, the TPC undertaking personal travel planning work with residents etc..</p>
6	Budget	<p>Additional details are required in respect of providing an indicative budget that developer considers will be required/committed to fund the various elements of the Travel Plan, such as TPC appointment, surveys, vouchers, promotion of events, travel packs etc... This budget does not need to be part of the S106 but informs</p>

		<p>the LCC that the developer has considered and allowed for sufficient monies to commit to the plan.</p> <p>In order to successfully monitor a travel plan, LCC require a monitoring fee, secured through a Section 106 agreement to be paid by the developer to cover the office time and overheads required to co-ordinate and complete the monitoring programme over the lifetime of the Travel Plan, normally £1000 per annum for 5 years.</p>
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Recommendation: That the comment provided be considered and a revised Travel Plan submitted for approval.

Layout:

The raised speed tables should be removed from the design as vertical deflections are not accepted by the Highways Authority.

Shared surface carriageways should be a minimum of 5.5 metres in width, with a 1.8 metre service strip.

Where no footways are provided a 0.5 metre strip has been shown on the drawings adjacent to the carriageway. This strip is not required.

The Highways and Lead Local Flood Authority advises that a minimum of 2 parking spaces is provided for all dwellings with 3 or less bedrooms and 3 spaces for dwellings with 4 or more bedrooms. A garage should only be considered as a parking space where it is of adequate size to accommodate both a car and storage. Although parking provision across the site a few plots fall short of the requirement.

The minimum widths of roads that are proposed for adoption should be 5 metres (where footways are to be provided). Isolated narrowings as a speed restraint measure may be accepted.

Drainage:

The drainage system that has been proposed for this development is mainly piped with an infiltrating attenuation basin. The highway does not consider this to be a SUD's system as the entire site is positively drained until the point at which it reaches the attenuation pond.

Based on the information that has been provided to date the site has low ground water levels and is likely to have good infiltration. The use of SUD's drainage features, such as permeable paving and swales should therefore be considered as a means of draining the site. A piped system should only be adopted as a last resort when a SUD's scheme cannot be achieved. The Highways Authority does not believe that this is the case in this instance.

The drainage strategy should therefore be amended and a SUD's drainage system proposed.

Greenfield run off rates should be provided along with calculations to demonstrate that the existing greenfield rates have not been exceeded.

Transport Assessment:

Overall the impact of the development on existing traffic flows in the area will not lead to a situation which could be considered to be a severe impact on the surrounding highway network.

The key junctions that suffer from significant congestion in the peak periods are Riseholme Road roundabout and the Burton Road/Yarborough Road roundabout. There will be some impact from the additional traffic generated by this proposal in the peak periods which will add to the significant queuing that is observed along Burton Road in particular. However, much of this queuing is caused by the delays encountered further down Yarborough Road at the Long Leys Road signalised junction, West Parade signalised junction and Newland signalised junction. Some of this will be mitigated in the short term following the opening of the LEB due to the redistribution of traffic (approx. £500,000 is being secured through CIL towards the LEB).

The additional traffic from this development expected to pass through the Riseholme Road roundabout is less than 1% of the existing flows. Again this will be mitigated in the short term following the opening of the LEB.

Section 106 contribution requests:

A Section 106 contribution of £5000 is required for the monitoring of the Travel Plan.

Highways and Lead Local Flood Authority response dated 8th May 2018:

The amended drainage strategy that has been submitted in response the drainage comments in my response above dated 28th February 2018, does not address the issues raised. No acceptable evidence has been provided to demonstrate that a SUD's drainage solution can not be found for the whole site. Permeable paving and swales (which may be lined to address high ground water issue or used for conveyance where infiltration is poor) could be used as an effective way of draining the site.

As previously stated a piped system should only be accepted as a means of draining the site when all other SUD's options have been found to be unsuitable. The report states that the western side of the site has good infiltration and therefore a SUD's solution should be easily achievable.

Evidence has been provided to demonstrate that the proposed pipe to pond system will effectively drain the site, but as the majority of the system is piped, I do not consider it to be a full SUD's system.

All other comments made in the Highway and Lead Local Flood Authority response dated 28th February (detailed above) still apply.

Case Officer:

Date: 08/05/18

Liz Burnley

for Warren Peppard

Flood Risk & Development Manager

Lincolnshire Police (Crime Prevention Advisor)

Lincolnshire Police have no objections to this application.

It is fully appreciated that this application is only seeking to establish the principle of development and that the finer detail of design will be submitted at a later date. However, the applicant needs to consider the following advice when drawing up a more detailed proposal:

Overall the permeability of this design within the context of a generally low crime area is acceptable however any pathways that are not necessary should be avoided, equally pathways to the rear of any property should likewise be avoided. Where pathways are deemed essential they should be at least 3m, devoid of potential hiding places, well overlooked with good natural surveillance, straight with no hidden curves, well-lit and maintained.

Building Regulations (October 1st 2015) provides that for the first time all new homes will be included within Approved Document Q: Security – Dwellings (ADQ).

Approved document Q applies to all new dwellings including those resulting from change of use, such as commercial premises, warehouse and barns undergoing conversions into dwellings. It also applies within Conservation Areas.

This will include doors at the entrance to dwellings, including all doors to flats or apartments, communal doors to multi-occupancy developments and garage doors where there is a direct access to the premises. Where bespoke timber doors are proposed, there is a technical specification in Appendix B of the document that must be met.

Windows: in respect of ground floor, basement and other easily accessible locations.

I have studied the online plans (Design and Access Statement) and would request that you consider the following points that if adhered to would help reduce the opportunity for crime and increase the safety and sustainability of the development.

- 1) Properties should be orientated to face streets and public areas. Windows of routinely occupied rooms (e.g. lounge/living room/kitchen) should be positioned to provide effective overlooking of the frontage and contribute to natural surveillance.
- 2) To encourage greater use and reduce the fear of crime, all footpath networks should be directly overlooked by housing.
- 3) It is important that space is clearly defined to delineate public, semi-private or private space. Avoid space which is unassigned. All space should become the clear responsibility of someone.

When it is unclear whether space is public or private it is difficult to determine what is acceptable behaviour. Uncertainty of ownership can reduce

responsibility and increase the likelihood of crime and anti-social behaviour going unchallenged.

- 4) Front gardens on all through roads should effectively be defined using low walls, railings or planting in order to effectively create defensible space to the housing. Boundaries between each property should be clearly defined.
- 5) Gable ends of properties should not directly adjoin public areas, as this often leads to nuisance for the residents. The provision of good gable end surveillance by way of windows can mitigate against this risk.
- 6) The profile of the entrance into the site (entrance gate and raised carriageway crossing) displays a presence which will give the impression that the facility and its grounds are 'private'.
- 7) Front doors should be located where they can be seen from the street and neighbouring houses. They must not be located in deep recesses or behind other obstacles that would provide cover for criminal activity.
- 8) The rear gardens of properties, where possible, should lock into each other, reducing the potential for an offender to gain access to the back of properties without being witnessed.
- 9) Effective division between front and rear gardens needs to be provided e.g., 1.8m high fencing and lockable gates.
- 10) It is strongly advised that if there are any rear access (service) alleyways incorporated, they must be gated at their entrances. The gates must not be easy to climb over or easily removed from their hinges and they must have a key operated lock. Alleyways giving access to rear gardens are frequently exploited by burglars and can become a focus for anti-social behaviour.
- 11) If properties have driveways to the side of the dwellings themselves, windows should be incorporated in the side elevation at landing or first floor level to allow residents to overlook their vehicles.
- 12) Appropriate street lighting should be provided around the site. Good lighting will deter intruders and reduce the fear of crime. Lighting should comply with British Standard 5489 -2013.
- 13) The proposed tree planting should be developed in tandem with any street lighting in order to avoid the scenario of tree canopies obscuring lighting. Street lighting should be provided which complies with British Standard 5489– 2013.
- 14) One of the most effective ways to prevent property crime is to make the property itself as secure as possible. With this in mind, it is highly recommended that all vulnerable ground floor windows and doors be security- tested to comply with British Standard PAS.24:2012 (Secured by Design Standards). *See note above.*

- 15) I would recommend that each dwelling be provided with lighting to illuminate all external doors, car parking and garage areas. Ideally lighting should be switched using a photo electric cell (dusk to dawn) with a manual override.
- 16) In respect of landscaping, it is important that in vulnerable locations, such as entrances, parking areas and footpaths, low planting should not exceed 1000mm in height, and tree canopies should not fall lower than 2m from the ground. This is in order to allow people to see their surroundings better, make a rational choice of routes and eliminate hiding places.
- 17) Car parking should ideally be located within curtilage of the property at the front. If properties have driveways to the side of the dwellings themselves, windows should be incorporated in the side elevation at landing or first floor level to allow residents to overlook their own vehicles. Consideration towards provision of suitable parking for visitors should be an element of this proposal as a failure to consider such a facility may lead to inconsiderate and inappropriate parking within the development.

Recent research conducted by Professor Rachael Armitage (Huddersfield University) on behalf of the Design Council/CABE, Home Office and Secured by Design, has clearly shown that rear parking courts are vulnerable to crime. They have higher levels of vehicle crime and criminal damage than other types of parking, and also facilitate offender access to the rear of properties. Residents do not tend to use their allocated spaces within these courts, preferring to park on street, regardless of whether the street was designed for on street parking.

Other research states: "The recent fashion for placing parking spaces behind buildings has led to many schemes around the country being blighted by cars parked to the front of the house where there is no space designed to accommodate them. It is an inefficient use of land, as a large proportion is used for roads and parking areas; the internal routes result in reduced garden sizes; there is loss of security and privacy to the rear of the home; and, with parking to the rear of the house, residents may be less likely to use their front doors with a consequent loss of activity in the street.

Communal Areas (Public Open Spaces) Play Areas (if to be considered)

Where a communal recreational area may be been created development it is important that adequate mechanisms and resources are in place to ensure its satisfactory future management. If a play-area (toddler) is to be included this should be so designed that it can be secured at night-time to help prevent any misuse such as damage or graffiti. The type and nature of any fencing should be specific to this area but should be to a minimum of 1200mm which can often discourage casual entry.

I would recommend that 'air lock' style access points (at least two) with grated flooring to prevent animal access and the resultant fouling that may occur. Such gating systems will also reduce the risk of younger children exiting onto the adjacent roadways.

One of the attributes of safe, sustainable places is 'Ownership' - places that promote a sense of ownership, respect, territorial responsibility and community. Ownership is

particularly relevant to this outline planning application in respect of social inclusion, particularly when you consider that as much as 40% of the housing proposed could be low cost/affordable homes. It is important to highlight that low cost/affordable housing must be pepper-potted throughout the development rather than concentrated in one area or isolated from the general housing market. Social inclusion promotes a sense of ownership, respect and territorial responsibility within the community.

“Rear servicing can undermine the security of dwellings by allowing strangers access to the rear of dwellings.”

The defensive character of the development should not be compromised through **excessive** permeability caused by the inclusion of too many, or unnecessary segregated footpaths which allows the criminal legitimate access to the rear or side boundaries of dwellings or footpath links. Better places to Live by Design, the Companion Guide to PPG3, Secure by Design, Manual for Streets all promote pedestrian, cycle and vehicular permeability through residential areas by designing roads on a network basis rather than using footpaths.

Should outline planning consent be granted, I would ask that consideration be given by the Authority to require full details of what crime prevention measures are to be incorporated into this development. These should be required as part of Reserved Matters. These measures should ideally take into account the contents of this report.

I would direct and recommend that the current *Police CPI New Homes 2016* is referred to as a source document in the planning and design process.

Further guides are available on www.securedbydesign.com that include SBD Commercial 2015 V2, SBD New Schools 2014 & Sheltered Accommodation. I would ask that you direct architects and developers to these documents and ensure their reference in the various Design & Access statements. Equally please do not hesitate involving this office in and on any further consultations.

Crime prevention advice is given free without the intention of creating a contract. Neither the Home Office nor the Police Service takes any legal responsibility for the advice given. However, if the advice is implemented it will reduce the opportunity for crimes to be committed.

Lincolnshire Wildlife Trust

We would like to make the comment below with specific reference to:

- Extended Phase 1 Habitat Survey
- Reptile Presence / Absence Survey
- Tree Survey
- Design and Access Statement
- Supporting Planning Statement
- Proposed Site Layout
- Site Layout plans 1-3

We accept the results and conclusions of the Extended Phase 1 Habitat Survey and that of the Reptile Presence / Absence Survey. We accept that the majority of land on the site is of low ecological value and that there are no constraints to the potential development of this site due to the potential presence of reptiles. Nevertheless, we would like to highlight the following elements.

Northern boundary hedgerow:

We strongly support that the northern boundary hedgerow is a significant landscape feature and has the potential to contribute to a landscape scale corridor on Lincoln's periphery. We would advocate that this hedgerow is:

- enhanced with native (only) shrub species to increase its diversity;
- augmented with well-spaced native (only) standard trees;
- managed sympathetically for wildlife (see recommendation for LEMP below);
- buffered on its south side by several metres of rough grassland which is managed to provide a sanctuary zone for wildlife (see recommendation for LEMP below).

Central linear woodland:

We strongly support the maximum, preferential retention of especially mature and native trees within the broadleaved, linear woodland which runs east-west across the centre of the site. We strongly support the following measures recommended by the Extended Phase 1 Habitat Survey and proposed by the Design and Access Statement:

- Full protection measures should be implemented to safeguard Root Protection Areas (RPAs) of all trees to be retained during the construction phase.
- Bat surveys should be undertaken to assess both the suitability of trees for bat roosting and the suitability of both the woodland strip and connected hedgerows for bat commuting and foraging.
- The central woodland pedestrian path should not be lit and the lighting of adjacent residential development should be designed to minimise light spill into the woodland in order to maintain a 'dark' corridor. This will provide a significant benefit for bats and invertebrates.
- If trees and underlying vegetation are to be disturbed during the bird nesting season (Feb-Aug incl.), all work must be preceded shortly beforehand by a survey from a qualified and experienced ecologist in order to identify any mitigation measures that should be undertaken.
- We note that on page 39 of the Design and Access Statement "Objectives for design of landscape and open spaces" include a woodland belt of "semi-natural character enhanced by naturalistic planting." We suggest that this would enhance the public's enjoyment and appreciation of this landscape element but would insist that if any planting were undertaken it must include only native species of UK or ideally regional provenance. Whereas non-native, ornamental planting may be more appropriate in much closer proximity to housing, we believe strongly that this woodland feature represents the most significant opportunity on site for the public to engage frequently with a naturalistic environment. Consequently we would insist that any seed/plant supplier should be able to guarantee local/national provenance and/or have Flora Locale accreditation (www.floralocale.org).

We would recommend sowing a suitable hedgerow seed mix (20% grass and 80% wildflower seed) in bordering strips north and south of the woodland belt which received no more than partial shade. For reference, examples include:

- Boston Seeds BS7M: <https://www.bostonseeds.com/library/BS7M-Hedgerow-and-Light-Shade-Wildflower-Meadow-Seed-Mixture.pdf>
- Naturescape N9: <https://www.naturescape.co.uk/product/n9-hedgerow-meadow-mixture/>
- Emorsgate EH1: <https://wildseed.co.uk/mixtures/view/12>

Our recommendation for sowing under the full shade of the woodland strip would be a suitable woodland seed mix. For reference, examples include:

- Boston Seeds BS8P: <https://www.bostonseeds.com/library/BS8P-Hedgerow-and-Heavy-Shade-Wildflower-Seed-Mixture.pdf>
- Naturescape NV10F: <https://www.naturescape.co.uk/product/nv10f-value-woodland-mix-flowersonly/>
- Emorsgate EW1F: <https://wildseed.co.uk/mixtures/view/33>

SUDS infiltration basins:

We note that the FRA and Drainage Strategy states that there is "a requirement for SUDS drainage." We also note that the Design and Access Statement mentions that further soak away tests will be carried out at the proposed locations of the SUDS infiltration basins. Given the permeable limestone geology beneath the site, the infiltration basins cannot be assumed to behave like ponds until infiltration tests show otherwise. They may remain dry except only in periods of heavy rainfall when standing water may only persist for hours or a few days. For this reason, we would recommend that the appropriate seed mix for the SUDS basins would be a neutral or wetland meadow mix for higher infiltration rates and only an aquatic/marginal pond mix for low infiltration rates where the basin is likely to hold standing water for more than half of the year. For reference, examples of appropriate wetland meadow mixes include:

- Boston Seeds BS6M: <https://www.bostonseeds.com/library/BS6M-Wetland-and-Pond-Edge-Wildflower-Meadow-Seed-Mixture.pdf>
- Naturescape N7: <https://www.naturescape.co.uk/product/n7-wetland-meadow-mixture/>
- Emorsgate EM8: <https://wildseed.co.uk/mixtures/view/9>

Open, dry grassland areas:

We strongly recommend that the potential for the creation of lowland calcareous (limestone) grassland should not be overlooked. This habitat is a Habitat of Principal Importance listed under Section 41 of the NERC Act 2006 and is a conservation priority within the Lincolnshire Biodiversity Action Plan. Lincoln sits on a north-south corridor of limestone geology which is well suited to support this threatened habitat. Any creation of limestone grassland at this location will enhance the network of sites in the local area and therefore be directly relevant to CLLP policy LP21.

The following locations on site offer the potential of lowland limestone grassland creation due to full sun, dry soil conditions and underlying limestone geology:

- outside of and around the SUDS basins
- to the south of the woodland belt beyond the semi-shade fringe
- in the areas labelled 'Landscape Feature' in Site Layout Plans 1-3

For reference, examples of appropriate calcareous meadow seed mixes include:

- Boston Seeds BS2M: <https://www.bostonseeds.com/products/2/Wildflowers-Seed/11/Wildflower-Seed-Mixtures-20/#product659>
- Naturescape NV13F: <https://www.naturescape.co.uk/product/nv13-value-chalk-limestone-soilsmeadow-mixture/>
- Emorsgate EM6: <https://wildseed.co.uk/mixtures/view/7>

We would strongly advise against amenity turf laying wherever possible. This will significantly diminish the biodiversity potential of the site if this is done extensively on public open space. We would also hope that garden plots could receive a flowering lawn or general purpose grassland mix seeding.

If the decision to use turf is mainly due to concern for the rapidity and evenness of establishment, then a general purpose seed mix (e.g. Emorsgate EM1 / Naturescape's N1 or N14) could be sown with an addition of @ 20kg/ha of Westerwolds Rye-grass (*Lolium multiflorum*) to act as a 'nurse grass'. The nurse grass will enable rapid bare ground cover but will eventually give way to the accompanying species. For advice on this technique see <https://wildseed.co.uk/page/annualwesterwolds-ryegrass-as-temporary-nurse-cover>.

If no alternative to turf is acceptable we would advocate that certain areas be considered for turf richer in native species. 'Species-rich Lawn Turf' offered by wildflowerturf.co.uk is an example of a less expensive option of wildflower turf. See examples of wildflower turf here:

- <http://www.wildflowerturf.co.uk/Products/species-rich-lawn-turf.aspx>
- <http://www.allturf.co.uk/wp-content/uploads/2013/12/wildturf.pdf>
- <https://www.tillersturf.co.uk/flora-meadow-wildflower-turf>

Turf and seeded grass biodiversity will benefit from lower fertility, so again we would advise against imported topsoil, compost or pre-seeding fertiliser.

Ornamental planting:

We would suggest that ornamental planting is best kept in close proximity to dwellings. We would encourage reference to the following resources. RHS 'Perfect for Pollinators':

<https://www.rhs.org.uk/science/pdf/conservation-and-biodiversity/wildlife/rhs-perfect-for-pollinatorsgarden-plants.pdf>

https://www.rhs.org.uk/science/pdf/conservation-andbiodiversity/wildlife/rhs_perfectforpollinators_plantlist-jan15.pdf

We would expect a development of this size to involve a Construction Environmental Management Plan (CEMP), a detailed planting plan and a Landscape and Ecology Management Plan (LEMP) to be submitted to the LPA that would follow the appropriate mitigation hierarchy (avoid, mitigate, compensate) and include detail on the following points:

CEMP

- appropriate surveys and mitigation for breeding birds and for bats
- potential risks of construction site lighting, noise, dust, chemical pollution and mitigation plan
- proposed plans to mitigate damage to trees and hedgerows through RPA protection during construction

- soil conservation: prevention of weed development in stored topsoil, utilisation of topsoil on site in garden plots; no imported topsoil for POS grassland establishment; establishment of grassland POS over mineral soil or minimum topsoil as far as levels and costs permit; no pre-seeding fertilisers or organic mulches for grassland POS
- minimal/no turf laying

LEMP

- Mowing of open spaces including dry areas around SUDS basins and SUDS basins
 - Management during the establishment year: Maintain a short turf (c.5-8cm which should be cut throughout the year). This will encourage development of the perennial species and minimise weed development. We would insist that all cuttings should be removed every time.
 - Ongoing management after the first year: Except for thoroughfares, we would insist that management of grassy areas be 1-2 cuts per year only with cuttings always collected. Cutting can be twice per year if grass growth is tall and rapid. Ideally a gap should be left between cuts of at least 10-12 weeks and ideally cuts from June to mid-July should be avoided. Cuts in May and September can therefore work well.
- Hedgerow cutting
 - We advise that a buffer of 1-2m of grassland at hedgerow bases be left unmown as a wildlife refuge but saplings that emerge within this zone be cut down every few years to prevent scrub encroachment over grassland.
 - In order to maximise the biodiversity benefit from hedgerows, they should only be cut on alternate sides every 3 years in January-February according to research cited in this report:
<https://www.woodlandtrust.org.uk/mediafile/100270134/j-wt-161014-wood-wise-summer-2014.pdf?cb=011b6965191740cdbf92535777d1a81b>. Cutting only one side at a time and less frequently will allow production of flowers and fruit for wildlife while minimising bird disturbance.

We would hope that the following enhancements would also be incorporated into the development:

- Bird boxes for garden birds and nesting cups for swallows/swifts house martins o Bat boxes/bat tiles/bat bricks nearest to hedgerows and woodland
- Hedgehog-permeable fencing
- Small, stacked dead wood piles within woodland or under hedgerows for wildlife refuge

NHS England

Over Page...

<p>Impact of new development on GP practice</p>	<p>The above development is proposing 325 dwellings which, based on the average of 2.2 people per dwelling for the Lincolnshire County Council area, would result in an increase in patient population of 715.</p> <p>The calculations below show the likely impact of this new population in terms of number of additional consultation time required by clinicians. This is based on the Department of Health calculation in HBN11-01: Facilities for Primary and Community Care Services.</p> <p>Consulting room GP</p> <table border="1" data-bbox="392 551 1308 898"> <tr> <td>Proposed population</td><td>715</td></tr> <tr> <td>Access rate</td><td>5260 per 1000 patients</td></tr> <tr> <td>Anticipated annual contacts</td><td>$0.715 \times 5260 = 3761$</td></tr> <tr> <td>Assume 100% patient use of room</td><td>3761</td></tr> <tr> <td>Assume surgery open 50 weeks per year</td><td>$3761/50 = 75.2$</td></tr> <tr> <td>Appointment duration</td><td>15 mins</td></tr> <tr> <td>Patient appointment time per week</td><td>$75.2 \times 15/60 = 18.8$ hrs per week</td></tr> </table> <p>Treatment room Practice Nurse</p> <table border="1" data-bbox="392 1003 1308 1350"> <tr> <td>Proposed population</td><td>715</td></tr> <tr> <td>Access rate</td><td>5260 per 1000 patients</td></tr> <tr> <td>Anticipated annual contacts</td><td>$0.715 \times 5260 = 3761$</td></tr> <tr> <td>Assume 20% patient use of room</td><td>$3761 \times 20\% = 752.2$</td></tr> <tr> <td>Assume surgery open 50 weeks per year</td><td>$752.2/50 = 15.044$</td></tr> <tr> <td>Appointment duration</td><td>20 mins</td></tr> <tr> <td>Patient appointment time per week</td><td>$15.044 \times 20/60 = 5$ hrs per week</td></tr> </table> <p>Therefore an increase in population of 715 in the City of Lincoln Council area will place extra pressure on existing provisions, for example- extra appointments requires additional consulting hours (as demonstrated in the calculations above.) This in turn impacts on premises, with extra consulting/treatment room requirements.</p>	Proposed population	715	Access rate	5260 per 1000 patients	Anticipated annual contacts	$0.715 \times 5260 = 3761$	Assume 100% patient use of room	3761	Assume surgery open 50 weeks per year	$3761/50 = 75.2$	Appointment duration	15 mins	Patient appointment time per week	$75.2 \times 15/60 = 18.8$ hrs per week	Proposed population	715	Access rate	5260 per 1000 patients	Anticipated annual contacts	$0.715 \times 5260 = 3761$	Assume 20% patient use of room	$3761 \times 20\% = 752.2$	Assume surgery open 50 weeks per year	$752.2/50 = 15.044$	Appointment duration	20 mins	Patient appointment time per week	$15.044 \times 20/60 = 5$ hrs per week
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<p>GP practice(s) most likely to be affected by the housing development</p>	<p>Due to the fact that patients can choose to register at any practice that covers the area of the development, and there are no waiting lists for patients, all practices that provide care for the region that the development falls within are obliged to take on patients, regardless of capacity.</p> <p>As such, the following practices may be affected by the development:</p> <ul style="list-style-type: none"> • Lindum Medical Practice • Abbey Medical Practice • Minster Medical Practice • Cliff House Medical Practice • Glebe Park Surgery • Brayford Medical Practice • The Witham Practice • University Health Centre 																												

Issues to be addressed to ensure the development is acceptable	<p>This development would put additional demands on the existing GP services for the area and additional infrastructure would be required to meet the increased demands.</p> <p>To mitigate this, the s106 funding from this development would be split between Minster Medical Practice and Glebe Park Surgery.</p> <p>An occupancy survey which was carried out on behalf of the CCG for Glebe Park Surgery has shown that they are currently working at capacity. The practice list size has increased substantially over recent years and they are anticipating an increase in growth of approximately 5% for the current year. This development would place further capacity pressures on the practice.</p> <p>The practice have proposed using the s106 funding to add a single storey extension to their existing premises and reconfiguring their existing building to allow the repurposing of office space. This would create extra clinical and admin space and enable the practice to be able to better serve the population and allow them to offer services that they are not currently in a position to be able to.</p> <p>Minster Medical Practice is also working at clinical capacity and would need to carry out improvement works to increase the number of consulting rooms in order to be able to provide services for the patients arising from this development.</p> <p>The practice currently has a large ground-floor room used for storing medical records. They have proposed that the s106 funding is used to convert and furnish their loft space to store their patients' medical records and reconfigure the existing storage room into clinical rooms for nurse use. This would free up two existing rooms for GP/other health care professional use. These changes would allow them to provide superior nursing facilities, which would benefit their existing patients as well as increasing capacity for new patients moving into the area.</p> <p>This of course would be subject to a full business case and approval by NHS England, with any proposed expenditure taking place when the s106 funds are released by the developer as per the agreement and within the agreed timescale for expenditure of the funds.</p>																																															
Fairly and reasonably related in scale and kind to the development.	<table><tr><td></td><td>Average list size per GP</td><td>Required m2</td><td>£ per m2</td><td>Total cost</td><td>£per person</td></tr><tr><td>GP team</td><td>1,800</td><td>170</td><td>1,500</td><td>£255,000</td><td>142</td></tr><tr><td>GP furnishings</td><td>1,800</td><td></td><td></td><td>£20,000</td><td>12</td></tr><tr><td colspan="5"></td><td>154</td></tr><tr><td colspan="5">Contingency requirements @ 20%</td><td>31</td></tr><tr><td colspan="5">Total per resident</td><td>185</td></tr><tr><td colspan="5">Total per dwelling (resident x 2.2)</td><td>407</td></tr></table> <p>The table above shows the contribution formula which is based on the needs of a Primary Care Health Team and associated administration support. By applying average national list sizes to these groups and identifying the required area and furnishings, a total cost of £185 per patient is determined. This figure is multiplied by 2.2 (the average number of persons per dwelling for the City of Lincoln Council) to provide a funding per dwelling of £407.</p>							Average list size per GP	Required m2	£ per m2	Total cost	£per person	GP team	1,800	170	1,500	£255,000	142	GP furnishings	1,800			£20,000	12						154	Contingency requirements @ 20%					31	Total per resident					185	Total per dwelling (resident x 2.2)					407
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Financial Contribution requested	The contribution requested for this development is £132,275.00 (£407 x 325 dwellings.)
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Vicky Allen
Primary Care Support Medical & Pharmacy
February 2018

Natural England

Natural England has no comments to make on this application.

Natural England has not assessed this application for impacts on protected species.

Natural England has published [Standing Advice](#) which you can use to assess impacts on protected species or you may wish to consult your own ecology services for advice.

Natural England and the Forestry Commission have also published standing advice on [ancient woodland and veteran trees](#) which you can use to assess any impacts on ancient woodland.

The lack of comment from Natural England does not imply that there are no impacts on the natural environment, but only that the application is not likely to result in significant impacts on statutory designated nature conservation sites or landscapes. It is for the local planning authority to determine whether or not this application is consistent with national and local policies on the natural environment. Other bodies and individuals may be able to provide information and advice on the environmental value of this site and the impacts of the proposal to assist the decision making process. We advise LPAs to obtain specialist ecological or other environmental advice when determining the environmental impacts of development.

We recommend referring to our SSSI Impact Risk Zones (available on [Magic](#) and as a downloadable [dataset](#)) prior to consultation with Natural England. Further guidance on when to consult Natural England on planning and development proposals is available on gov.uk at <https://www.gov.uk/guidance/local-planning-authorities-get-environmental-advice>

Neighbours

Mr N. Williams (375 Burton Road)

Objects in relation to the following:

Proposed footpath to Burton Road

I object to the footpath which the plans propose will run East to West, to the north of our property, from the newly extended Garfield Close to Burton Road. Access for pedestrians is already provided in this regard by the well-used footpath running through the line of trees only a few metres to the north, which the plans propose to retain. Construction of the proposed new path runs contrary to the advice of Lincolnshire Police, in that it is "not necessary," would provide easy access to the rear of my property, would not be directly overlooked and, as such, "should be avoided" as a security concern. Such pathways, Lincolnshire Police recommend, "should be well lit" which would result in persistent light pollution of my property and be to the detriment of local wildlife in the nearby trees (see advice of Lincolnshire Wildlife Trust).

Furthermore, there is regularly evidence of anti-social behaviour on the existing unlit pathway through the trees (discarded beer cans and drug paraphernalia) which the new pathway would bring that bit closer to my property and family. The proposed pathway runs close to the private drive which provides access to my property, which is protected by only a two-bar wooden fence, and which would offer a tempting shortcut to those heading South onto Burton Road.

Instead of the footpath, I support the recommendation of the Lincolnshire Wildlife Trust, that this area be retained and developed as an area of lowland limestone grassland.

SuDS pond

Without further detail, access to and around the proposed SuDS pond poses the same security concerns as the footpath, in that it may offer easy, unlit access to the rear of my property. I would also echo the concerns of other residents with regard to the safety provision around the pond.

Finally, although I understand the aim of the SuDS pond is to contain the affects of heavy rain, I should like to point out that, in the ten years or so of living here, there have been no instances of flooding on my property. Should this pond change that situation, I will hold you responsible.

Noise

I see there has been a noise/acoustic assessment of the proposed finished development, but nothing with regard to noise during construction. Currently our property benefits from very little daytime noise, which is of great advantage to me, as a night shift worker who must rest and sleep in daylight hours, both before and after work. It is difficult to see how that peaceful environment can be maintained while you knock down two properties, build a road and dig a SuDS pond just a few feet from my

bedroom window. If, as a consequence, I am unfit for work, the costs to my employer might easily run into many thousands of pounds. Therefore, should the noise become detrimental to sound rest, I shall have to seek alternative accommodation, the cost of which, I believe, should be born by you or the developer.

Mr. M. Stafford (38 Clarendon Gardens)

There is already a considerable build up of traffic at the Queen Elizabeth Road / Riseholme Road and the Queen Elizabeth Road / Burton Road junctions at peak times.

The current road system is simply not capable of coping with the increase in traffic that 325 new dwellings would cause at these junctions at peak times.

For a development of this size to be even remotely feasible, the aforementioned junctions would need to be traffic light controlled, with dedicated turn filter lanes.

I do not believe that there is sufficient available land on either of these roads, at the required points, to build the new junctions required to take the increase in vehicular traffic. It is for this reason that I object to the currently proposed development.

Mr. C. R. C. Greenwold (2 Edendale View, Via Email)

Re the letter about the development of the land adjacent to A46 ring road and North of Queen Elizabeth road Lincoln. I am writing to say I have no objection to this development but seek one assurance that the security fencing behind my property at 2 Edendale view stays in place all through the development and afterwards.

Yours sincerely

C.R.C.Greenwold

Mrs. J. Farrar (1 Riverton View)

It seems that every small available green space is being built on, on the Ermine Estate. Whilst there is always the need for housing, surely some consideration should be given to natural surroundings and appearance of the area for those that are living here? 325 dwellings will have an enormous effect on the Estate and the open land, trees etc. behind Queen Elizabeth Road be destroyed.

Mr. M. Foster (35 Garfield Close)

Two letters setting out objections to the application, dated 09 and 11 December 2017:

Concerning your development on land adjacent to A46 ring road and the demolition of Garfield View Flats. On a number of occasions I have wrote into your department and visited City Hall and spoke to Emma. Regarding my pre-planning application to land adjoining Garfield View I have also been to Sudbrooke Community Centre on the 18th September to discuss my concerns and because of the increase in traffic the possibility of having off road parking to 35 Garfield Close. There seem to be no problem with my pre-planning application, as I was told it could be implemented into the development, however I am none the wiser on what is happening. As this development does affect mine, as well as the demolition of Garfield View that adjoins my land and well with other issues I have not been consulted with. I have no choice but to object to your development.

Concerning your letter dated 11th December. I wrote to your department, on a number of occasions and spoke to Emma as well but to no avail. Regarding your development as it has an impact on my land as Garfield View shares the boundary with my land. I have not been consulted and no permission sought from me in any way. At the meeting or open day on the 18th September I inform your staff of my own development and with the increase of traffic, off-road parking for 35 Garfield Close, that your development will bring. The ponds are a concern for children safety. I understand the line of trees have a tree protection order or preservation on them. Your development is cramming houses onto the site that will affect the quality of life, for those that live in them and nothing is being done, to reduce the traffic noise from the A46 by-pass. I object to your development on the grounds it is not suitable or environmentally friendly.

Consultation Responses Received by West Lindsey District Council **(From Consultees Covering Matters not Sent to CoLC)**

Anglian Water

ASSETS

Section 1 – Assets Affected

- 1.1 There are assets owned by Anglian Water or those subject to an adoption agreement within or close to the development boundary that may affect the layout of the site. Anglian Water would ask that the following text be included within your Notice should permission be granted.

"Anglian Water has assets close to or crossing this site or there are assets subject to an adoption agreement. Therefore the site layout should take this into account and accommodate those assets within either prospectively adoptable highways or public open space. If this is not practicable then the sewers will need to be diverted at the developers cost under Section 185 of the Water Industry Act 1991. or, in the case of apparatus under an adoption agreement, liaise with the owners of the apparatus. It should be noted that the diversion works should normally be completed before development can commence."

- 1.2 The development site is within 15 metres of a sewage pumping station. This asset requires access for maintenance and will have sewerage infrastructure leading to it. For practical reasons therefore it cannot be easily relocated.

Anglian Water consider that dwellings located within 15 metres of the pumping station would place them at risk of nuisance in the form of noise, odour or the general disruption from maintenance work caused by the normal operation of the pumping station.

The site layout should take this into account and accommodate this infrastructure type through a necessary cordon sanitaire, through public space or highway infrastructure to ensure that no development within 15 metres from the boundary of a sewage pumping station if the development is potentially sensitive to noise or other disturbance or to ensure future amenity issues are not created.

WASTEWATER SERVICES

Section 2 – Wastewater Treatment

- 2.1 The foul drainage from this development is in the catchment of Canwick Water Recycling Centre that will have available capacity for these flows

Section 3 – Foul Sewerage Network

- 3.1 Development will lead to an unacceptable risk of flooding downstream. A drainage strategy will need to be prepared in consultation with Anglian Water to determine mitigation measures.

We request a condition requiring the drainage strategy covering the issue(s) to be agreed.

Section 4 – Surface Water Disposal

- 4.1 The preferred method of surface water disposal would be to a sustainable drainage system (SuDS) with connection to sewer seen as the last option.

Building Regulations (part H) on Drainage and Waste Disposal for England includes a surface water drainage hierarchy, with infiltration on site as the preferred disposal option, followed by discharge to watercourse and then connection to a sewer.

- 4.2 The surface water strategy/flood risk assessment submitted with the planning application relevant to Anglian Water is unacceptable. We would therefore recommend that the applicant needs to consult with Anglian Water and the Lead Local Flood Authority (LLFA).

We request a condition requiring a drainage strategy covering the issue(s) to be agreed.

Section 5 – Trade Effluent

- 5.1 The planning application includes employment/commercial use. To discharge trade effluent from trade premises to a public sewer vested in Anglian Water requires our consent. It is an offence under section 118 of the Water Industry Act 1991 to discharge trade effluent to sewer without consent. Anglian Water would ask that the following text be included within your Notice should permission be granted.

"An application to discharge trade effluent must be made to Anglian Water and must have been obtained before any discharge of trade effluent can be made to the public sewer.

Anglian Water recommends that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of such facilities could result in pollution of the local watercourse and may constitute an offence.

Anglian Water also recommends the installation of a properly maintained fat traps on all catering establishments. Failure to do so may result in this and other properties suffering blocked drains, sewage flooding and consequential environmental and amenity impact and may also constitute an offence under section 111 of the Water Industry Act 1991."

Section 6 – Suggested Planning Conditions

Anglian Water would therefore recommend the following planning condition if the Local Planning Authority is mindful to grant planning approval.

Foul Sewerage Network (Section 3)

CONDITION

No development shall commence until a foul water strategy has been submitted to and approved in writing by the Local Planning Authority. No dwellings shall be occupied until the works have been carried out in accordance with the foul water strategy so approved unless otherwise approved in writing by the Local Planning Authority.

REASON

To prevent environmental and amenity problems arising from flooding.

Surface Water Disposal (Section 4)

CONDITION

No drainage works shall commence until a surface water management strategy has been submitted to and approved in writing by the Local Planning Authority. No hard-standing areas to be constructed until the works have been carried out in accordance with the surface water strategy so approved unless otherwise agreed in writing by the Local Planning Authority.

REASON

To prevent environmental and amenity problems arising from flooding.

Burton Parish Council

Although not formally consulted on this application the application appears to suggest this is in Burton Parish.

We have looked at the application and would wish to raise very particular concerns over the traffic increase that this may have on already congested roads. We would wish Highways to carefully consider if this application is suitable from a Highway perspective and whether the current road network has sufficient capacity for this increase. The Parish of Burton would be affected by significant increase in road usage.

If Highways do believe it is acceptable we would ask that planning conditions be included to minimise any impact and to ameliorate the position.

Please could you include the above comment on this application.

Caroline Emerson
Burton-by-Lincoln Parish Clerk

Lincolnshire Fire and Rescue

LINCOLNSHIRE FIRE AND RESCUE

Chief Fire Officer: Nick Borrill



My Ref: YS
Your Ref: 137101

Lincolnshire Fire and Rescue
Grantham Fire Station
Harlaxton Road
Grantham
NG31 7SG

Head of Planning Services
West Lindsey District Council
Guildhall
Marshall's Yard
Gainsborough
DN21 2NA

Sent by email to planning.customer.care@west-lindsey.gov.uk

20 December 2017

Dear Sir

**TOWN AND COUNTRY PLANNING ACT 1990
PLANNING CONSULTATION - LAND NORTH OF QUEEN ELIZABETH ROAD,
LINCOLN**

I refer to the planning application reference 137101. The Fire Authority object to the application on the grounds of inadequate access and water supplies.

It is the opinion of the Fire Authority that in order to remove the objection the following measures are required

1. Access to buildings for fire appliances and fire fighters must meet with the requirements specified in Building Regulations 2010 Part B5. These requirements may be satisfied with other equivalent standards relating to access for fire-fighting, in which case those standards should be quoted in correspondence.

Lincolnshire Fire and Rescue also requires a minimum carrying capacity for access routes and hard standing for pumping appliances of 18 tonnes, not 12.5 tonnes as detailed in the Building Regulations 2010 part B5.

2. Lincolnshire Fire and Rescue recommends that fire hydrants be installed within this development at the developer's expense. However, it is not possible, at this time, to determine the number of fire hydrants required for fire-fighting purposes. The requirement will be determined at the water planning stage when site plans have been submitted by the water companies.

Should you wish to discuss this matter, please do not hesitate to contact me on the telephone number below.



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Riseholme Parish Council

Riseholme Parish Council have looked at this application due to the proximity of the site to the Parish.

They have concerns due to the scale of the development.

The concerns relate to the increase in traffic on an already congested road network. As you will know the Riseholme roundabout is backlogged for a considerable period of time during the day. This is exacerbated by show ground events which are increasing.

We would wish that these concerns be referred to the Highways department and for them to consider very carefully how this will impact on the current network.

If it is felt to be an appropriate development we would look to be appropriately conditioned to ameliorate the potential problems.

Neighbours

Mr. & Mrs. Buckthorpe (29 Garfield Close)

I am bring to your attention the following facts. The woodland on the development site has a tree preservation order on it. The woodland is also a habitat for a species of bats, that are protected. I will also be informing other organizations who have an interest in protecting our environment and wildlife. I will remind you of your duty as well as responsibility as Chief Operating Officer, your legal requirements in accordance with the legislation, to protect our environment and wildlife and in no way compromise the legislation already in force.

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Application Number:	2018/0626/RM
Site Address:	Grantham Street Car Park, Grantham Street, Lincoln
Target Date:	10th August 2018
Agent Name:	Globe Consultants Ltd
Applicant Name:	Jackson & Jackson Developments Limited
Proposal:	Submission of Reserved Matters including access, appearance, landscaping, layout and scale for the erection of a six-storey building incorporating student accommodation and car parking as required by outline planning permission 2017/0721/OUT

Background - Site Location and Description

Site Location

The application site is situated at the south-western corner of the junction of Grantham Street with Flaxengate but also adjoins Swan Street to the west. In general terms, it is situated to the east of the High Street.

The application site is irregular but roughly square in shape and is currently utilised as a surface car park. It is adjacent to commercial uses within The Terrace, to the north, and with frontages to Clasketgate, to the south; there are residential apartments to the west and northwest on Swan Street and Grantham Street respectively; and student accommodation to the east in the Danesgate House building. Meanwhile, the County Council occupy a building across Flaxengate and the theatre is to the southwest.

The northern and southern boundaries are currently delineated by trees.

Description of Development

The application is for the submission of all Matters that were reserved following the grant of outline planning permission (ref: 2017/0721/OUT). These include access, appearance, landscaping, layout and scale for the erection of a six-storey building incorporating student accommodation and car parking.

The car parking would be at lower ground floor level and accessed from Flaxengate. It would accommodate 26 spaces, two of which would be DDA compliant. The student accommodation would be for ten clusters of bedrooms with a shared living room and kitchen over five floors, i.e. two clusters to each floor (clusters of 7 and 14 at ground floor; clusters of 9 and 16 on floors 1-3; and clusters of 9 and 13 on the fourth floor). There will be four DDA compliant rooms for students.

Site History

As alluded to in the description of development, only the principle of development was established by the outline planning permission parking along with one or a mixture of the following uses:

- Residential Units (C3);
- Student Accommodation;
- Offices (B1); and / or
- Hotel Accommodation (C1).

Notwithstanding this, maximum scale parameters were also set for the proposed development within which the reserved matters would be brought forward. These include the maximum footprint (including the resultant floor areas) and height of the building. The indicative floor plans submitted showed a student accommodation use.

Site History

Reference:	Description	Status	Decision Date:
2017/0721/OUT	Erection of a building to include 2 levels of car parking and 4 storeys above to provide either residential units (use class C3); and/or student accommodation; and/or office (use class B1); and/or Hotel (use class C1) (Outline) (REVISED PLANS AND DESCRIPTION)	Granted Conditionally	5th April 2018

Case Officer Site Visit

Undertaken on 11th May 2018.

Policies Referred to

- Policy LP1 A Presumption in Favour of Sustainable Development
- Policy LP2 The Spatial Strategy and Settlement Hierarchy
- Policy LP3 Level and Distribution of Growth
- Policy LP5 Delivering Prosperity and Jobs
- Policy LP6 Retail and Town Centres in Central Lincolnshire
- Policy LP7 A Sustainable Visitor Economy
- Policy LP9 Health and Wellbeing
- Policy LP11 Affordable Housing
- Policy LP12 Infrastructure to Support Growth
- Policy LP13 Accessibility and Transport
- Policy LP14 Managing Water Resources and Flood Risk
- Policy LP16 Development on Land Affected by Contamination
- Policy LP17 Landscape, Townscape and Views
- Policy LP21 Biodiversity and Geodiversity
- Policy LP24 Creation of New Open Space, Sports and Recreation Facilities
- Policy LP25 The Historic Environment
- Policy LP26 Design and Amenity
- Policy LP29 Protecting Lincoln's Setting and Character
- Policy LP31 Lincoln's Economy
- Policy LP33 Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area
- Policy LP36 Access and Movement within the Lincoln Area
- Policy LP37 Sub-division and multi-occupation of dwellings within Lincoln

- National Planning Policy Framework

Issues

As alluded to above, the principle of the erection of development encompassing student accommodation was agreed through the approval of outline planning permission for the site. In light of this, it would not be possible to revisit the principle of this form of development. Furthermore, the maximum scale parameters of the building were also agreed at this point, including the overall footprint and height of the development. However, the details of the access, appearance of the building, landscaping and layout are for consideration. The main issues referred to below therefore need to be considered as part of this application:

1. The Impact of the Design of the Proposals;
2. The Implications of the Proposals upon Amenity;
3. Sustainable Access and Highway Safety;
4. Archaeology;
5. Matters Controlled by Planning Conditions on the Outline Planning Permission;
6. Other Matters; and
7. The Planning Balance.

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Monks Road Neighbourhood Initiative	Response Awaited
Lincolnshire Police	Comments
Lincoln Civic Trust	Object
Education Planning Manager, Lincolnshire County Council	No request for S106 due to the development being for student accommodation
Historic England	No Comments
Highways & Planning	Response Awaited

Public Consultation Responses

Name	Address
Richard Tibenham	Greenlite Energy Assessors
Mr Thomas Foley	7 Swan Street Lincoln LN2 1LF

Consideration

1) The Impact of the Design of the Proposals

a) *Relevant Planning Policy*

So far as this issue is concerned, as alluded to above, the proposals must achieve sustainable development and it is the social dimension of sustainability that relates to design. Moreover, Paragraph 7 of the National Planning Policy Framework (the Framework) requires the creation of high quality built environment. In addition, the policy principles outlined in Paragraphs 17, 58, 60, 61 and 64 of the Framework also apply. Moreover, the Framework states that good design is a key aspect of sustainable development and is indivisible from good planning. Design is to contribute positively to making places better for people (para. 56). To accomplish this development is to establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live and responding to local character and history (para. 58). It is also proper to seek to promote or reinforce local distinctiveness (para. 60).

At the local level, the Council, in partnership with English Heritage, have undertaken the Lincoln Townscape Appraisal (the LTA), which has resulted in the systematic identification of 105 separate “character areas” within the City. The application site lies within the High Street Character Area. Policy LP29 refers to the LTA and requires that developments should “protect the dominance and approach views of Lincoln Cathedral, Lincoln Castle and uphill Lincoln on the skyline”. This policy is supported by Policy LP17, which is relevant to the protection of views and suggests that:-

“All development proposals should take account of views in to, out of and within development areas: schemes should be designed (through considerate development, layout and design) to preserve or enhance key local views and vistas, and create new public views where possible. Particular consideration should be given to views of significant buildings and views within landscapes which are more sensitive to change due to their open, exposed nature and extensive intervisibility from various viewpoints.”

Policy LP26 refers to design in wider terms and requires that “all development, including extensions and alterations to existing buildings, must achieve high quality sustainable design that contributes positively to local character, landscape and townscape, and supports diversity, equality and access for all.” The policy includes 12 detailed and diverse principles which should be assessed. This policy is supported by Policy LP5 which also refers to the impact on the character and appearance of the area; and Policy LP31, which refers to the protection and enhancement of the character of the city.

In terms of the wider impacts upon built heritage, Policy LP29 also requires that “proposals within, adjoining or affecting the setting of the 11 Conservation Areas and 3 historic parks and gardens within the built up area of Lincoln, should preserve and enhance their special character, setting, appearance and respecting their special historic and architectural

context”; and “protect, conserve and, where appropriate, enhance heritage assets, key landmarks and their settings and their contribution to local distinctiveness and sense of place, including through sensitive development and environmental improvements”.

Meanwhile, conservation is enshrined in the Core Planning Principles of the Framework (Paragraph 17) as planning is expected to “conserve heritage assets in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of this and future generations”. In addition, Section 12 of the Framework also refers to the impacts of development upon designated heritage assets and is supported by Policy LP25 also applies as it specifically refers to the impacts of developments upon these assets. In terms of conservation areas, the policy requires that development should either enhance or reinforce features that contribute positively to the area’s character, appearance and setting. Meanwhile, proposals also need to have regard to the setting of other designated assets, including listed buildings.

b) Assessment of the Implications of the Proposals

i) The Site Context and Submissions

The application site is contained within the Cathedral and City Centre Conservation Area City Centre and is considered to have the potential to affect views into and within the Area. As such, the visual implications of the proposals for the site are key to the assimilation of development into its context and the creation of high quality built environment.

In terms of the scale of the development, officers have worked with the applicant on both applications in order to ensure that this would not have a detrimental impact upon townscape. Crucially, Members should note that the height of the building has reduced even further from that submitted as part of the outline planning application. This is referred to in further detail below.

ii) The Impact of the Development in its Wider Context

As the height of the building has been reduced again from the maximum parameters referred to in the outline planning permission, the impact upon views within and into the Conservation Area would be less than it was previously with that application. Moreover, the impacts would not be harmful in the context of the townscape as the building would either be sat against the backdrop of existing townscape or would be no higher than that townscape.

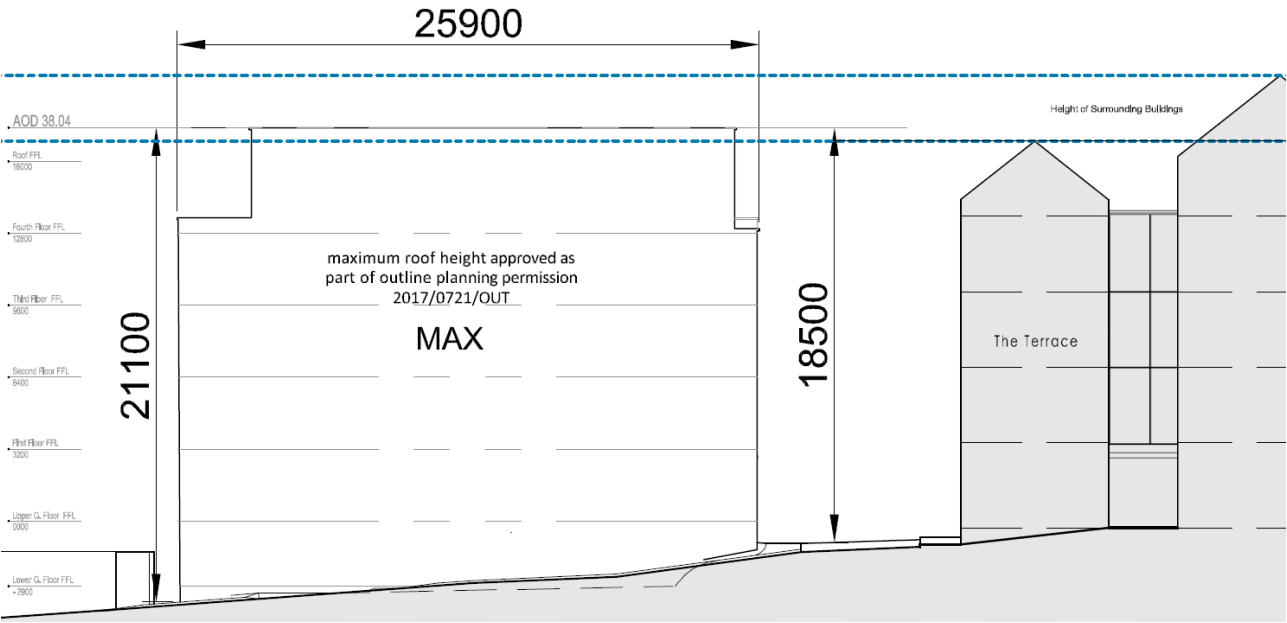
iii) Implications of the Development in its Immediate Context

As Members will appreciate, the details shown in the outline planning application were only indicative and it is the current application that shapes the appearance of the building and how it would assimilate within its context.

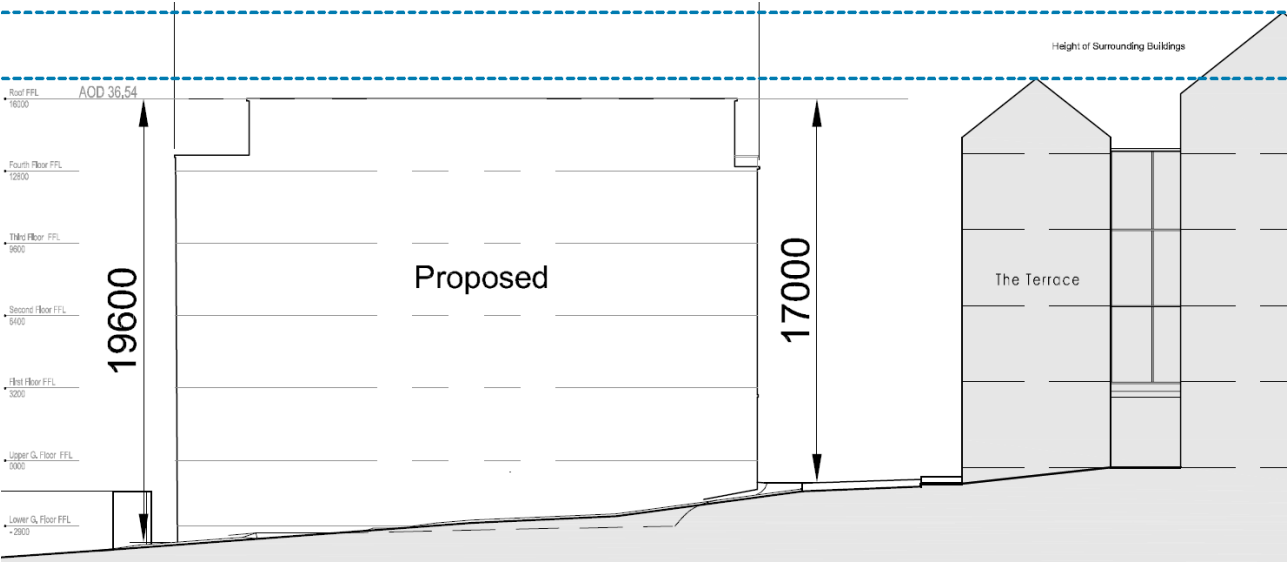
It is noted that the Civic Trust have raised concerns regarding the content of the application submission including the materials to be used in the construction of the development. However, the plans submitted for the development are clear and include specific references to the type of materials proposed for the building. Nonetheless, the report details the design in further detail below.

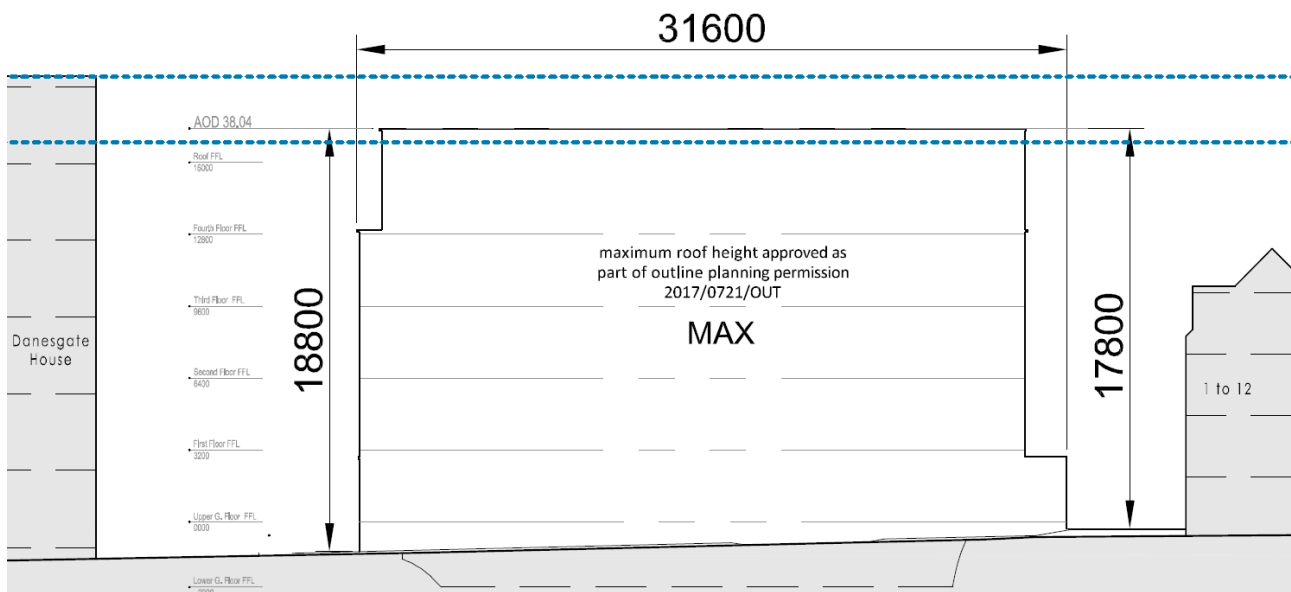
However, prior to this, it is first important to refer to the scale of the building. As alluded to in the report for the outline planning application, it is clear that there are tall buildings within

the vicinity of the application site, particularly to the northern side of Grantham Street, in the form of The Terrace, and to the opposite side of Flaxengate, with Danesgate House. Even so, that application was carefully considered in order to ensure that the scale of the proposed building would be appropriate in its context.

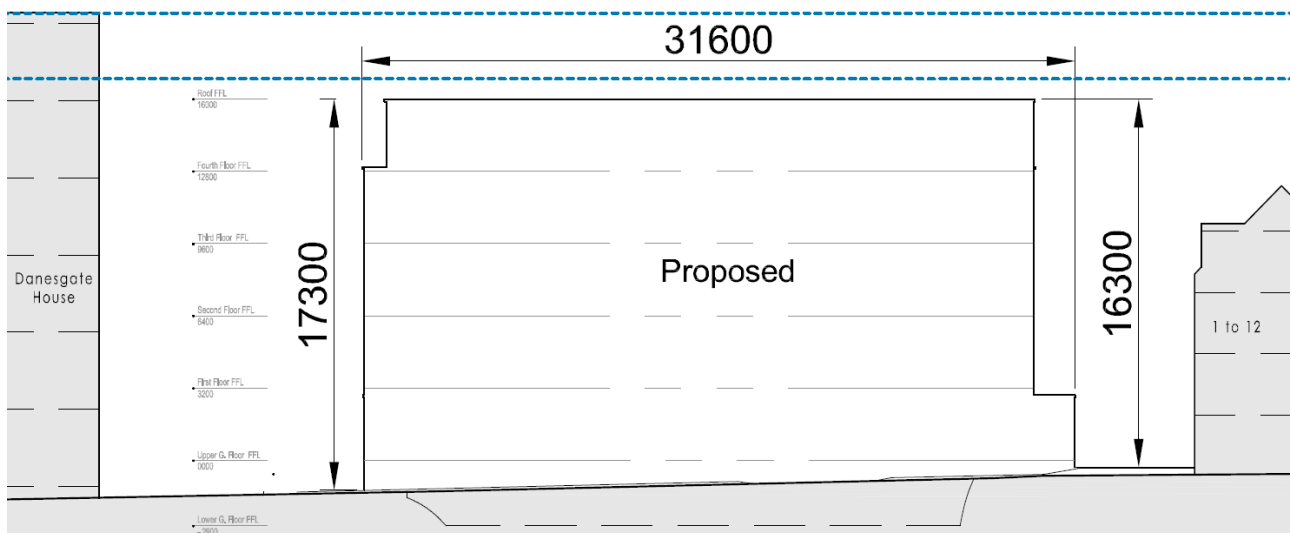


The images above and below illustrate the cross section north/south of the development with The Terrace to the right and neighbouring site on Danesgate to the left. Moreover, the above image shows the maximum parameters of development agreed through the outline planning application, whereas the image below presents the scale of the proposed building. Members will note that there is a 1.5m reduction in height from the agreed height.





The aforementioned 1.5m reduction in height from the outline parameters is shown in the images above and below for this east/west cross section, with Swan Street to the right and Flaxengate to the left of the images.



In addition to the reduction in the height of the building, the form and appearance of the building, including the overall proposed materials palette, have also been utilised carefully to ensure that the building would have a suitable modern appearance that would enhance the character and appearance of the Conservation Area.

Due to the intended end use of the building, the principal elements of the façades of the building will include a consistent layout of window apertures set within a wider buff brick frame. The choice of this lighter material would align with other similar bricks used elsewhere in the locality but would be more appropriate in the context of the architecture of the building. The frame would also share some similarities with the recladding undertaken of Danesgate House.

Within the window apertures themselves, the solid recessed elements will be of a different buff brick to distinguish them from the main façade. Meanwhile, the frames for the windows will be metal (bronze in colour).

All buildings have components that are more functional, such as stairwells but where possible the applicant has sought to provide an architectural solution. In particular, at the corner of Swan Street and Grantham Street, the stairwell and lift shafts have been incorporated in a contrasting reconstituted stone clad feature which wraps around from above the entrance on Swan Street to Grantham Street. This would incorporate a recessed section at the corner of solid bronze coloured aluminium framed curtain walling.

Meanwhile, in terms of the Flaxengate frontage, a key change from the original indicative outline scheme is that the car park would only be served by one access/egress from Flaxengate, which would be positioned centrally to the elevation to provide balance to that façade and framed in the same stone as the feature corner to Swan Street/Grantham Street. Furthermore, the reduction in the number of the entrances to the car park would ensure that there is minimal disruption to the façades of the building and would assist in drawing activity from Grantham Street around to the entrance on Flaxengate.

In a similar manner, the plant and machinery to serve the building is proposed to be accommodated internally on the ground floor behind the bin store and covered by louvered screens. As such, there would not be visual implications associated with these. Nonetheless, there was also a planning condition included on the outline planning permission to ensure that there would be suitable mitigation measures in place to protect residents from noise.

As with the outline planning application, the top floor of the building remains recessed back along the majority of the Flaxengate and Grantham Street frontages, in order to further reduce the perceived scale of the building. The external materials of these sections, as well as most of the west facing elevation of the fourth floor, will be bronze standing seam cladding.

Finally, whilst there would be limited opportunities within the site for landscaping, the applicant has included details of landscaping for the roof of the internal courtyard of the building and in certain locations at the site perimeter, as shown below:



c) *Summary in Relation to this Issue*

Officers are satisfied that the proposals would result in a tall modern building that will assimilate well within its context, particularly the façade treatments, which address the street edge in a similar way to other buildings within the vicinity and are sufficiently broken down into component parts in order to reduce the perceived mass of the building. Moreover, the proposals offer the opportunity to regenerate this important area with a high quality development that is suitably scaled to appropriately integrate with the surrounding townscape that contributes to the valued character and appearance of the conservation area. Therefore, it is considered that the proposal satisfies the duty contained within section 72 (1) of the Planning (Listed Buildings and Conservation Areas Act) 1990 'In the exercise, with respect to any buildings or other land in a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area'. Furthermore, the proposal is in accordance with the guidance contained within paragraph 137 of the Framework which advises that Local Planning Authorities should look for new development within a Conservation Area and within the setting of heritage assets to reveal or better enhance significance.

2) Implications of the Proposals upon Amenity

a) *Relevant Planning Policy*

In terms of national policy, the NPPF suggests that development that results in poor design and/or impacts upon the quality of peoples' lives would not amount to sustainable development. Consequently, the implications of both are key to the consideration of the acceptability of the principle of development within a given site. Moreover, the Framework (Paragraph 9) sees "seeking positive improvements in the quality of the built, natural and historic environment, as well as in people's quality of life" as being important to the delivery of sustainable development, through "replacing poor design with better design" and "improving the conditions in which people live" amongst others. Furthermore, the core principles of the Framework (Paragraph 17) indicate that "planning should...always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings".

Policy LP26 of the Plan deals with design and amenity. The latter refers to the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy and suggests that these must not be unduly harmed by, or as a result of, the development. There are nine specific criteria which must be considered. The policy is in line with the policy principles outlined in Paragraphs 17, 59 and 123 of the NPPF. Indeed, Paragraph 123 of the Framework suggests that "decisions should aim to...avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development".

b) *Assessment of the Implications of the Proposals*

i) *Impacts of the Scale of the Building*

As alluded to in the first section above, contrary to the assertion made by a resident, Members will note that the height of the proposed building is actually presented as being lower than the maximum parameters agreed through the outline planning application. Moreover, the height has been reduced by 1.5metres, which means that there would be an

improvement as a result of the actual scale of the building in relation to its impacts upon outlook, overshadowing and loss of light.

Furthermore, the perception of the scale of development has also been attended to through the architectural design of the building. In particular, as alluded to in the assessment of the design of the building, its façades would not be stark or oppressive. Conversely, officers would advise Members that the combination of the movement back and forth across the plane of the façades of the building, as well as the use of lighter coloured high quality materials, would provide sufficient visual interest to the façades to ensure that they would not be harmful to the outlook of the neighboring properties, and thereby the amenities that they would expect to enjoy.

ii) Overlooking and Loss of Privacy

Members will note from the outline planning application process that a number of concerns were raised from the occupants of neighbouring properties regarding the potential for overlooking or a direct loss of privacy resulting from the development.

However, following the grant of that permission, officers have worked with the applicant in order to ensure that the design of the west façade, adjacent to Swan Street, would ensure that habitable spaces are served by windows angled south, i.e. the occupants of these rooms would only be able to look south down the street and not toward the residential apartments opposite. Furthermore, whilst the southern aspect of the building seeks to maximise the opportunities for a greater degree of glazing, particularly to shared spaces, the glazing will also be obscured to lower elements to reduce the possibilities for loss of privacy in either direction in this elevation to and from other development.

In terms of other relationships to the north between the proposals and apartments and commercial premises in the Terrace across Grantham Street, officers are satisfied that the window to window relationship presented would be similar to that already found within the immediate context, e.g. between the apartments in the Sparkhouse and Swan Street. Therefore officers are satisfied that the relationship presented would not have a detrimental impact upon the occupants of existing properties from the perspective of overlooking or loss of privacy

iii) Noise and Disturbance

Whilst a resident has again raised concerns regarding the potential for noise and disturbance from the proposed use, there has not been a material change in circumstances from the outline to the current application to lead to a different conclusion being made with respect to the impacts of noise and disturbance from the proposals. Nonetheless, given that the proposed end use and the internal layout of the building is now fixed, including access and egress from the building, it would be reasonable to ensure that the management of the building is through an appropriate level building management plan, such as a 24 hour concierge serving the main entrance/reception.

c) The Planning Balance

Taking all the above in to account, it is considered that the proposed development of the site could be accommodated in the future in a manner that would not cause unacceptable harm in respect of most matters relevant to the protection of amenity. Moreover, with satisfactory controls over the mitigation employed in relation to the future management of

the building, the proposals would be socially and environmentally sustainable in the context of the Framework and would accord with the policies in the Local Plan.

3) Sustainable Access and Highway Safety

a) *Relevant Planning Policies*

The impacts of growth are enshrined in the Core Planning Principles of the Framework (Paragraph 17), which expects planning to actively manage this growth “to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable”. As such, Paragraph 35 requires that: “developments should be located and designed where practical to [amongst other things] give priority to pedestrian and cycle movements, and have access to high quality public transport facilities; and should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones”.

A number of Local Plan Policies are relevant to the access, parking and highway design of proposals. In particular, the key points of Policy LP13 are that “all developments should demonstrate, where appropriate, that they have had regard to the following criteria:

- a) Located where travel can be minimised and the use of sustainable transport modes maximised;
- b) Minimise additional travel demand through the use of measures such as travel planning, safe and convenient public transport, walking and cycling links and integration with existing infrastructure;
- c) Should provide well designed, safe and convenient access for all, giving priority to the needs of pedestrians, cyclists, people with impaired mobility and users of public transport by providing a network of pedestrian and cycle routes and green corridors, linking to existing routes where opportunities exist, that give easy access and permeability to adjacent areas”

There are also transport measures referred to in Policy LP36, which more specifically refers to development in the ‘Lincoln Area’, the key measures add to and reinforce the criteria within Policies LP5 and LP13. As such, they are intended to reduce the impact upon the local highway network and improve opportunities for modal shift away from the private car. In particular, development should support the East West Link in order to reduce congestion, improve air quality and encourage regeneration; and improve connectivity by means of transport other than the car. Similarly, Policy LP33 also requires that developments do not result in “levels of traffic or on-street parking which would cause either road safety or amenity problems.” Moreover, the policy also highlights the importance of providing appropriate parking for vehicles and cycles for all users within developments; and that walking and cycling links are maintained and promoted.

Paragraph 32 of the Framework suggests that the residual cumulative impacts of the development would need to be severe for proposals to warrant refusal. This is reinforced by Policy LP13 of the Local Plan which suggests that only proposals that would have “severe transport implications will not be granted planning permission unless deliverable mitigation measures have been identified, and arrangements secured for their implementation, which will make the development acceptable in transport terms.”

b) Assessment of the Implications of the Proposals

A resident has suggested that the application is not complete as the details of access are not sufficiently detailed. However, the layout plans for the application are sufficient for officers and statutory consultee (the Highway Authority) to be able to take a view on the appropriateness of the development. However, as with all applications, the precise design of the intersection and alignment with the highway would need to be agreed at a later date with the Highway Authority.

Moreover, whilst the formal response to the application is awaited from the Highway Authority, in informal discussions with their officer, there would not be any concerns in relation to the proposals, including the reduction upon existing car parking within the site by four spaces (from 30 to 26). However, their officer is in discussions with the applicant regarding some technical matters. Nonetheless, as alluded to above, it would be necessary for the applicant to carry out highway works as part of the application, including closing up the existing accesses on Grantham Street and Swan Street and agree works to the footpath and highway in connection with the proposed access to Flaxengate.

Consequently, it is the advice of officers that it would be difficult to argue that there would be a harmful impact upon highway safety. In particular, the inclusion of only one access / egress from Flaxengate for the car park is a positive outcome as it would result in:

- a reduction in the number of access points to one in Flaxengate which is inherently wider than Grantham Street and Swan Street;
- there being only one void in the ground floor elevations of the building; and
- the scale of the building reducing, as parking would no longer be required on two levels of the building.

In light of this, officers consider that it would be difficult to raise concerns regarding the development.

4) Archaeology

a) Relevant Planning Policy

The Framework and Planning Practice Guide as well as good practice advice notes produced by Historic England on behalf of the Historic Environment Forum including *Managing Significance in Decision-Taking in the Historic Environment* and *The Setting of Heritage Assets* are relevant to the consideration of Planning Applications.

Indeed, heritage is referred to within the core principles of the Framework (Paragraph 17) and Paragraph 128 of the Framework states that “in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.”

Paragraph 141 of the Framework states that LPAs should ‘require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible.’

Policy LP25 in the Central Lincolnshire Local Plan requires that development does not lead to significant detrimental impacts on heritage assets. This issue is directed in relation to archaeology that could be non-designated heritage assets.

b) Assessment of the Implications of the Proposals

Several planning conditions were imposed on the outline planning permission to require the provision of further information informative to reserved matters. The applicant has carried out further evaluation and provided the details of a foundation design, which is currently being negotiated with officers. At present, the building has been designed to ensure that the formation level of the development (and thereby the floor level of the car park) would be above Roman remains, which would be a part of a preservation strategy. However, officers are still in discussions with the applicant in relation to the final design of the foundations for the building, as this element needs to inform the design of the building.

Consequently, officers will provide Members with an update in relation to the progress made since the completion of this report on the update sheet.

5) Matters Controlled by Planning Conditions on the Outline Planning Permission

Unless indicated otherwise below, these matters will be controlled by the conditions included on the Outline Planning Permission:

a) Ecology, Biodiversity and Arboriculture

i) Relevant Planning Policies

Paragraph 118 of the NPPF requires LPAs to conserve and enhance biodiversity by refusing planning permission where significant harm resulting from a development cannot be avoided, mitigated or compensated for. Meanwhile, Policy LP21 refers to biodiversity and requires development proposals to “protect, manage and enhance the network of habitats, species and sites of international, national and local importance (statutory and non-statutory), including sites that meet the criteria for selection as a Local Site; minimise impacts on biodiversity and geodiversity; and seek to deliver a net gain in biodiversity and geodiversity.” The policy then goes on to consider the implications of any harm associated with development and how this should be mitigated.

ii) Assessment of the Impact of the Development

Members may recall that the approval of Outline Planning Permission enabled the developer to remove trees that would suppress the implementation of the development. However, a planning condition imposed on that permission ensures that the trees are only removed from the site once an appropriately designed scheme has been approved and a contractor has been appointed to develop the site. Until such time as both points have been satisfied the trees would remain protected in the conservation area.

Notwithstanding this, the scheme is now known and this would remove those trees, as such, it would be reasonable and proportionate to request the details of bird boxes for the building.

b) *Site Drainage*

In accordance with the requirements of Policy LP14 of the Plan and Paragraph 103 of the Framework, the design of schemes to deal with foul and surface water disposal were agreed by planning condition. As such, there would not be in conflict with the environmental dimension of sustainability outlined in Paragraph 7 of the Framework.

c) *Land Contamination*

In accordance with the requirements of Policy LP16 of the Plan and Paragraphs 109, 120 and 121 of the Framework, planning conditions were imposed on the outline planning permission to deal with land contamination prior to development being undertaken.

d) *Air Quality*

In accordance with the requirements of Policy LP13 of the Plan and Paragraphs 109, 120 and 124 of the Framework, a planning condition was imposed on the outline planning permission to request the developer to provide a scheme to include one or more charging points for electric vehicles within the development.

e) *Cycle Storage / Parking*

The outline planning application established that the site is sustainably located in the heart of the city, close to the facilities, services and employment opportunities that would support students; as well as the Universities which are accessible by cycle and walking routes.

Consequently, the proposed ground floor plan has therefore been updated to include provision of Sheffield Cycle Stands to provide secure cycle storage. This format of storage is recommended by the National Cycling Association and would offer a secure storage option for future residents.

f) *Impacts of Construction*

A planning condition was imposed on the outline planning permission which committed the applicant to providing details of the proposed parking for construction vehicles; the layout and location of the compound; and construction working and delivery hours.

g) *External Lighting*

In addition, the applicant is also committed through a further condition to providing details of any functional or architectural external lighting of the building or its curtilage, in order to avoid a detrimental impact upon neighbouring properties. It is therefore recommended that an appropriate scheme of lighting is controlled by planning condition.

h) *CCTV Cameras and Safety*

Officers of the Council, including the Council's CCTV Team Leader, have been in discussions with the applicant to ensure that CCTV coverage of the area would not be adversely affected by the development. The applicant has provisionally agreed to CCTV

Cameras being installed on the building and Members will note that one location has been indicated in the plans accompanying this report. However, it is anticipated that the precise location would almost certainly require further discussions so a condition would be required for these details to be agreed. This would ensure that existing monitoring of public areas for safety reasons would not be adversely impacted upon.

6) Other Matters

A resident has raised concerns regarding the number of proposed student rooms within the building and has compared the scheme presented at outline with that now proposed. As Members will appreciate, the scheme at outline was indicative and there is nothing to suggest that the applicant should provide a certain number of units of accommodation within the building. What is more, Members will note from other similar schemes in the city that the preference is for a clustering of bedrooms as this has been found to be a more appropriate social experience for students.

In addition, a further party has made comment in relation to the thermal properties of the building, including overheating but there are no policies within the Local Plan to insist that developments include measures to address the implications of solar gain. However, the applicant would be able to consider these matters in terms of the design of the plant and machinery proposed to serve the building.

7) Planning Balance

Paragraph 14 of the Framework sets out a presumption in favour of sustainable development which for decision taking means that where relevant policies of the development plan are out-of-date planning permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against policies in the Framework, taken as a whole; or specific Framework policies indicate development should be restricted. There are no restrictive policies that would lead to the proposals not being sustainable. However, a conclusion whether a development is sustainable is a decision that has to be taken in the round having regard to all of the dimensions that go to constitute sustainable development.

In this case, the principle of the development of the use proposed was established through the outline planning permission and this would deliver economic and social sustainability directly through the construction of the development and the use proposed therein; and indirectly through the occupation of the building. Moreover, as alluded to through that application, the provision of additional purpose-built student bed spaces available in a location relatively close to both universities in the city should hopefully reduce the dependency further upon houses in multiple occupation. This would also improve environmental sustainability.

It is clear from the main body of the report that the proposed building would be smaller than that approved at outline planning and the building has been designed to avoid overlooking or a loss of privacy, which were concerns identified with the outline scheme. It is therefore considered that the proposed building would not lead to any harm to amenity subject to the existing planning conditions on the outline planning permission and those recommended below.

There is no evidence to suggest matters of congestion or road safety would be harmful due to the social or environmental sustainability of the development. Furthermore, the

implications upon the character of the area and the residential amenities of near neighbours would not have negative sustainability implications for the local community, as they would lead to a development that would be socially sustainable. As such, with compliance with existing planning conditions and those recommended here, the development would be environmentally sustainable.

Subject to the foundation design of the proposed development being a suitable means of preservation for buried archaeological remains, in this instance officers would advise Members that the planning balance should fall in favour of the proposals due to the long term implications of the enhancement that would be brought to the conservation area, as well as the potential stimulus that the proposals could be for further wider enhancement of the historic townscape. This is particularly important given the proximity of Grantham Street to the High Street.

Thus, assessing the development as a whole in relation to its economic, social and environmental dimensions and benefits, officers are satisfied that the benefits of developing this site would, in the long-term, be more important than the potential impacts of not doing so. As such, it is considered that, in the round, this proposal could be considered as sustainable development and would accord with the Local Plan and Framework, sufficient for the recommendation of officers to be that suitable planning permission should be granted subject to planning conditions.

Application Negotiated either at Pre-Application or during Process of Application

Yes additional information provided and the scheme revised following officer feedback.

Financial Implications

The proposals would offer benefits to economic and social sustainability through spend by new and existing residents and visitors, jobs created/sustained through construction and the operation of the development respectively.

Legal Implications

None.

Equality Implications

None.

Conclusion

The presumption in favour of sustainable development required by the National Planning Policy Framework would apply to the proposals as there would not be conflict with the three strands of sustainability that would apply to development as set out in the planning balance. Therefore, there would not be harm caused by approving the development. As such, it is considered that the application should benefit from planning permission for the reasons identified in the report and subject to the conditions outlined below.

Application Determined within Target Date

Yes.

Recommendation

That the application is approved, with authority delegated to the Planning Manager to formulate Planning Conditions covering the matters referred to below:-

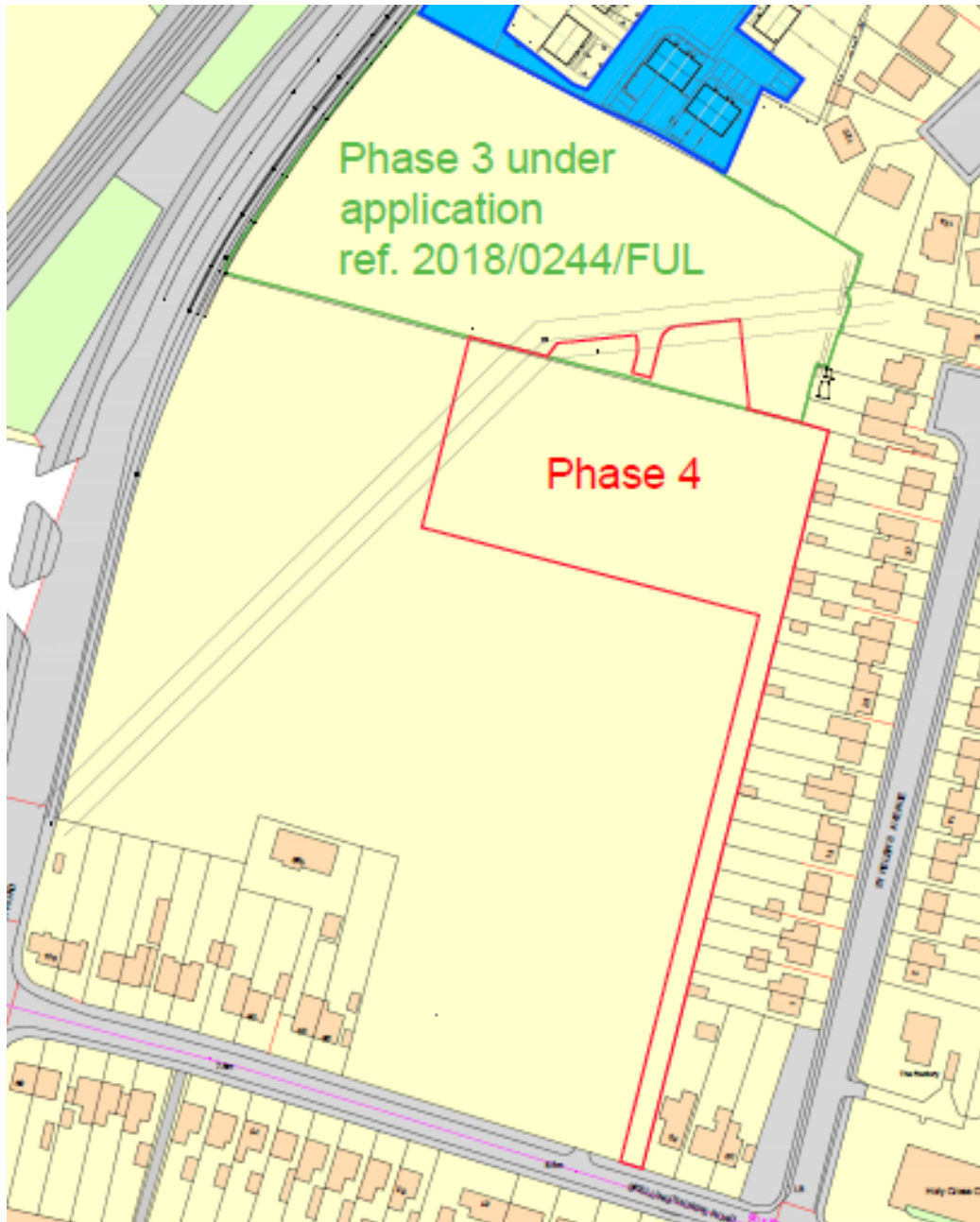
- Schedule of materials;
- Scheme for the inclusion of bird boxes on or as part of the fabric of the building;
- Scheme for Future Management of the Building; and
- Scheme for CCTV Cameras to Replace the Existing Provision.

Report by Planning Manager

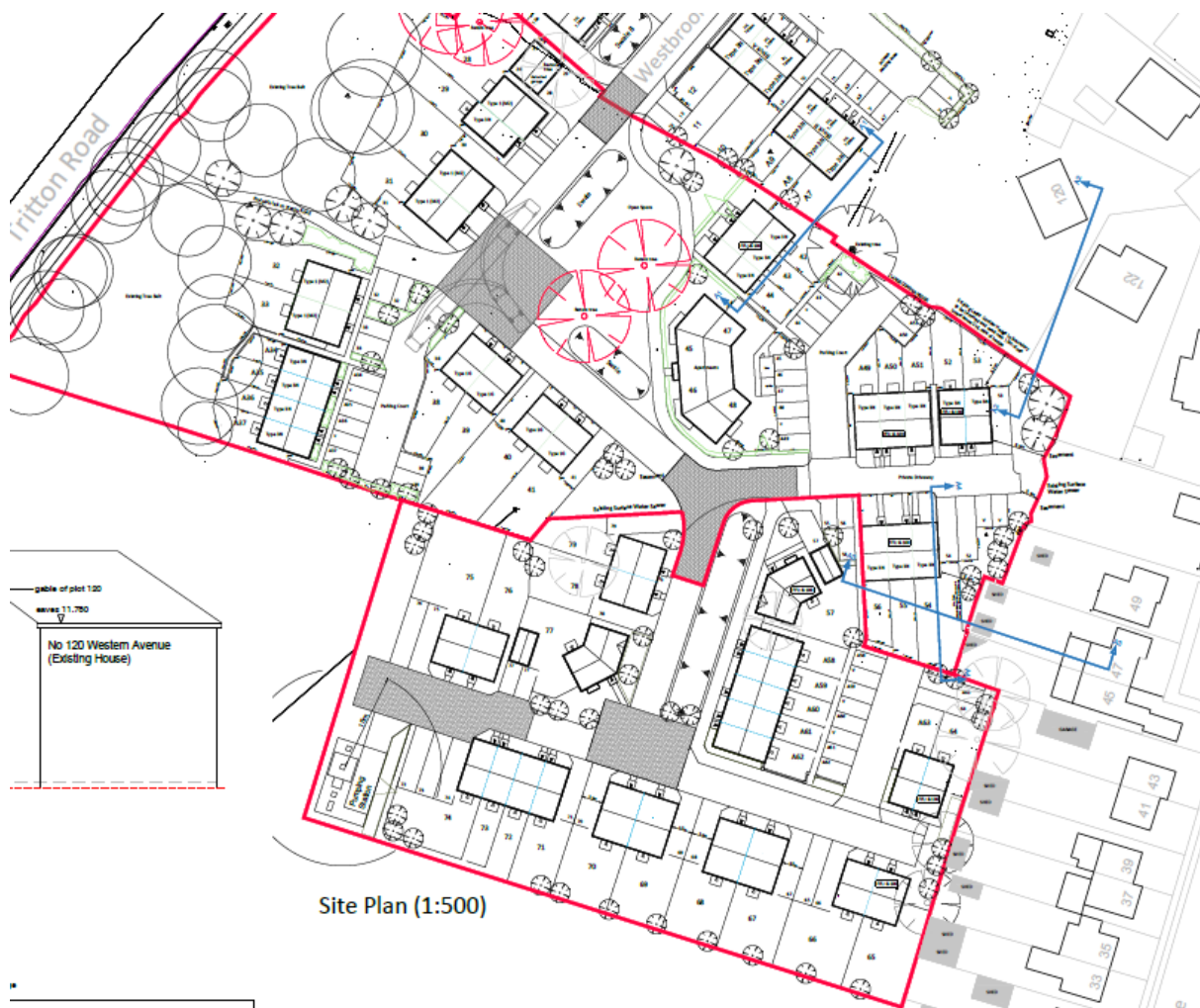
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Site location plan to show all Phases to LN6 development (Phases 1, 2 and 3 approved and proposed phase 4).





Site location phase 4



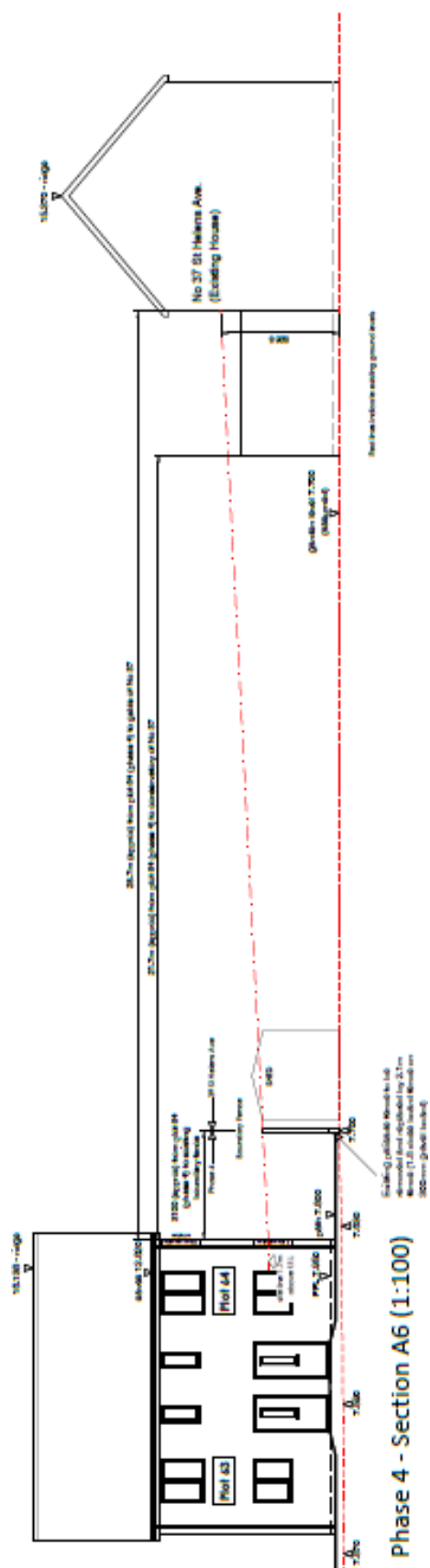
Combined site layout plan Phases 3 & 4.

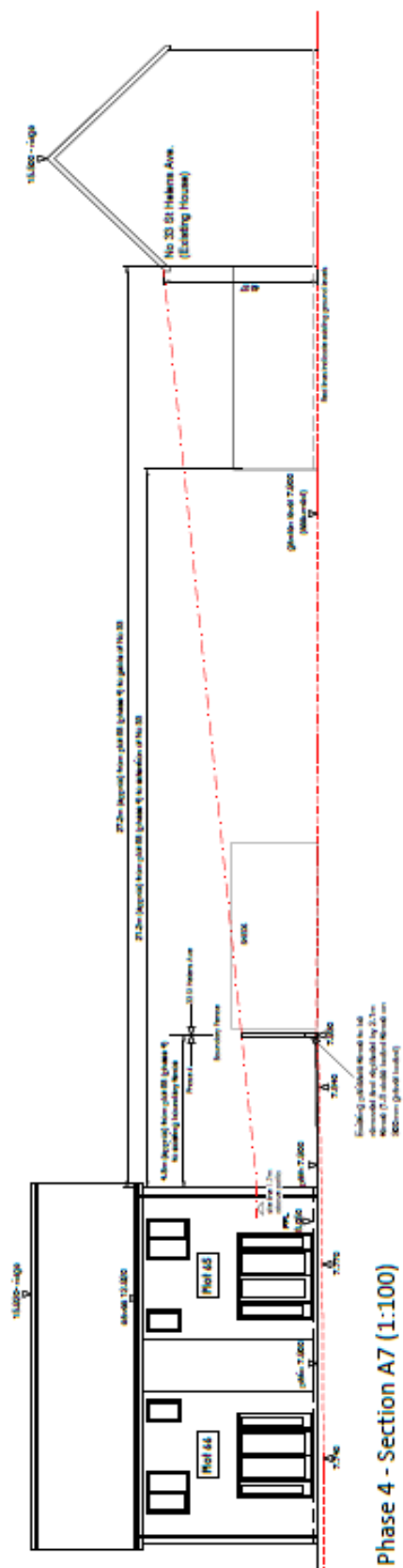


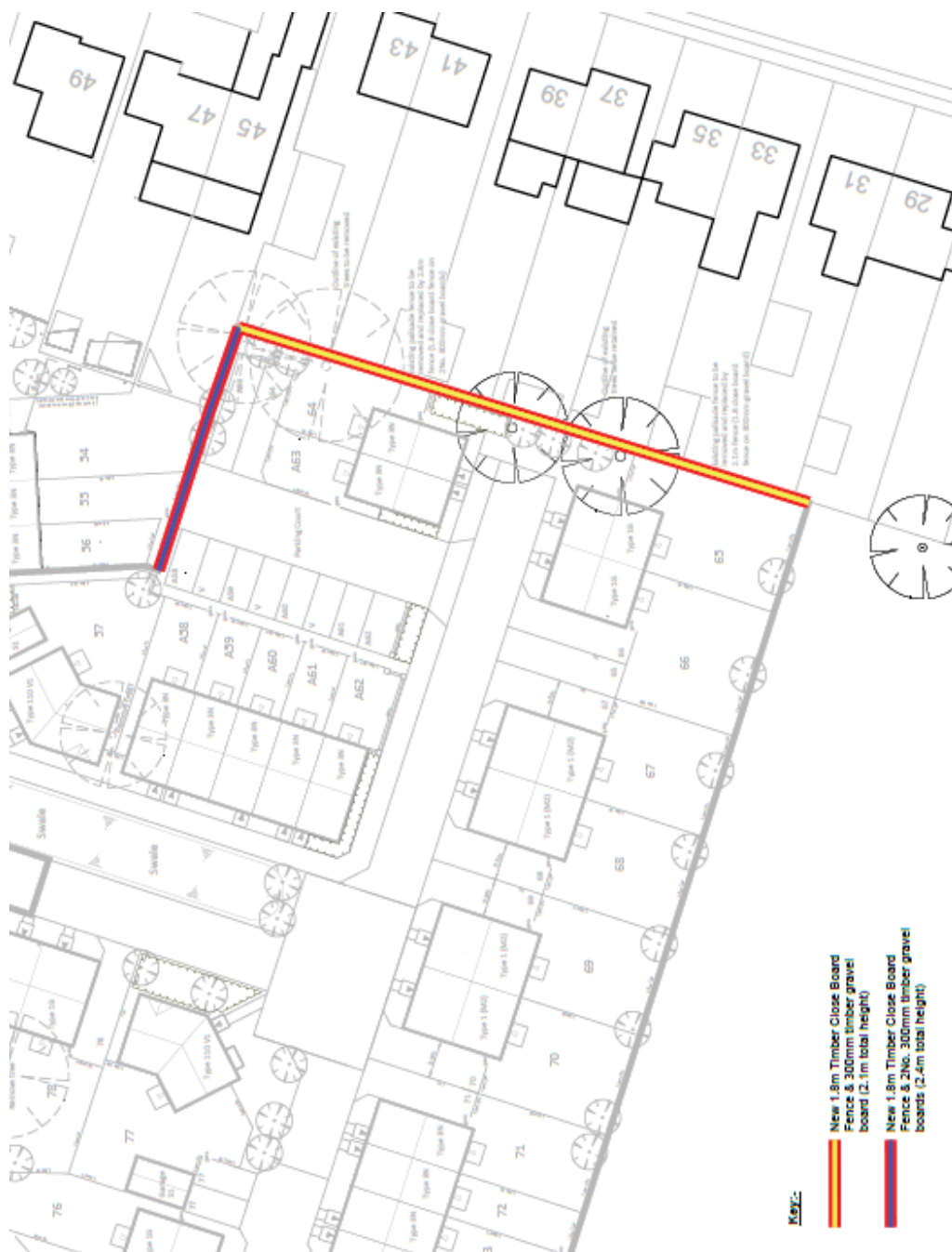
Revised site layout plan for Phase 4 showing the omission of the previously proposed new footpath link to Skellingthorpe Road



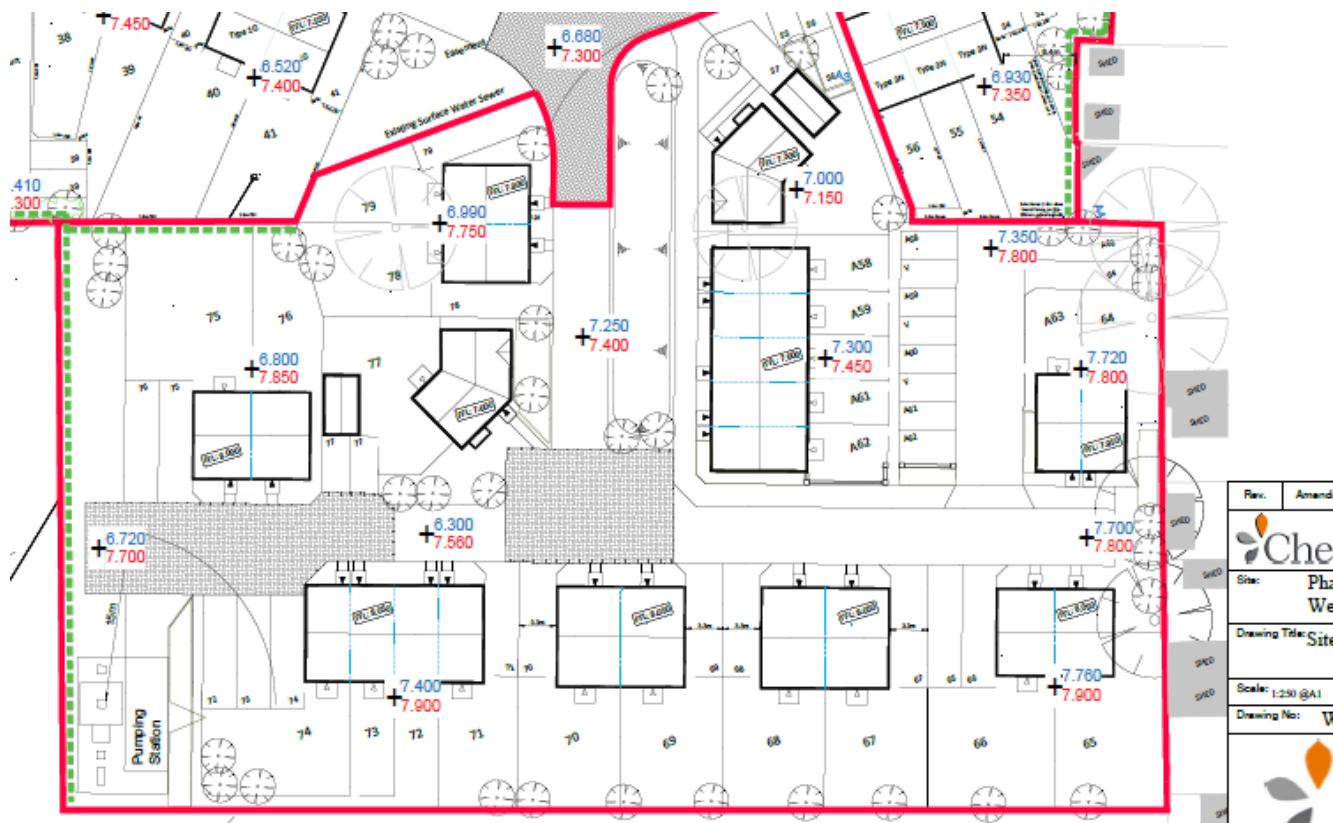
Plan to show site sections







Proposed Boundary treatments

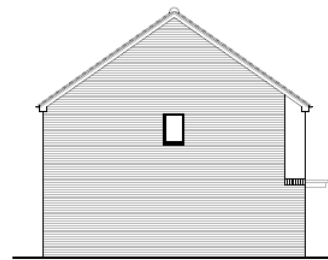


Plan to show existing (blue) and proposed site levels (red) and finished floor levels.

Proposed house types



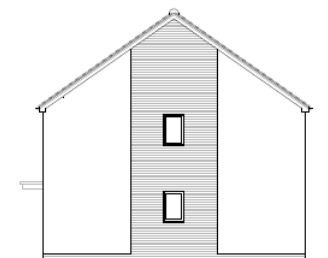
Front Elevation



Left Side Elevation



Rear Elevation



Right Side Elevation

Type 3N-5Block Elevations
Plots A58, A59, A60, A61, A62
no. No. 3N-5Block-Plannino-02 Scale 1:100 @ A3 March 2018





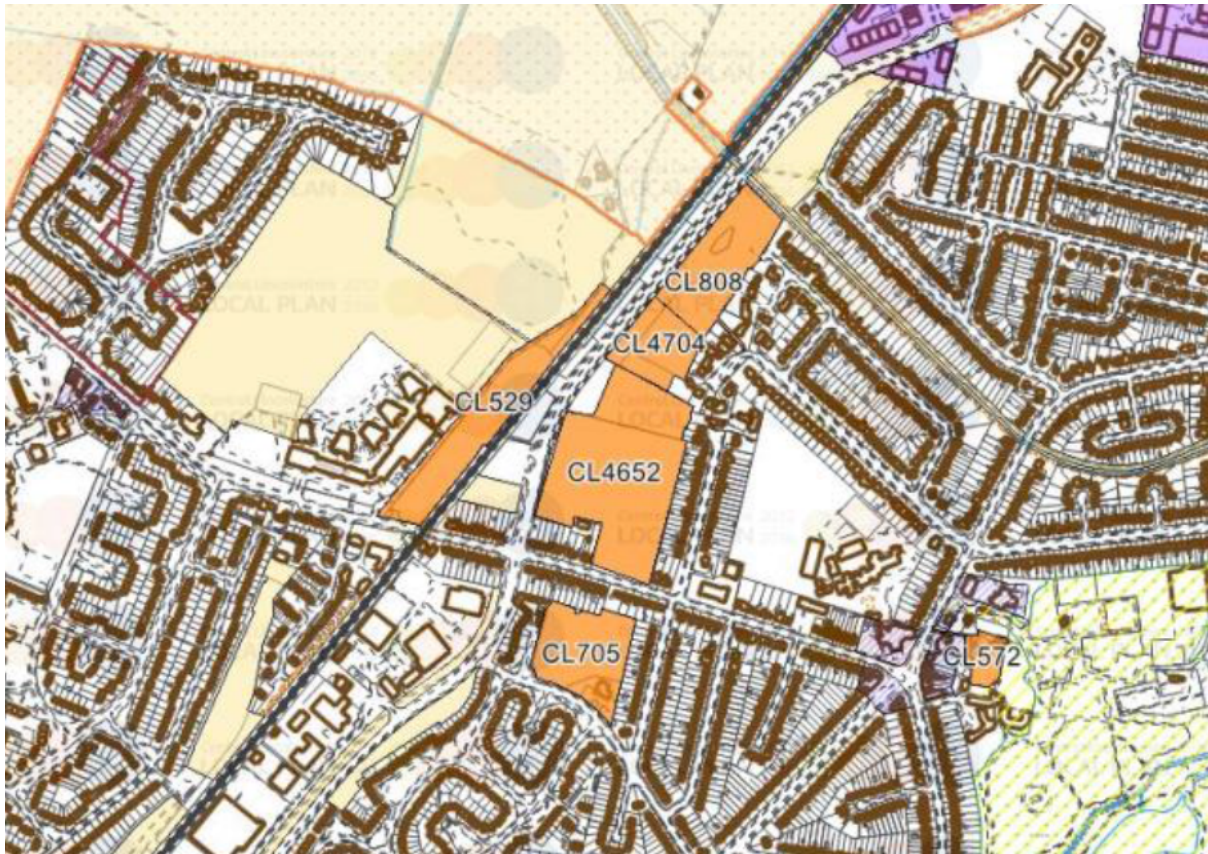
Type 1(M2) Floor Plans & Elevations Plots 65, 66, 69, 70

Dep. No. 1942/Planning01 Scale 1:100 @ A3 March 2018



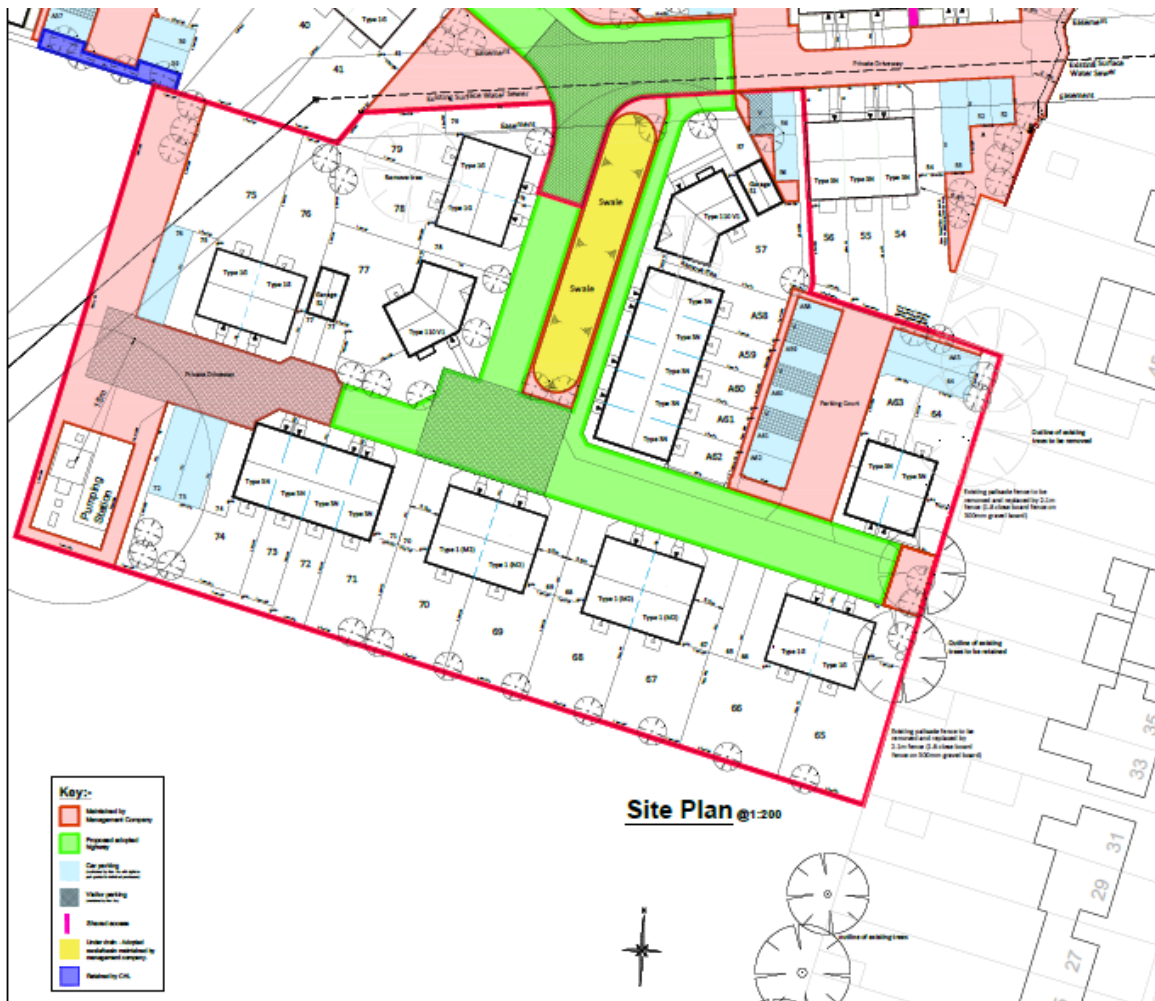
Type 3N Floor Plans & Elevations Plots 63, 64, 67, 68





Extract from CLLP to showing housing allocations

Revised Master Conveyance Plan



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Phase 3 looking towards the rear of St. Helen's Avenue



View South towards Skellingthorpe Road proposed Phase 4





Landscape buffer to western boundary with Tritton Road to be retained



View South of the rough grassland area to Phase 4. Boundary with Tritton Road to the right



View towards the Eastern site boundary with dwellings on St. Helen's Avenue beyond and 2no. trees to be removed to the centre and left of the photograph.

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2018/0458/FUL – Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.- Land at Westbrooke Road

Neighbour Comments

Mr Neville Coupland 27 St Helens Avenue Lincoln

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Mr Neville Coupland

Address: 27 st helens avenue Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer made comments neither objecting to or supporting the Planning Application

Comment Reasons:

Comment: Could you assist the deer..it has been driven/trapped in the field since development has moved across..it cannot return to open space due to fence and new development..it is good to see the large trees are remaining insitu along the perimeter...Are there further plans ahead for the remaining space? It would be good to see additional public services as well as new builds which are filling up green spaces

Brian J Alexander 84 Skellingthorpe Road Lincoln

84 Skellingthorpe Road
Lincoln
LN6 7QZ
17 April 2018

Your Ref: 32018/0458/Ful

Dear Sir

I wish to comment on this application in particular regarding the footpath planned to link the site with Skellingthorpe Road. As can be seen by the plan below it passes to the side of our property. I have added to the figure the approximate line of our drains which run beside our property to the road (blue).



Just over the fence there are a number of trees and bushes etc whose roots have in the past caused the drains to be blocked. I wish to avoid this happening again. Thus I request that a condition is placed that this boundary vegetation is removed as part of the placement of the footpath. Thereby reducing the chance of subsequent blockage of my drains and the inherent costs associated to me and whoever is responsible for these trees and bushes, the land owner. At the moment given that the drains are very close to the boundary it is a matter of when and not if.

Yours sincerely

Brian J Alexander.

David R Hipworth 1 Westbrooke Close

Director of Communities & Environment
Simon Walters MBA, ACIS, MCMI
City Hall, Beaumont Fee
Lincoln, LN1 1DF



1 Westbrooke Close
Lincoln
LN6 7TL

19 April 2018

Your Ref: 2018/0458/FUL

Dear Sir

CONSULTATION ON APPLICATION FOR PLANNING PERMISSION

**Proposed Development: Land at Westbrooke Road, Lincoln, Lincolnshire, LN6 7TB –
Erection of 23 dwellings with vehicular access from Westbrooke Place / Road**

Thank you for your letter dated 10 April 2018 in respect of the above.

It was not a surprise to see an application for Phase 4 in before Phase 3 has been approved and no doubt an application for Phase 5 is not far behind.

The development often appears to precede the planning permission and it all seems a forgone conclusion that the whole development has been agreed in advance. Established residents that I have spoken to say what is the point of objecting – you are wasting your time.

My main OBJECTION is that Westbrooke Road was built off Western Crescent for a dozen bungalows and it is the only access for residents and service vehicles for up to 200+ properties on development completion.

Yours faithfully

A black rectangular box redacting the signature of David R Hipworth.

David R Hipworth



Dr Andrea Paoli 47 Westbrooke Road Lincoln

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Dr Andrea Paoli

Address: 47 Westbrooke rd Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: We object to vehicular access from Westbrooke road only which would cause further noise and disturbance. Westbrooke Rd has already suffered from the many inconvenience of the development site: builder staff parking outside of any doors/drives of properties, vans and trucks passing by at any time causing frequent vibrations to the upper floor of the house. Vans and trucks too often drive at high speed through Westbrooke Rd compromising the overall safety of the road (which would need speed limit bumps!). The real problem here is that the residents have already gone through all these inconveniences for too long in Westbrooke RD, so building on the other side would require a different vehicular access.

Keith Iley and Alison Iley-Haigh 115 Western Avenue Lincoln

Bray, Kelly (City of Lincoln Council)

From: Technical Team (City of Lincoln Council)
Subject: FW: LN6 Phase 4, 2018/0458/FUL

Dear Alex

Further to our conversation I would like to formally oppose the plans for the phase 4 development of land at Westbrooke Road, Lincoln.

Reason being that they'll be too many cars coming down Western Ave, Western Crescent and Westbrook rd, especially the latter two as the roads are narrow with cars already parking on the road like in the pictures attached.

If the development goes ahead it needs another road out on Skellingthorpe Rd.





Plus the extra traffic on Boutham Rd in rush hour as this road is already very busy at that time.
Can you keep us informed on any developments please.

Kind regards

Keith Iley and Alison Iley-Haigh
115 Western Ave.

Michael Smalley 1 Westbrooke Road Lincoln LN6 7TB

Directorate of Communities & Environment

City Hall

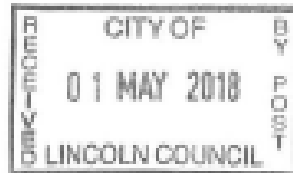
Beaumont Fee

LINCOLN LN1 1DF

25th April 2018

RE LN6 Phase 4

FTAO Alex Leatherland.



I wish to register my protest against the above application. The access from Westbrooke Road is not suitable for the traffic that this application will generate.

With the progression of each phase I have become aware of an increase in traffic on Westbrooke Road, both construction traffic and normal from the increase in residents. As yet not all the properties have been sold so there is going to be a continuing increase.

Westbrooke Road is a narrow road that was not built to take the amount of traffic this build will require. The original plan was for 56 properties, this application will take the total to 137. The original application was based on traffic assumptions from 56 properties not the figure this application will mean.

Wear and tear on the road is having an effect particularly on the junction of Westbrooke Road and Western Crescent. This is becoming close to collapse.

Yours Faithfully

Michael Smalley

1 Westbrooke Road

LINCOLN LN67TB

Mr A McCall 104 Western Avenue Lincoln LN6 7SZ

Mr. A. McCall
104, Western Ave.
Lincoln. LN67SZ
13/04/2018

Dear Alex Leatherland, in response to your letter of 10th april 2018, regarding the proposed development of Land at Westbrook Road, Lincoln. On the first development on this land myself and others requested a road through to Skellingthorpe road, this was blocked by The city council as a strip of land in it's path had not been sold, but this Land has now been sold to this same developer hence this new development, therefore this road could be built reducing the excessive Use of Westbrook road and all the damage that is already being done To the roads to Boultham park road, regardless of the amount of road Traffic will increase putting it from one estate through another, therefore I would request not a walkway but a road through to Skellingthorpe road. Thankyou for your time.

Alan McCall.

Mr and Mrs D B Marshall, B.Sc. 14 Westbrook Road Lincoln LN6 7TB

Director of Communities & Environment
Simon Walters MBA, ACIS, MCMI
City Hall
Beaumont Fee
Lincoln
LN1 1DF

14 Westbrooke Road
Lincoln
LN6 7TB
22 April 2018

Your Ref: 2018/0244/FUL and 2018/0458/FUL

cc: Planning Committee Chair & Vice Chair, Boutham Councillors, Cllr R Hills, all by email

Dear Sir

CONSULTATION ON APPLICATION FOR PLANNING PERMISSION

Location: Land at Westbrooke Road, Lincoln, LN6 7TB

Proposal: Erection of 29 dwellings with vehicular access from Westbrooke Road.

AND

Location: Land at Westbrooke Road, Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place.

OBJECTION

Residents are sick of this. What is the point of objecting? You are bending over backwards dancing to Chestnut Homes' tune. You have done everything in your power to help their piecemeal approach achieve their objective of 131 dwellings with the only vehicular access to them through Westbrooke Road. You have also allowed them to purchase public land, twice, without giving anyone else the opportunity to pay more for it. Land they are already using before officially getting planning permission!

Deals have been done behind closed doors, neither you, nor the County Council, will tell us what they paid for the land yet.

So much for local democracy and openness.

Yours disgustedly,

(Mr & Mrs) D B Marshall, B.Sc.

Mr and Mrs D B Marshall, B.Sc. 14 Westbrook Road Lincoln LN6 7TB

Director of Communities & Environment
Simon Walters MBA, ACIS, MCMI
City Hall
Beaumont Fee
Lincoln
LN1 1DF

14 Westbrooke Road
Lincoln
LN6 7TB
22 April 2018

Your Ref: 2018/0244/FUL and 2018/0458/FUL

cc: Planning Committee Chair & Vice Chair, Boultham Councillors, Cllr R Hills, all by email

Dear Sir

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Proposal: Erection of 29 dwellings with vehicular access from Westbrooke Road.

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So much for local democracy and openness.

Yours disgustedly,

(Mr & Mrs) D B Marshall, B.Sc.

Mr J M Edwards and Mrs B Edwards 10 Westbrooke Road Lincoln

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Mr James Edwards

Address: 10 Westbrooke Road Lincoln Lincoln

Comment Details

Commenter Type: Member of the Public

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: Director of Communities and Environment Mr J M & Mrs B Edwards

Simon Walters MBA ACIS MCMI 10 Westbrooke Road

City Hall, Beaumont Fee Lincoln

Lincoln LN1 1DF LN6 7TB

17 April 2018

Ref 2018/0458/FUL

Dear Sir

Objection to planning application 2018/0458/FUL

Land at Westbrooke Road, Lincoln.

Erection of 23 dwellings with vehicular access from Westbrooke Place and Westbrooke Road.

Dear Sir

We object to the Application on the following grounds.

Planning Process

In 2014 an initial proposal to build 60 dwellings on this development site was reduced to 52, a significant reduction clearly based on the concerns of the Planning Department.

If current applications are approved this will take the number of dwellings to about 130 and the concerns of residents will continue to be ignored.

Access

The only access to the site is via Westbrooke Road. The entrance to the road is a narrow sharp blind bend which is unsuitable for the speed and frequency of current traffic, any increase in the volume of traffic would make it more dangerous, and further affect the Quality of life of the residents.

Objections made to Phase1 and Phase2 Applications predicted serious deterioration of the road surface and stated that the road was not designed for the traffic that would use the road. These predictions have proved to be true; the road is in a deplorable state, any increase in traffic will make this worse.

Design

The application lays great emphasis on environmental considerations, with a focus on sustainable transport. This aim will only succeed if the number of vehicles is limited to the current level.

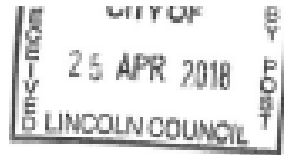
Possible Future Development

The previous Application made reference to 'Possible future link to the site to the south west' The link shown gives access to a very large development site. Allowing vehicle access to the LN6 development from a Large Development would be incompatible with the 'sustainable transport' ethos of the current and previous Planning Applications

The Planning Committee should insist that the layout of the site should NOT ALLOW VEHICLE ACCESS FROM ANY FUTURE DEVELOPMENT.

Mr J M Edwards Mrs B Edwards

Mr Karl Hutchinson and Miss Holly Wray 17 St Helens Avenue Lincoln LN6 7RA



17 St Helens Avenue
Lincoln
LN6 7RA

Development Team
City of Lincoln Council
City Hall
Beaumont Fee
Lincoln
LN1 1DF



Dear Sirs

**RE: YOUR REF: 2018/0458/FUL - OUR REF: LN6 PHASE 4
ERECTION OF 23 DWELLINGS WITH VEHICULAR ACCESS FROM WESTBROOKE PLACE AND
PEDESTRIAN LINK TO SKELLINGTHORPE ROAD**

Following your recent communication regarding the above development, we are writing to express our concerns with regards to the proposed pedestrian access link to Skellingthorpe Road from Phase 4 of the development which will run directly to the rear of our property. Please see detailed below our concerns for this pedestrian link.

- We feel the security of our property will definitely be compromised by this cycle/pedestrian link by providing easy public access to the rear of our property where there has previously been none.
- We do have elderly neighbours who would be more vulnerable with their property being able to be accessed by the rear boundary.
- We would like to know the proposed height of any fence boundaries and how these fences will be constructed and materials that will be used to ensure maximum security/privacy.
- Unless the fencing to be erected is of a reasonable height our privacy and enjoyment of our outside space will also be compromised.
- Can you confirm there will be appropriate lighting for the cycle/pedestrian link.
- Who is or will be responsible for maintaining and pruning the trees at the rear of our property. We have contacted yourselves previously with regards to the trees to the rear, as they are already overhanging our property and blocking sunlight from our garden, but no-one at the Council could confirm who was responsible for maintaining/pruning the trees and we are not aware of any maintenance that has been carried out on these trees since the school building was demolished. We would request that the trees to the rear of our property be removed as they are very overgrown.
- Who will be responsible for the cycle/pedestrian link, will this be adopted by the Council or will Chestnut Homes be responsible for maintaining and clearing rubbish which accumulates in the link, which could cause Health issues if not properly maintained.
- We also feel the link will be a big attraction for gangs of youths as a place to congregate, creating noise pollution at all hours of the day.
- The cycle/pedestrian link could also attract youths on motorbikes, etc racing up and down the cycle/pedestrian link at all hours of the day and night.

- We also feel there is no real benefit of the cycle/pedestrian Link from the development to Skellingthorpe Road and feel a better cycle/pedestrian Link would be to run alongside Phase 3 directly onto Tritton Road and the already established cycle path, as there is currently no cycle path to the area on Skellingthorpe Road where the link will be as the road is not wide enough and to encourage extra cycles exiting onto this busy road could result in an increase of unnecessary accidents occurring, especially children who often have no regard for road safety when riding cycles.
- Dog walkers will also use the link as a toilet for their pets, can you confirm you are proposing to provide doggy bins for their excrement as we imagine there would be nothing worse than sitting in our garden in the summer with the overpowering smell of dog excrement
- There is also an increased risk, with the current numbers of homeless people in the city, that they could set up camps on the cycle path as they will not be seen from the main roads.

We feel due consideration should be given to all the points above and would like some further information from either yourselves or Chestnut Homes in relation to the points we have raised.

Yours sincerely

Mr Karl Hutchinson & Miss Holly Wray

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Mr Rodney Mountcastle

Address: 6 Westbrooke Road, Lincoln, Lincolnshire LN6 7TB

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: As this application is so close in timescale to Chestnut Homes application Ref No 2018/0244/FUL I would like to think the Planning Department would consider them as one application and not two separate ones.

My reasons for objecting to this one are the same as application Ref No 2018/0244/FUL as yet again residents of Westbrooke Road find ourselves in a position of being steamrollered into having these small applications being accepted more or less as a forgone conclusion. If Chestnut Homes and the Planning Department were more transparent as to the size and intention of the whole development I dare say far more people would object to it.

I recently contacted the planning department with reference the volume and speed of traffic in the Westbrooke Road/ Western Crescent area and was told it wasn't your problem. Well by approving these applications without putting in any constraints or conditions I feel it is your problem and should be looked into as a matter of urgency.

The residents were asked by Chestnut Homes at the start of phase 1 to park their cars so as to make access for HGV's easier along Westbrooke Road. Chestnut Homes has had the cooperation of local residents for the last 3yrs regarding this matter but if we were all to park our cars on the road as we are legally entitled to HGV's delivering building supplies would have no access route to the site. I would therefore suggest as the old school site on Skellingthorpe Road looks to be next in line for development that an access road be brought in from there for construction traffic thus alleviating some of the problems on Westbrooke Road.

Mrs Maureen Bailey 12 Westbrooke Road LN6 7TB

Taylor, Louise (City of Lincoln Council)

From: Technical Team (City of Lincoln Council)
Sent: 08 May 2018 12:27
To: Taylor, Louise (City of Lincoln Council)
Subject: FW: Planning objection 2018/0458/FUL

From: M A Bailey [REDACTED]
Sent: 07 May 2018 18:29
To: Leatherland, Alex (City of Lincoln Council) <Alex.Leachland@lincoln.gov.uk>
Subject: Planning objection

Application Reference:	2018/0458/FUL
Address:	Land At Westbrooke Road Lincoln
Proposal:	Erection of 23 dwellings with vehicular access from '1' Skellingthorpe Road.
Case Officer:	Alex Leatherland

OBJECTION

I wish to object to phase 4 of the development LN6 7TB by Chestnut Homes

Reasons:

My reasons are unchanged from my previous objections to Phase 1 and Phase 2 and 3.

1....FLOODING ... The development is in an area prone to flooding

SUDs and the new drainage methods being used have yet to be proved to work in extreme wet conditions, and we do not wish to be used as guineapigs for these drainage systems.

I am particularly concerned that Anglian Water quotes the following in its consultation letter. This highlights the very real concerns regarding the unacceptable risk of flooding should the development continue to grow at its current rate.

Quote from ANGLIAN WATER:

***Surface Water Disposal (Section 4)**

CONDITION (4.2 Only) No hard-standing areas to be constructed until the works have been carried out in accordance with the surface water strategy so approved unless otherwise agreed in writing by the Local Planning Authority.

REASON

To prevent environmental and amenity problems arising from flooding.

FOR THE ATTENTION OF THE APPLICANT:

Next steps

Desktop analysis has suggested that the proposed development will lead to an unacceptable risk of flooding downstream. We therefore highly recommend that you engage with Anglian Water at your earliest convenience to develop in consultation with us a feasible drainage strategy"

2....**TRAFFIC** There are several serious traffic concerns...Westbrooke Road is neither suitable structurally or from a safety aspect. The junction with Western crescent is on a blind, narrow bend and the surface of the road, at this corner, is breaking up and cracking around the manhole covers. the surface of Westbrooke road and also the pavements are showing damage from the heavy traffic accessing the development site.

ACCESS to the site for road traffic is shown on the application as Westbrooke PLACE.
THIS FOOLS NOBODY.

Access to the site **REMAINS** via the very narrow **AND DANGEROUS** junction of Western Crescent/Westbrooke ROAD.

3....**CONGESTION** The junction at Western Avenue and Boultham Park Road is already seriously congested, particularly at school times, resulting in serious pedestrian safety **AND HEALTH** concerns.

4**PROCEDURES** ...Of particular, general concern is the way in which the development has been passed through the planning committee on a piecemeal basis. This has made it difficult for residents to reasonably object to what now amounts to a very large development, which will, according to the plans, extend into the Usher School Site on Skellingthorpe Road, thereby creating a future rat-run for traffic from Skellingthorpe Road to Boultham Park Road.

I would also like an answer to the following concern:-

Has the land being used for phase 3 and 4 been advertised on the open market. I understood that it belonged to Lincoln City Council (ie residents of Lincoln) and Lincolnshire Council Education Department. If so, did the city and county get the best possible price for it? **NO-ONE AT EITHER OF THE COUNCIL DEPARTMENTS SEEMS TO BE PREPARED TO ANSWER THIS SIMPLE QUESTION.**

Is no one listening to ordinary residents?

The planning committee has already allowed the area around Brayford Pool etc be destroyed by overcrowding it with high rise buildings, thereby blocking out the cathedral view for local residents and visitors to the southern end of the city.

I strongly request that the planning committee begins to use some ordinary common sense this time and uses its not insignificant powers to turn down this application,

I look forward to receiving a positive reply to my concerns

Mrs Maureen Bailey
12 Westbrooke Road
Lincoln.
LN67TB

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Ms Judy Gray

Address: 69 Skellingthorpe Road Lincoln

Comment Details

Commenter Type: Member of the Public

Stance: Customer made comments neither objecting to or supporting the Planning Application

Comment Reasons:

Comment: I write with reference to the plan to put a footpath from the new housing development onto Skellingthorpe Road.

Whilst in theory public footpaths are a good idea I have some concerns about the proposed access

Already there are many cyclists using the footpath on Skellingthorpe Road as if it were a designated cycle path. It is

not . Should this pedestrian access be used by cyclists coming out onto the footpath on Skellingthorpe Road I would be concerned for the safety of the public walking by the end of the proposed access . It must be a priority that the bollards placed at the end of the path are close enough to ensure that cyclists have to dismount before joining Skellingthorpe Road. This will ensure the safety of pedestrians and also of the cyclists themselves.

I would ask the question, is this footpath going to benefit from street lighting. In a time when lights are being turned off I suspect not. This raises the question of the safety of the users. The path will no doubt be in use as a shortcut but in winter when it is dark early and at night some thought needs to be given to the safety of the people using the path if it is not to be lit.

Regarding security, if the path is not lit I would suggest that this compromises the security of the properties on St Helens Avenue the back gardens of which will be adjacent to the path.

The space is currently a 'lung' in the centre of a fairly densely populated area. Currently there are deer and foxes in the field and recently we have seen kites flying over . Before any more

development takes place perhaps some thought could be given to the conservation of part of this land as a public amenity.

Finally, should the pedestrian access to Skellingthorpe Road be given permission I would hope that this is not a precedent for opening up vehicular access . Already this road is extremely busy, Tritton Road junction and the level crossing both working against a free flow of traffic. To add in another junction would add to this and would be likely to increase the number of accidents, of which there are ready more than enough in this area.

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Ms Donna Perkins

Address: 33 St Helens Avenue Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I wish to object to this proposal VERY STRONGLY

1. The security Gates at the back should remain as they give further security and privacy to my property. Anyway I thought residents of St. Helens had first refusal of buying the land. off to the solicitor to check my deeds.
2. The houses are too close to my property affecting the evening light in the garden at present I can see the sun going down putting these houses up will block the sun in the afternoon and evening into my garden of which I have had the pleasure of since I moved here in 2007
3. There is a considerable amount of wildlife with nowhere to go A deer, 3 foxes a red kite, hedgehogs snakes and rabbits. The red kite is a protected species!!
4. The noise from the development going up and the added people living there will lower the price of my property.
5. Just because the land is available for housing why should you not consider the people who live next to the field and most certainly the wildlife of which I think is very inconsiderate. I hope this objection is taken seriously and not just a paper exercise or a money making scheme for the builders.

Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Customer Details

Name: Mr Gary Fountaine

Address: 35 ST HELENS AVENUE LINCOLN

Comment Details

Commenter Type: Member of the Public

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: The plans have changed significantly since first submission, I object on three counts.

Firstly the houses will block our natural sunlight light from late afternoon/early evening, we already have that issue with the existing trees but that is bearable with some management of the trees.

Secondly the new houses are significantly closer to our boundary lines than originally proposed and will leave us in danger of being overlooked (how will that affect the value of my house should I wish to sell???)

Thirdly the removal of the green security fencing leaves my property exposed and I suspect the removal of this is purely down to aesthetics, if that is the case then leave the fence where it is and put the proposed wooden fence in front of that, that will then mean mine and others properties are still reasonably secure. If the green security fencing is removed what is the rational behind that decision?

Comments gratefully received.

Gary

Consultee Comments

Dear Sir/Madam

Thank you for referring the above application, which was received on 06 April 2018.

This proposal falls outside the scope of matters on which the Environment Agency is a statutory consultee. Therefore we have no comment to make on this application.

Should you require any additional information, or wish to discuss these matters further, please do not hesitate to contact me on the number below.

Kind regards,

Keri Monger

Sustainable Places – Planning Adviser | Lincolnshire and Northamptonshire

Environment Agency | Nene House, Pytchley Road Industrial Estate, Pytchley Lodge Road, Kettering, NN15 6JQ

keri.monger@environment-agency.gov.uk | LNplanning@environment-agency.gov.uk

Direct Dial: 020 847 48545 | Team Dial: 020 302 53536



Planning Applications – Suggested Informative Statements and Conditions Report

AW Reference: 00027620
Local Planning Authority: Lincoln City Council
Site: Land At Westbrooke Road, (Phase 4),
LINCOLN - Boultham
Proposal: Erection of 23 dwellings with vehicular access
from Westbrooke Place and
pedestrian link to Skellingthorpe Road.
Planning Application: 2018/0458/FUL

Prepared by: Pre-Development Team

Date: 27 April 2018

If you would like to discuss any of the points in this document please
contact me on 0345 606 6087 or email
planningliaison@anglianwater.co.uk

ASSETS

Section 1 – Assets Affected

- 1.1 There are assets owned by Anglian Water or those subject to an adoption agreement within or close to the development boundary that may affect the layout of the site. Anglian Water would ask that the following text be included within your Notice should permission be granted.

"Anglian Water has assets close to or crossing this site or there are assets subject to an adoption agreement. Therefore the site layout should take this into account and accommodate those assets within either prospectively adoptable highways or public open space. If this is not practicable then the sewers will need to be diverted at the developers cost under Section 185 of the Water Industry Act 1991. or, in the case of apparatus under an adoption agreement, liaise with the owners of the apparatus. It should be noted that the diversion works should normally be completed before development can commence."

WASTEWATER SERVICES

Section 2 – Wastewater Treatment

- 2.1 The foul drainage from this development is in the catchment of Canwick Water Recycling Centre that will have available capacity for these flows

Section 3 – Foul Sewerage Network

- 3.1 The sewerage system at present has available capacity for these flows. If the developer wishes to connect to our sewerage network they should serve notice under Section 106 of the Water Industry Act 1991. We will then advise them of the most suitable point of connection.

Section 4 – Surface Water Disposal

- 4.1 The preferred method of surface water disposal would be to a sustainable drainage system (SuDS) with connection to sewer seen as the last option.

Building Regulations (part H) on Drainage and Waste Disposal for England includes a surface water drainage hierarchy, with infiltration on site as the preferred disposal option, followed by discharge to watercourse and then connection to a sewer.

- 4.2 The surface water strategy/flood risk assessment submitted with the planning application relevant to Anglian Water is acceptable.

We request that the agreed strategy is reflected in the planning approval

Section 5 – Trade Effluent

- 5.1 Not applicable

Section 6 – Suggested Planning Conditions

Anglian Water would therefore recommend the following planning condition if the Local Planning Authority is mindful to grant planning approval.

Surface Water Disposal (Section 4)

CONDITION (4.2 Only)

No hard-standing areas to be constructed until the works have been carried out in accordance with the surface water strategy so approved unless otherwise agreed in writing by the Local Planning Authority.

REASON

To prevent environmental and amenity problems arising from flooding.

FOR THE ATTENTION OF THE APPLICANT:

Next steps

Desktop analysis has suggested that the proposed development will lead to an unacceptable risk of flooding downstream. We therefore highly recommend that you engage with Anglian Water at your earliest convenience to develop in consultation with us a feasible drainage strategy.

If you have not done so already, we recommend that you submit a Pre-planning enquiry with our Pre-Development team. This can be completed online at our website <http://www.anglianwater.co.uk/developers/pre-development.aspx>

Once submitted, we will work with you in developing a feasible mitigation solution.

If a foul or surface water condition is applied by the Local Planning Authority to the Decision Notice, we will require a copy of the following information prior to recommending discharging the condition:

Foul water:

- Feasible drainage strategy agreed with Anglian Water detailing the discharge solution including:
 - Development size
 - Proposed discharge rate (Should you require a pumped connection, please note that our minimum pumped discharge rate is 3.8l/s)
 - Connecting manhole discharge location (No connections can be made into a public rising main)
- Notification of intention to connect to the public sewer under S106 of the Water Industry Act (More information can be found on our website)
- Feasible mitigation strategy in agreement with Anglian Water (if required)

Surface water:

- Feasible drainage strategy agreed with Anglian Water detailing the discharge solution, including:
 - Development hectare size
 - Proposed discharge rate (Our minimum discharge rate is 5l/s. The applicant can verify the site's existing 1 in 1 year greenfield run off rate on the following HR Wallingford website - <http://www.uksuds.com/drainage-calculation-tools/greenfield-runoff-rate-estimation>. For Brownfield sites being demolished, the site should be treated as Greenfield. Where this is not practical Anglian Water would assess the roof area of the former development site and subject to capacity, permit the 1 in 1 year calculated rate)
 - Connecting manhole discharge location
- Sufficient evidence to prove that all surface water disposal routes have been explored as detailed in the surface water hierarchy, stipulated in Building Regulations Part H (Our Surface Water Policy can be found on our website)

Comments from Guy Hird Upper Witham Internal Drainage Board 13/04/2018

UD-3944-2018-PLN

Dear Sir/Madam

REFERENCE: 2018/0458/FUL
 DEVELOPMENT: ERECTION OF 23 DWELLINGS WITH VEHICULAR ACCESS FROM WESTBROOKE PLACE AND PEDESTRIAN LINK TO SKELLINGTHORPE ROAD
 LOCATION: LAND AT WESTBROOKE ROAD, LINCOLN

Thank you for the opportunity to comment on the above application. The site is within the Upper Witham Internal Drainage Board district.

This is the fourth phase for this site which has been built sequentially, good practice would be to design the surface water drainage system for the whole of the development area in order to provide a comprehensive and efficient SuDS scheme.

In absence of a suitable a Drainage Strategy and details the Board **Objects** to this application, see comments below.

Comment and information to Lincolnshire CC Highway SUDs Support

No development should be commenced until the Local Planning Authority, in consultation with the Lead Local Flood Authority has approved a scheme for the provision, implementation and future maintenance of a surface water drainage system. A Drainage Strategy is included in the submission however

- there are no details of the proposed drainage system, including attenuation
- the Anglian Water information refers to 132 dwellings and an allowable discharge of 13.33l/s. The Drainage Strategy refers to a Phase 3 (29 houses) rate of 6.0 l/s and Phase 4 (23 houses) rate of 7.33 l/s. This leaves a zero discharge rate for the other 80 houses.
- any discharge should be limited to the greenfield rate, the proposed discharge to the Anglian Water surface water sewer (13.33l/s) is above this rate given the site area of 1.584Ha (Phase 3 and Phase 4). As indicated above the Anglian Water allowable discharge must allow for a larger site.
- discharge into the Anglian Water may be restricted when there are high water levels in the Boultham Catchwater.
- the ground raising of up to 1.5m will create a dam to the land south of the site, the Strategy fails to address this.

All drainage routes through the Site should be maintained both during the works on Site and after completion of the works. Provisions should be made to ensure that upstream and downstream riparian owners and those areas that are presently served by any drainage routes passing through or adjacent to the Site are not adversely affected by the development. Drainage routes shall include all methods by which water may be transferred through the Site and shall include such systems as "ridge and furrow" and "overland flows". The effect of raising Site levels on adjacent property must be carefully considered and measures taken to negate influences must be approved by the Local Planning Authority.

Any ground raising would affect the area to the south of the site.

Regards

Councillor Gary Hewson

Will be viewing the Westbrooke Road application Why is there a pedestrian link to Skellingthorpe Road ? This would seem to point towards the likely building of housing on the old Usher School site ? I would be concerned if any further vehicle access would be allowed from any development on that site on to the Westbrooke Road development The area and amenity as been effected enough by the present developments with out encountering any more from the Usher School site I would ask please for a site visit for this planning application and the reasons for the footpath who is paying for it and also have those houses on St Helens Avenue been given notice of this proposed footpath which is adjacent to their rear gardens

I understand the Council owns the land for this latest planning application and should not allow any further Road connection to the Usher School site with these proposals

Thanks Gary



LINCOLNSHIRE POLICE

POLICE HEADQUARTERS
PO Box 999
LINCOLN LN5 7PH
Fax: (01522) 558128
DDI: (01522) 558292
email
john.manuel@lincoln.pnn.police.uk

Your Ref: App. 2018/0458/FUL

8th April 2018

Development & Environmental Services

City Hall, Beaumont Fee
Lincoln, LN1 1DF

Re: Westbrooke Road, Lincoln, Lincolnshire, LN6 7TB (23 Units)

Thank you for your correspondence and opportunity to comment on the proposed development.

Lincolnshire Police has no formal objections to the planning application in principle but would recommend that the attached recommendations are implemented.

External Doors and Windows

Building Regulations (October 1st 2015) provides that for the first time all new homes will be included within Approved Document Q: Security – Dwellings (ADQ).

Approved document Q applies to all new dwellings including those resulting from change of use, such as commercial premises, warehouse and barns undergoing conversions into dwellings. It also applies within Conservation Areas.

This will include doors at the entrance to dwellings, including all doors to flats or apartments, communal doors to multi-occupancy developments and garage doors where there is a direct access to the premises. **Where bespoke timber doors are proposed, there is a technical specification in Appendix B of the document that must be met.**

Windows: in respect of ground floor, basement and other easily accessible locations.

The secured by design requirement for all dwelling external doors is PAS 24:2016 (doors of an enhanced Security) or WCL 1 (WCL 1 is the reference number for PAS 23/24 and is published by Warrington Certification Laboratories).

All ground floor windows and doors and those that are easily accessible from the ground must conform to improved security standard PAS24:2016. **Window retainers should be provided on all windows that are accessible.**

Door Chains and viewers

A door chain must be installed on the door set that the occupier would expect to be the main entry (front) door. A door viewer must also be fitted between 1200 mm and 1500 mm from the bottom of the door (not required if the door set is installed with clear glazing or adjacent windows provide a clear view of the front door entrance).

Lighting

Lighting should be designed to cover the external doors and be controlled by *photoelectric cell* (dusk to dawn) with a manual override. The use of low consumption lamps with an efficacy of greater than 40 lumens per circuit watt is required; it is recommended that they be positioned to prevent possible attack.

Utilities

In order to reduce the opportunities for theft by 'bogus officials' the utility meters should, where possible, be located to the outside of the dwelling at a point where they can be overlooked. This will negate the need for an official to enter the building in order to read a meter, which will in turn reduce the opportunity for distraction burglary. Where possible utility meters in multi occupancy developments should be located on the ground floor between access controlled doors (air lock system) so that access can be restricted to the meters

Note 33.1: Where a utility provider refuses to provide external meters, and there is an obvious (historic) risk of distraction burglary within the location, the developer should consider an alternative supplier.

Intruder Alarm

Where an intruder alarm is installed it should be compliant and meet the standards of BSEN 50131. It is recommended that any alarms system is provided and installed by a police approved company registered with the National Security Inspectorate (NSI) or the Security Systems & Alarms Inspection Board (SSAIB). It is also important that residents are clearly instructed in its use.

Landscaping

Any landscaping should be kept to a maximum growth height of 1 metre. Whilst any trees should be pruned up to a minimum height of 2 metres, thereby maintaining a clear field of vision around the development. Trees when fully grown should not mask any lighting columns or become climbing aids.

Inclusive to the application should be strict management conditions that ensure the maintenance of and general good management of the estate additional to established security of the properties.

Boundaries between public and what is private space should be clearly defined and open accessible spaces should not allow for any unintended purpose which may cause any form of anti-social behaviour or nuisance. I would recommend that these spaces are defined clearly by low level (carefully considered) planting of limited growth height and maintenance shrubbery (maximum growth height of 1m).

Pedestrian Access Route / Footpath

This design has made good and effective use of a cul-de-sac (single point of entry) design which has been proven to help reduce crime and disorder and anti-social behaviour and additionally enhance community cohesion within the development.

It is preferable that footpaths are not placed to the rear of properties or provide such access, if deemed essential they should be securely gated. The gates should be placed to the front of the building line and suitable illuminated with a key operated lock from both sides of the gate. Effort should be made to ensure that the opportunity for concealed and easy access to individual premises is avoided; this is particularly relevant to ground floor windows.

I do have concerns regarding the public footpath that provides unrestricted access to this development thus undermining the benefits of a cul-de-sac layout which has been proven to help reduce crime and disorder and enhance community cohesion. This footpath should benefit from appropriate lighting and an agreed rolling programme of maintenance to ensure that shrubbery and natural foliage is kept under control and not allowing for potential hiding places or hidden recesses. I do not consider that given the length of this footpath it will command an image of a safe and secure route particular during hours of darkness and ideally should be avoided.

It is strongly advised that if there are any rear access (service) alleyways incorporated, they must be gated at their entrances. The gates must not be easy to climb over or easily removed from their hinges and they must have a key operated lock. By Design – Better Places to Live (Companion Guide to Planning Policy Statement 3) page 46 states:

"Rear servicing can undermine the security of dwellings by allowing strangers access to the rear of dwellings."

Parking Provision and Parking Courtyards.

If properties have driveways to the side of the dwellings themselves, windows should be incorporated in the side elevation at landing or first floor level to allow residents to overlook their own vehicles. Consideration towards the provision of appropriate and sufficient parking for visitors should be part of this plan as failure to do so may result in inconsiderate and obstructive parking.

Where unit (Plot) 63 abuts the entrance to the proposed parking courtyard I would recommend (if not already included – I could not determine this from the plans) that suitable windows from active rooms are installed to provide some natural surveillance opportunities.

Recent research conducted by Professor Rachael Armitage (Huddersfield University) on behalf of the Design Council/CABE, Home Office and Secured by Design, has clearly shown that rear parking courts are vulnerable to crime. They have higher levels of vehicle crime and criminal damage than other types of parking, and also facilitate offender access to the rear of properties. Residents do not tend to use their allocated spaces within these courts, preferring to park on street, regardless of whether the street was designed for on street parking.

Other research states: "The recent fashion for placing parking spaces behind buildings has led to many schemes around the country being blighted by cars parked to the front of the house where there is no space designed to accommodate them. It is an inefficient use of land, as a large proportion is used for roads and parking areas; the internal routes result in reduced garden sizes; there is loss of security and privacy to the rear of the home; and, with parking to the rear of the house, residents may be less likely to use their front doors with a consequent loss of activity in the street.

Please do not hesitate to contact me should you need further information or clarification.

Please refer to *New Homes 2016* which can be located on www.securedbydesign.com

Crime prevention advice is given free without the intention of creating a contract. Neither the Home Office nor the Police Service takes any legal responsibility for the advice given. However, if the advice is implemented it will reduce the opportunity for crimes to be committed.

Yours sincerely,

John Manuel MA BA (Hons) PGCE Dip Bus.
Force Designing Out Crime Officer

Consultee Comments for Planning Application 2018/0458/FUL

Application Summary

Application Number: 2018/0458/FUL

Address: Land At Westbrooke Road Lincoln

Proposal: Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road.

Case Officer: Alex Leatherland

Consultee Details

Name: Ms Catherine Waby

Address: St Mary's Guildhall, 385 High Street, Lincoln LN5 7SF

Email: lincolncivictrust@btconnect.com

On Behalf Of: Lincoln Civic Trust

Comments

OBJECTION: Our objections to this application are based on the same objections that we have made for the application 2018/0244/ful this being Phase 4 of the LN6 Project

We have serious misgivings that none our concerns about the whole project are being addressed and that these applications are abusing the planning system.

1. The access road to the site is totally inappropriate and that a further access on to Skellingthorpe Road should be created BEFORE any further applications are considered and this should even be in place now in order to remove the construction traffic from what is a minor interior estate road.
2. Limited parking spaces provided for each dwelling given that the road structure is of a shared nature and hence on-Street parking will be very difficult.
3. The shared use of the internal road structure with virtually no dedicated pedestrian walkways of cycle tracks.
4. The risk of flooding to the existing residences does not appear to have been adequately considered

Further Comment: As with the previous application we are concerned that the phased method of applying for planning permission does not fully consider the impact that the total development will have on services, transport and the environment. Access to the whole development via Westbrooke Drive and Westbrooke Crescent and then on to Boultham Park Road is in our view totally unacceptable and are very surprised that City Council Planning Department and the County Council Highways Department have seen fit to allow this creeping development to continue. The

only acceptable solution is for an additional road from Skellingthorpe Road to be built to alleviate the misery of the current residents in the area and for the future residents of the whole development.

Application ref: 2018/0458/FUL

Our ref: 243670

Natural England has no comments to make on this application.

Natural England has not assessed this application for impacts on protected species. Natural England has published [Standing Advice](#) which you can use to assess impacts on protected species or you may wish to consult your own ecology services for advice.

Natural England and the Forestry Commission have also published standing advice on [ancient woodland and veteran trees](#) which you can use to assess any impacts on ancient woodland.

The lack of comment from Natural England does not imply that there are no impacts on the natural environment, but only that the application is not likely to result in significant impacts on statutory designated nature conservation sites or landscapes. It is for the local planning authority to determine whether or not this application is consistent with national and local policies on the natural environment. Other bodies and individuals may be able to provide information and advice on the environmental value of this site and the impacts of the proposal to assist the decision making process. We advise LPAs to obtain specialist ecological or other environmental advice when determining the environmental impacts of development.

We recommend referring to our SSSI Impact Risk Zones (available on [Magic](#) and as a downloadable [dataset](#)) prior to consultation with Natural England. Further guidance on when to consult Natural England on planning and development proposals is available on gov.uk at <https://www.gov.uk/guidance/local-planning-authorities-get-environmental-advice>

Yours sincerely

Joanne Widgery

Natural England

Consultations Team

Hi Alex

The County Council has no comments to make in relation to education from this scheme. Prior to the implementation of CIL, a secondary request of £33,982 would have been made, however this is for information only with secondary education being an item on the City Council's Reg.123 list.

Kind regards

Simon

Simon Challis

Strategic Development Officer

Corporate Property

Comments from NHS England

Good morning,

On this occasion NHS England will not be putting in a section 106 tender for the development of 23 dwellings in Lincoln.

Vicky Allen

NHS England

Environment & Economy
Lancaster House
36 Orchard Street
Lincoln LN1 1XX
Tel: (01522) 782070
E-Mail: Highwayssudssupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2018/0458/FUL

With reference to this application dated 4 April 2018 relating to the following proposed development:

Address or location

Land At Westbrooke Road, Lincoln

Date application referred by the LPA
6 April 2018

Type of application: Outline/Full/RM/
FUL

Description of development

Erection of 23 dwellings with vehicular access from Westbrooke Place and pedestrian link to Skellingthorpe Road

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Requests that any permission given by the Local Planning Authority shall include the conditions below.

CONDITIONS (INCLUDING REASONS)

HI03

Prior to the submission of details for any access works within the public highway you must contact the Head of Highways - on 01522 782070 for application, specification and construction information.

HI05

You are advised to contact Lincolnshire County Council as the local highway authority for approval of the road construction specification and programme before carrying out any works on site.

HI08

Please contact Lincolnshire County Council Streetworks & Permitting team on 01522 782070 to discuss any proposed statutory utility connections and any other works which will be required in the public highway in association with this application. This will enable Lincolnshire County Council to assist you in the coordination and timings of such works.

HP29

No development shall be commenced until an Estate Street Phasing and Completion Plan has been submitted to and approved in writing by the Local Planning Authority. The Estate Street Phasing and Completion Plan shall set out the development phases and the standards that estate streets serving each phase of the development will be completed.

Reason: To ensure that the estate streets serving the development are completed and thereafter maintained to an acceptable standard in the interest of residential/highway safety; to ensure a satisfactory appearance to the highways infrastructure serving the development; and to safeguard the visual amenities of the locality and users of the highway, in accordance with Policies of the Development Plan.

HP30

No dwellings shall be occupied until the estate streets affording access to those dwellings has been completed in accordance with the Estate Street Development Plan.

Reason: To ensure that the estate streets serving the development and completed and maintained to the approved standard, and are available for use by the occupants, and other users of the development, in the interest of highway safety; to ensure a satisfactory appearance to the highways infrastructure serving the approved development; and to safeguard the visual amenities of the locality and users of the highway, in accordance with Policies of the Development Plan.

HP31

No development shall be commenced until details of the proposed arrangements for future management and maintenance of the proposed streets within the development have been submitted to and approved by the Local Planning Authority

Reason: To ensure that the future maintenance of the streets serving the development thereafter, are secured and shall be maintained by the Local Highway Authority under Section 38 of the Highways Act 1980 or via an established private management and maintenance company.

HP32

No development shall be commenced until full engineering, drainage, street lighting and constructional details of the streets proposed for adoption have been submitted to and approved in writing by the Local Planning Authority. The development shall, thereafter, be constructed in accordance with the approved details, unless otherwise agreed in writing with the Local Planning Authority.

Reason: In the interest of highway safety; to ensure a satisfactory appearance to the highways infrastructure serving the development; and to safeguard the visual amenities of the locality and users of the highway in accordance with Policies of the Development Plan.

Case Officer:

Date: 8 June 2018

Becky Melhuish

for Warren Peppard

Flood Risk & Development Manager

Comments received from LCC Highways regarding the submitted Transport Statement.

I have now had the opportunity to review the Transport Statement submitted in support of Phases 3 and 4 of the development served off Westbrooke Road, Lincoln.

Overall the contents of the Transport Statement are acceptable in that it provides a reasonable explanation of the likely impact on the surrounding highway network. There were a couple of transposing errors with the traffic count diagrams but I corrected for them in the analysis.

The site is already served with a reasonable level of sustainable infrastructure to allow occupants the opportunity to walk, cycle or use Public Transport to access the centre of the city in particular, albeit the car is still likely to be the major mode choice.

From a highway impact perspective they have assessed the impact on the Boultham Park Road/Western Avenue junction which is the only vehicular access to the development from the main highway network. The traffic counts were undertaken on a neutral day in February 2018 and are representative of the average traffic conditions in the AM and PM peak hours. They have considered those dwellings not yet occupied on the existing phases of development, future traffic growth up to 2023 and the new phases of development in their analysis.

The results indicate that there is predicted to be over 50% capacity left in the junction over the peak hours although at times during the peaks some moderate queuing (single figure) and delay can be expected due to the traffic levels on Boultham Park Road and also the proximity of the school on Western Avenue.

In conclusion this response is based not only upon sound and reasoned highway engineering principles but also with regard to a fundamental principle of paragraph 32 of the NPPF (National Planning Policy Framework) which is unequivocal about the presumption of approving development unless that development would be expected to cause a severe impact upon congestion. Reports within the industry of recent Planning Appeal decisions indicate that Planning Inspectors are robustly upholding that principle. The traffic generation from this development, in my view, cannot be considered to have a severe impact on the surrounding local highway network as described within the NPPF. Although it is acknowledged it will add to the cumulative impact on traffic at the Boultham Park Road/Western Avenue junction.

Hope this helps.

Kind Regards

Karl

Karl Gibson BSc (Hons) ~~Eng~~ FIHE

Principal Transportation Projects Officer

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Environment and Economy Directorate

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Application Number:	2018/0533/FUL
Site Address:	Land At 94 And 96 Newland (Taste Of Marrakesh) And No. 100, 102 And 104 Newland
Target Date:	17th August 2018
Agent Name:	Globe Consultants Ltd
Applicant Name:	Jackson and Jackson Developments
Proposal:	Erection of four storey building to the south of Nos. 100, 102 and 104 Newland to provide student accommodation (Sui Generis) and demolition of rear extension to Nos. 100, 102 and 104 Newland; Demolition of Nos. 94 and 96 Newland (Taste of Marrakesh) and erection of partial subterranean four and five storey building to provide student accommodation (Sui Generis); and, erection of partial subterranean building to provide four storeys of student accommodation (Sui Generis) between Nos. 96 and 100 Newland, including glazed link to No. 100 Newland (REVISED DESCRIPTION)(REVISED PLANS)

Background - Site Location

From east to west, the application site is occupied by a mix of buildings and parking. Moreover, to the east is the Taste of Marrakesh restaurant (Nos. 94-96 Newland) and to the west is Nos. 100-104 Newland and the car park to its rear. Between these is the forecourt to Viking House (No. 98 Newland).

The Taste of Marrakesh restaurant (Nos. 94-96 Newland) is a three-storey building incorporating residential accommodation above a restaurant. Viking House itself was previously occupied for office use by the Department for Work and Pensions. The forecourt area of the building was used as part of the access to the building to provide stepped and ramped access along the eastern side of the building.

Nos. 100-104 Newland (even only) are a terrace of former dwellings that are three-storeys in height. The building was most recently occupied as the offices of CAD Associates. The car park to the rear of the building is accessed from the southbound side of Carholme Road.

Meanwhile, in the context of the surrounding area, to the east is the Doubletree by Hilton hotel, which has recently been extended toward Newland. To the south is One The Brayford (formerly Mill House), which has also been extended to provide a mixture of commercial, office and residential uses. Furthermore, to the south and west is the Horse and Groom Public House and its car park.

The access to the buildings is currently a mixture of pedestrian access from Newland and vehicular access from Carholme Road.

Background – Description of Development

Extension to 98 Newland (Viking House)

In the first instance, it is important to acknowledge that Members considered a planning application at the Planning Committee on 23 May 2018 in relation to the original Viking House and 100-104 Newland buildings, as well as the erection of an extension to Viking House toward Newland. Members subsequently delegated authority to the Planning

Manager to finalise the approval of the application following the completion of public consultation and the drafting of planning conditions.

Notwithstanding this, since the Planning Committee, the applicant has carried out some exploratory work to determine the position of utilities. As a result of the position of a gas pipe, the applicant has again included the extension to Viking House, as the building may subsequently need to be moved 1.4m to the east should it not be possible to relocate the gas pipe. This would therefore provide the applicant with some flexibility.

As before, this element would be a red brick and slate roof extension towards Newland which would be partially subterranean as the ground floor from Viking House would continue towards the street. Moreover, the ground floor would be lower than the street and face a void created next to the footpath. Therefore, only three floors would be visible from the street. The top floor would be partially accommodated in the roof space of the building, which would be steeply pitched facing Newland with windows half below and half above the eaves level.

However, as a result of the changes, the single-storey building that would be positioned between this and No. 100 Newland would be slightly wider, this would provide a covered entrance to the reception, as well as a secure access to the courtyard to the rear; and the extension to Viking House would be inset from the north-western corner of the building.

Demolition of Taste of Marrakesh and Erection of New Building

The proposals to demolish the Taste of Marrakesh restaurant and replace it with a new building would incorporate a partial subterranean four and five storey building. The lowest floor would be below street level like that of the extension to Viking House. As such, only four of the five floors would be visible from the street. The top floor would be partially set back in a manner similar to the neighbouring hotel. The building would be accessed by a glazed stairwell adjacent to Newland at its frontage, which would serve the ground to third floors; and the fourth floor would be accessed by an independent stairwell due to this top floor being set back from Newland.

This element of the development would be self-contained and would incorporate 17 studio apartments, each with their own cooking facilities. These either face Newland or towards the rear of the site of the building.

Erection of New Building to Rear of Nos. 100-104 Newland

Finally, the largest part of this application would be for the erection of a four storey building that would be positioned to the south of Nos. 100-104 Newland, this would be accessed by a glazed link building that would be positioned on the rear wall of Nos. 100-104, which would be facilitated by the removal of a rear projection. The building would then follow the boundary of the site with the adjacent public house car park toward One The Brayford in roughly an L-shape. This would enclose the courtyard between the new building, 98 and 100-104 Newland.

This building would be accessed by stair and lift access and serve clusters bedrooms arranged around shared accommodation. This would be in clusters of eight on the ground floor and ten on the floors above, totalling 76 bedrooms (4 of which would be adapted for disabled occupants).

Meanwhile, the plant servicing the building and the refuse storage for the wider development would be sited within the building at ground floor.

Site History

Reference:	Description	Status	Decision Date:
2018/0531/FUL	Change of Use of Viking House to student accommodation (Sui Generis) alongside external alterations; Erection of partial subterranean building to provide four storeys of student accommodation (Sui Generis) between No. 96 and No. 100 Newland including glazed link to No. 100 Newland; Change of Use of No. 100, 102 and 104 Newland to student accommodation (Sui Generis); and associated hard and soft landscaping works including creation of internal courtyard.	**Pending Issue of the Decision**	
2018/0221/FUL	External alterations to existing building to include re-cladding and the provision of balconies to north elevation.	Granted Conditionally	1st May 2018
2016/1222/PAC	Determination as to whether prior approval is required for the change of use from existing office (Use Class B1(a)) to 40no. apartments (Use Class C3)	Prior Approval Required and Approved With Conditions	27th January 2017

Case Officer Site Visit

Undertaken on 24th May 2018.

Policies Referred to

- Policy LP1 A Presumption in Favour of Sustainable Development
- Policy LP2 The Spatial Strategy and Settlement Hierarchy
- Policy LP3 Level and Distribution of Growth

- Policy LP5 Delivering Prosperity and Jobs
- Policy LP13 Accessibility and Transport
- Policy LP14 Managing Water Resources and Flood Risk
- Policy LP16 Development on Land affected by Contamination
- Policy LP17 Landscape, Townscape and Views
- Policy LP25 The Historic Environment
- Policy LP26 Design and Amenity
- Policy LP29 Protecting Lincoln's Setting and Character
- Policy LP31 Lincoln's Economy
- Policy LP33 Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area
- Policy LP36 Access and Movement within the Lincoln Area
- Policy LP37 Sub-division and multi-occupation of dwellings within Lincoln
- National Planning Policy Framework

Issues

In this instance the main issues relevant to the consideration of the application are as follows:

1. The Principle of the Development;
2. The Impact of the Design of the Proposals;
3. The Implications of the Proposals upon Amenity;
4. Sustainable Access, Highway Safety and Air Quality
5. Other Matters; and
6. The Planning Balance.

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

The wider public consultation period for the application does not expire until 21 June 2018, this is the day following the Planning Committee Meeting. The timing is due to the date of the Press Notice. Meanwhile, the Site Notice consultation date expires on 14 June 2018 and consultations with consultees run until 15 June 2018.

In terms of direct consultations undertaken with neighbours, the majority of consultations expire on 13 June 2018. However, officers undertook further consultation with 14 apartments within One The Brayford which face out towards the site. This was undertaken on 31 May 2018 and expires on 23 June 2018. The applicant has confirmed that they currently retain the ownership of seven of these apartments (one of the two apartments on floor 3; both apartments on floor 5; and all four apartments on Floor 6).

In addition, the applicant has also asked that Members are made aware of the consultation that they undertook independently of the Council with all of the owners/occupants within the One The Brayford building, as well as potential purchasers of apartments. A copy of this letter is therefore attached to this report (Appendix A).

Any responses received as part of the consultation process prior to the closing of the agenda are copied in full as part of the agenda. All subsequent correspondence received as part of the consultation will be added to the update sheet or reported directly at the planning

committee if appropriate.

Statutory Consultation Responses

Consultee	Comment
Lincolnshire Police	Comments Received
Highways & Planning	Comments Received
Anglian Water	Response Awaited
Lincoln Civic Trust	No Objection
West End Residents Association	Response Awaited
Environment Agency	Response Awaited
Upper Witham, Witham First District & Witham Third District	Response Awaited

Public Consultation Responses

No responses received to date by virtue of all the public consultation undertaken.

Consideration

1) The Principle of the Development

a) *Relevant Planning Policies*

i) *Sustainable Development and the Proposed Uses*

Section 38(6) of the Planning and Compulsory Purchase Act 2004 and S70(2) of the Town and Country Planning Act 1990 requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The National Planning Policy Framework (the Framework) is a material consideration in determining planning applications. Framework paragraph 215 indicates that due weight should be given to relevant policies in the development plan according to their consistency with the Framework i.e. the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given.

The development plan comprises the adopted Central Lincolnshire Local Plan (the Plan). During its examination the policies therein were tested for their compliance with the

Framework and ensures that there is a very clear picture of the options for growth in Central Lincolnshire.

Paragraph 14 of the National Planning Policy Framework (the Framework) outlines the following in relation to the principle of development:

"At the heart of the National Planning Policy Framework is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan making and decision taking.

For decision taking this means (unless material considerations indicate otherwise):

- approving development proposals that accord with the development plan without delay; and
- where the development plan is absent, silent or relevant policies are out of date, granting permission unless:
 - any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole; or
 - specific policies in this Framework indicate development should be restricted.

In terms of sustainable development, Paragraph 7 of the Framework suggests that there are three dimensions: economic, social and environmental. "These dimensions give rise to the need for the planning system to perform a number of roles:

- **an economic role** – contributing to building a strong, responsive and competitive economy, by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth and innovation; and by identifying and coordinating development requirements, including the provision of infrastructure;
- **a social role** – supporting strong, vibrant and healthy communities, by providing the supply of housing required to meet the needs of present and future generations; and by creating a high quality built environment, with accessible local services that reflect the community's needs and support its health, social and cultural well-being; and
- **an environmental role** – contributing to protecting and enhancing our natural, built and historic environment; and, as part of this, helping to improve biodiversity, use natural resources prudently, minimise waste and pollution, and mitigate and adapt to climate change including moving to a low carbon economy."

Meanwhile, at the heart of the Core Planning Principles within the Framework (Paragraph 17) is the expectation that planning should:-

"proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs. Every effort should be made objectively to identify and then meet the housing, business and other development needs of an area, and respond positively to wider opportunities for growth"

Turning to Local Plan Policy, Policy LP1 of the Plan supports this approach and advocates that proposals that accord with the Plan should be approved, unless material considerations indicate otherwise.

In terms of the spatial dimension of sustainability, proposals need to demonstrate that they contribute to the creation of a strong, cohesive and inclusive community, making use of previously developed land and enable larger numbers of people to access jobs, services and facilities locally, whilst not affecting the delivery of allocated sites and strengthening the role of Lincoln (Policy LP2). Meanwhile, Policy LP3 sets out how growth would be prioritised and Lincoln is the main focus for urban regeneration; and Policy LP5 supports the growth of job creating development which also supports economic prosperity but only where proposals have considered suitable allocated sites or buildings or within the built up area of the settlement; and the scale of what is proposed is commensurate with its location.

In more broader spatial terms, Policy LP33 sets out the general mix of uses that would be supported within the Central Mixed Use Area, including shops (A1); offices used by the public (A2); Food and Drink Outlets (A3, A4 and A5); houses and flats (C3); hotels (C1); student halls of residence and theatres.

b) Assessment of the Principle of the Proposed Use

The proposals are for further purpose built student accommodation within the grounds of Nos. 100-104 Newland and in the form of a replacement for the Taste of Marrakesh restaurant. Meanwhile, the building recently consented as an extension to Viking House is amended due to the footprint moving slightly to the east to avoid utilities beneath the site.

As with the application for the first phase of the development of the wider site, officers appreciate that the incorporation of additional student accommodation would be an emotive issue, particularly as there were concerns with that application that students could have a negative impact upon the locality. However, as alluded to in the relevant policies above, the incorporation of student housing within the redevelopment of the site is an appropriate use, as the site is located within the Central Mixed Use Area where such uses are acceptable.

Furthermore, Members may recall that the previous officer report and discussion at the Planning Committee in May outlined that there is now no requirement within Local Plan Policy for developers to evidence a need or demand for student accommodation. Similarly, the future use of the buildings and the risks associated with non-occupation by students are also points that are not currently addressed by the policies of the Local Plan. As such, it would be difficult to insist that the developer addresses the future implications of potential changes of use. However, the trajectory of the growth of the universities in the city would point to a strong market for student accommodation so such risks should be low. Notwithstanding this, it would still be necessary to consider the implications of the proposals, which will be dealt with elsewhere in the report.

Meanwhile, in terms of the city-wide impact of student accommodation, it is a valid argument that the provision of managed purpose-built student accommodation could have a positive impact upon the social imbalance within nearby residential areas, i.e. the proposals could make a positive impact upon the demand for student housing in those areas. Moreover, the demand for houses in multiple occupation could reduce thereby facilitating a return of dwellings to family occupation. Notwithstanding this, it is clear that the site is sustainably located in the heart of the city, close to the facilities and services that would support this use and the Universities in the city are accessible by cycle and walking routes. This ensures that this form of residential accommodation would be appropriate in this location.

In terms of the sustainability dimensions of the development, officers recognise that the development would deliver economic and social sustainability directly through the

construction of the development. There would also be indirect benefits through the occupation of the student accommodation and the potential spend of occupiers in the City, as well as the retention/creation of other jobs due to the location of the development within the City.

Overall, the erection of development in this location would not in itself undermine sustainable principles of development subject to other matters referred to in the relevant policies, so it is important to consider the wider sustainability of the development.

2) The Impact of the Design of the Proposals

a) *Relevant Planning Policy*

So far as this issue is concerned, as alluded to above, the proposals must achieve sustainable development and it is the social dimension of sustainability that relates to design. Moreover, Paragraph 7 of the Framework requires the creation of high quality built environment. In addition, the policy principles outlined in Paragraphs 17, 58, 60, 61 and 64 of the Framework also apply. Moreover, the Framework states that good design is a key aspect of sustainable development and is indivisible from good planning. Design is to contribute positively to making places better for people (para. 56). To accomplish this development is to establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live and responding to local character and history (para. 58). It is also proper to seek to promote or reinforce local distinctiveness (para. 60).

At the local level, the Council, in partnership with English Heritage, have undertaken the Lincoln Townscape Appraisal (the LTA), which has resulted in the systematic identification of 105 separate “character areas” within the City. The application site lies within the ‘*Newland*’ and ‘*Brayford*’ Character Areas. Policy LP29 refers to the LTA and requires that developments should “protect the dominance and approach views of Lincoln Cathedral, Lincoln Castle and uphill Lincoln on the skyline”. This policy is supported by Policy LP17, which is relevant to the protection of views and suggests that:-

“All development proposals should take account of views in to, out of and within development areas: schemes should be designed (through considerate development, layout and design) to preserve or enhance key local views and vistas, and create new public views where possible. Particular consideration should be given to views of significant buildings and views within landscapes which are more sensitive to change due to their open, exposed nature and extensive intervisibility from various viewpoints.”

Policy LP26 refers to design in wider terms and requires that “all development, including extensions and alterations to existing buildings, must achieve high quality sustainable design that contributes positively to local character, landscape and townscape, and supports diversity, equality and access for all.” The policy includes 12 detailed and diverse principles which should be assessed. This policy is supported by Policy LP31, which refers to the protection and enhancement of the character of the city.

In terms of the wider impacts upon built heritage, Policy LP29 also requires that “proposals within, adjoining or affecting the setting of the 11 Conservation Areas and 3 historic parks and gardens within the built up area of Lincoln, should preserve and enhance their special character, setting, appearance and respecting their special historic and architectural context”; and “protect, conserve and, where appropriate, enhance heritage assets, key

landmarks and their settings and their contribution to local distinctiveness and sense of place, including through sensitive development and environmental improvements”.

Meanwhile, conservation is enshrined in the Core Planning Principles of the Framework (Paragraph 17) as planning is expected to “conserve heritage assets in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of this and future generations”. In addition, Section 12 of the Framework also refers to the impacts of development upon designated heritage assets and is supported by Policy LP25 also applies as it specifically refers to the impacts of developments upon these assets. In terms of conservation areas, the policy requires that development should either enhance or reinforce features that contribute positively to the area’s character, appearance and setting. Meanwhile, proposals also need to have regard to the setting of other designated assets, including listed buildings.

b) Assessment of the Implications of the Proposals

i) The Site Context and Submission

The application site is contained within the West Parade and Brayford Conservation Area and is considered to have the potential to affect views into and within the Area. As such, the visual implications of the proposals for the site are key to the assimilation of development into its context and the creation of high quality built environment. As part of the pre-application process, officers have worked with the applicant in order to ensure that the visual appearance of the development would not have a detrimental impact upon townscape.

As referred to in the relevant site history, this application is one of two applications for two independent phases of development for the land occupied by Viking House and Nos. 100-104 Newland. Moreover, this second application has been prepared to deal with the building to the rear of Nos. 100-104 within its car park, which would wrap around the edge of the site boundary to the car park to the Horse and Groom public house toward One The Brayford. This would provide an expanded courtyard (larger than that which was shown in the first application) and encompass refuse storage to the southern end of the building.

Both phases of the development are shown in the images on the following pages.



The application buildings included in this phase of development are to the left and rear of the three storey building situated at the corner of Newland and Carholme Road. Moreover, whilst this image has not been updated to show the slightly revised siting of the proposed extension to Viking House toward Newland, the impact in this view would be very similar. Meanwhile to the east of this, a white building with a recessed top floor is shown to replace the Taste of Marrakesh restaurant. The stepped appearance would align with the approach to the massing of the extension to the Doubletree by Hilton hotel. Furthermore, to the right at the rear of 100-104 Newland is a further building proposed, which is shown in greater detail below.



This image is a view northwest across Carholme Road towards the rear of 100-104 Newland and the four storey building proposed as part of this second phase of the development of the site. Viking House is visible above the building and the building joins to Nos 100-104 with a lower glazed building, which replaces the rear projection to the building that staggers down along the Carholme Road frontage.

ii) The Impact of the Development in its Context

The existing taste of Marrakesh building has unfortunately been unsympathetically altered over time and its fabric compromised as a result. Consequently, it does not make a positive contribution to the character and appearance of the Conservation Area and weakens the street frontage due to its presence at the footpath edge.

The architecture of the replacement building is simple in its composition but maintains the rhythm and balance of windows and façade found elsewhere along Newland and would be a suitable transition between the extensions to Viking House and the Doubletree. Moreover, the architectural solution would have the individuality to ensure that it aligns with the approach taken over time with different buildings. The step in the scale of the building will also ensure that the building will sit comfortably in wider views of the street and roofscape.

Meanwhile, the proposals to the rear of Nos. 100-104 Newland would be of a scale similar to that of the existing building and would begin to fill the gap between that building and the adjacent public house whilst also providing an appropriate transition up to the much taller Viking House building, which would soften the visual implications of this taller building. The alignment of the building is not necessarily commonplace within the locality but this does not mean that it would not be an appropriate intervention. Rather, the use of red brick façades and a strong roof form of darker materials would ensure that this building assimilates appropriately with the other surrounding buildings and wider context.

Both of the new buildings on Newland would accommodate rooms below street level in a similar manner to the listed terrace to the east, so this would not be an alien approach to the accommodation of buildings in the locality. What is more, the inclusion of glazed stairwells and entrance building between the new and existing buildings would help to provide a break between the different approaches to the architecture of the façades of the buildings within the site.

c) Summary in Relation to this Issue

It is accepted that the introduction of a new building between Viking House and Carholme Road would undoubtedly make a significant contribution to townscape and the grain of development. However, the development would help to provide a suitable step up to that building with the result that it would soften its impact in its context. What is more, the other new buildings would be complimentary to the existing form and scale of buildings and would be detailed to ensure that they have sufficient prominence. Therefore, it is considered that the development would enhance the character and appearance of the Conservation Area, as the proposals would regenerate Newland and the important spaces around the prominent Viking House building with a high quality development that would integrate with the surrounding townscape that contributes to the valued character and appearance of the conservation area.

Consequently, officers would advise Members that the proposal satisfies the duty contained within section 72 (1) of the Planning (Listed Buildings and Conservation Areas Act) 1990 'In the exercise, with respect to any buildings or other land in a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area'. Furthermore, the proposals would be in accordance with the guidance contained within paragraph 137 of the NPPF which advises that Local Planning Authorities should look for new development within a Conservation Area and within the setting of heritage assets to reveal or better enhance significance.

3) Implications of the Proposals upon Amenity

a) *Relevant Planning Policy*

In terms of national policy, the NPPF suggests that development that results in poor design and/or impacts upon the quality of peoples' lives would not amount to sustainable development. Consequently, the implications of both are key to the consideration of the acceptability of the principle of development within a given site. Moreover, the Framework (Paragraph 9) sees "seeking positive improvements in the quality of the built, natural and historic environment, as well as in people's quality of life" as being important to the delivery of sustainable development, through "replacing poor design with better design" and "improving the conditions in which people live" amongst others. Furthermore, the core principles of the Framework (Paragraph 17) indicate that "planning should...always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings".

Policy LP26 of the Plan deals with design and amenity. The latter refers to the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy and suggests that these must not be unduly harmed by, or as a result of, the development. There are nine specific criteria which must be considered. The policy is in line with the policy principles outlined in Paragraphs 17, 59 and 123 of the NPPF. Indeed, Paragraph 123 of the Framework suggests that "decisions should aim to...avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development".

b) *Assessment of the Implications of the Proposals*

As there are existing buildings adjacent to the proposed accommodation, it is important to establish whether there would be any harmful impacts associated with the proposals in connection with these; and whether the proposals would be appropriate for future occupants.

i) *Impacts of Overlooking / Loss of Privacy*

In terms of the apartments in One The Brayford that face north, it is proposed that there would not be any directly facing windows in the development that could not be obscure glazed to ensure that there would not be a loss of privacy. Similarly, the elevations of the hotel that face north and west are either blank or incorporate obscure glazing (this was in order to protect the original Taste of Marrakesh building).

The greatest impact would undoubtedly be between the facing windows of the student accommodation but, given that the development is owned / managed by one party, it is considered that the relationship would not be unreasonable. The same could not be said for independent residential accommodation where a greater degree of protection would be expected. However, the proposals would not promote direct facing windows between the student accommodation and the apartments in One The Brayford so there would not be a harmful impact upon privacy.

ii) *Impacts of the Scale of the Building and Outlook*

The dense urban context within which the area is situated would mean that one would not necessarily expect the same degree of protection of amenity in this context as in a suburban

context. Notwithstanding this, it is clear that the occupants of a number of the apartments within the lower levels of One The Brayford would face towards the southern façade of the building proposed to the rear of Nos. 100-104 Newland, whilst others would look over the top of the building so would not be affected.

Moreover, whilst there would be some overshadowing and loss of light, as well as implications upon outlook resulting from the close proximity of two developments, it is important to acknowledge that the properties in One The Brayford currently face north. Given this orientation, very much upon balance, it is considered that the change in circumstances with respect to loss of light and overshadowing would not be so significantly different to the existing situation to be harmful to the occupants of those properties. Likewise, with respect to the outlook from those apartments, officers are mindful that the site is situated within a more densely spaced context, as such, the expectations of occupants would not necessarily be the same as within a suburban or looser grain of development.

Notwithstanding this, as alluded to in the 'Consultation' section of this report, the applicant has confirmed that they currently retain the ownership of 7 of the 14 apartments located in the north facing façade of the building (one of the two apartments on floor 3; both apartments on floor 5; and all four apartments on Floor 6). As such, those properties remain empty and any potential purchasers would be aware of the proposals were they to be consented. However, the consultation section also refers to the letter sent by the applicants to all residents and those in discussions to purchase apartments regarding the proposals.

Whilst the lack of objections or comments in relation to the scheme from occupants of the remaining properties should not necessarily be seen as support for the development, extensive consultation has been undertaken for the application and no such responses have been received to date. Consequently, very much on balance, having regard to the context referred to above, officers would not raise objections to the proposals upon the grounds of the impacts upon those properties.

Meanwhile, in terms of the other adjoining developments, given that the new building within the site of the Taste of Marrakesh would not result in a material change in the manner in which accommodation adjoins the extension to the Doubletree by Hilton hotel or Viking House, it is considered that there would not be any harmful impacts upon the future occupants of the existing and proposed buildings. Furthermore, in terms of the outlook from the subterranean areas of the new buildings, this would not be significantly different from the accommodation contained within the listed terrace to the east of the site so it would be difficult to argue that this approach to residential accommodation would not be appropriate.

iii) Noise from the Proposed Use

Due to the large number of occupants that the proposed development could accommodate, there is potential for comings and goings associated with student accommodation to impact upon the residents of the adjacent apartments, particularly at unsociable hours. However, as with the remainder of this developments and others in the city, this potential issue could be mitigated by implementing an appropriate building-wide management plan, such as a 24 hour concierge serving the main entrance/reception. This could be controlled by a suitable condition requiring that details of a management plan be submitted prior to occupation.

At this time, it is not possible to be sure what plant and machinery may be required for the proposed use, particularly in the context of the mitigation of noise and/or air pollution from nearby roads. However, these matters can be controlled through the use of planning

conditions, alongside controls over the collection hours for refuse and the construction working hours for the development.

iv) External Lighting

As the site is close to residential properties, any lighting used to illuminate the building or its entrances may have an impact upon those residents. It is therefore important that this is appropriately designed not to have a detrimental impact upon neighbouring properties. It is therefore recommended that an appropriate scheme of lighting is controlled by planning condition.

c) The Planning Balance

Taking all the above in to account, it is considered that the proposed development of the site could be accommodated in a manner that would not cause unacceptable harm in respect of the protection of amenity. Moreover, with satisfactory controls over the mitigation employed in relation to noise (including from plant and machinery), servicing / working and external lighting, the proposals would be socially and environmentally sustainable in the context of the Framework and would accord with the policies in the Local Plan.

4) Sustainable Access, Highway Safety and Air Quality

a) Relevant Planning Policies

i) Sustainable Access and Highway Safety

The impacts of growth are enshrined in the Core Planning Principles of the Framework (Paragraph 17), which expects planning to actively manage this growth “to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable”. As such, Paragraph 35 requires that: “developments should be located and designed where practical to [amongst other things] give priority to pedestrian and cycle movements, and have access to high quality public transport facilities; and should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones”.

A number of Local Plan Policies are relevant to the access, parking and highway design of proposals. In particular, the key points of Policy LP13 are that “all developments should demonstrate, where appropriate, that they have had regard to the following criteria:

- a) Located where travel can be minimised and the use of sustainable transport modes maximised;
- b) Minimise additional travel demand through the use of measures such as travel planning, safe and convenient public transport, walking and cycling links and integration with existing infrastructure;
- c) Should provide well designed, safe and convenient access for all, giving priority to the needs of pedestrians, cyclists, people with impaired mobility and users of public transport by providing a network of pedestrian and cycle routes and green corridors, linking to existing routes where opportunities exist, that give easy access and permeability to adjacent areas”

There are also transport measures referred to in Policy LP36, which more specifically refers to development in the 'Lincoln Area', the key measures add to and reinforce the criteria within Policies LP5 and LP13. As such, they are intended to reduce the impact upon the local highway network and improve opportunities for modal shift away from the private car. In particular, development should support the East West Link in order to reduce congestion, improve air quality and encourage regeneration; and improve connectivity by means of transport other than the car. Similarly, Policy LP33 also requires that developments do not result in "levels of traffic or on-street parking which would cause either road safety or amenity problems." Moreover, the policy also highlights the importance of providing appropriate parking for vehicles and cycles for all users within developments; and that walking and cycling links are maintained and promoted.

Paragraph 32 of the Framework suggests that the residual cumulative impacts of the development would need to be severe for proposals to warrant refusal. This is reinforced by Policy LP13 of the Local Plan which suggests that only proposals that would have "severe transport implications will not be granted planning permission unless deliverable mitigation measures have been identified, and arrangements secured for their implementation, which will make the development acceptable in transport terms."

ii) Air Quality

Paragraph 109 of the NPPF introduces the section in relation to the conservation and enhancement of the natural environment. Given that the site is located adjacent within the Air Quality Management Areas (declared by the Council due to the likely exceedance of the national air quality objectives for nitrogen dioxide and particulate matter), this section of the NPPF should be given great weight. It states that "the planning system should contribute to and enhance the natural and local environment by...preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of soil, air, water or noise pollution or land instability".

Paragraph 120 sets the scene and refers to development being "appropriate for its location". It goes on to say that "the effects (including cumulative effects) of pollution on health, the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account." Paragraph 124 refers in more detail to the implications of the location of development within an Air Quality Management Area and requires that "planning decisions should ensure that any new development in Air Quality Management Areas is consistent with the local air quality action plan".

Meanwhile, Local Plan Policy LP13 also refers to air quality and requires that "all developments should demonstrate, where appropriate, that they...ensure allowance is made for low and ultra-low emission vehicle refuelling infrastructure."

b) Sustainable Access and Highway Safety

It is important to consider the implications of the proposals upon the highway network from the perspective of access, safety and traffic capacity. In this instance, the proposals have been discussed with the Highway Authority prior to the submission of the application. Whilst the application would result in the removal of parking spaces currently provided to the rear of Nos. 100-104 Newland, the Highway Authority has historically not requested parking needs to be provided as part of student residential schemes. In this instance, given the proximity of the development to the University of Lincoln and the city centre, it is ideally

located adjacent to Newland to be accessible on foot and by bicycle. Meanwhile, in terms of student arrivals, the building can also be reached conveniently by public transport as it is not far from the bus and railway stations and there are public car parks nearby.

In terms of the immediate impacts of the proposals upon the highway, officers are satisfied that it would be possible to provide safe and secure access for refuse collections through the One The Brayford car park to the south of the site, which can be controlled by planning condition. In addition, it is also important to note that the proposals would result in a positive impact upon highway safety as the vehicular access/egress to the Carholme Road frontage would be closed off as part of the proposals as it would be blocked by the extension to the rear of Nos. 100-104 Newland.

The Highway Authority has provided their formal response to the application as part of a dual response to the application for Phase 1 of the development. This includes reference to the surface water drainage scheme for the site as well as the closing up of an access to Newland. However, this would also need to include the access to the car park from Carholme Road, as this would also no longer be in use.

Consequently, as the Highway Authority's response does not raise any concerns regarding the implications upon traffic capacity, parking or highway safety, officers consider that it would be difficult to raise concerns regarding the development. Moreover, there would not be conflict with Paragraph 32 of the Framework.

c) *Air Quality*

Whilst there has been no specific supplementary planning guidance produced in relation to air quality, the quality of air throughout the city has been monitored, and the clear goal of the City's action plan is to improve air quality.

The site was used relatively recently for office purposes in connection with the parking at the rear but the proposals, meanwhile, would result in a car-free scheme and thereby the removal of directly linked daily trips. As such, the redevelopment would lead to a direct reduction in the impact upon the city's air quality.

Notwithstanding this, as alluded to above, the occupants of the development could still be effected by the air quality in the vicinity of the site and the applicant is aware that it may be necessary to provide mitigation of the rooms with a close relationship with the roads adjacent to the site. The applicant is currently undertaking their own air quality analysis in order to establish what mitigation, if any, would be required. This would need to be designed in accordance with any scheme to address the implications of noise from the highway.

Consequently, this matter would not, in itself, result in insurmountable issues that would warrant the refusal of the application.

5) Other Matters

a) *Archaeological Implications of the Development of the Site*

i) *Relevant Planning Policies*

The Framework and Planning Practice Guide as well as good practice advice notes produced by Historic England on behalf of the Historic Environment Forum including

Managing Significance in Decision-Taking in the Historic Environment and *The Setting of Heritage Assets* are relevant to the consideration of Planning Applications.

Indeed, heritage is referred to within the core principles of the Framework (Paragraph 17) and Paragraph 128 of the Framework states that “in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.”

Paragraph 141 of the Framework states that LPAs should ‘require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible.’

Policy LP25 in the Central Lincolnshire Local Plan requires that development does not lead to significant detrimental impacts on heritage assets. This issue is directed in relation to archaeology that could be non-designated heritage assets.

ii) Assessment of the Implications of the Proposals

The proposals have been the subject of pre-application and in-application discussion regarding the archaeological implications of the proposals. The applicant has carried out some evaluation but will ultimately need to carry out further evaluation to inform the foundation design. This is currently being negotiated with officers in order to establish whether this can be utilised as a preservation strategy. However, officers are still in discussions with the applicant in relation to the final design of the foundations for the building and the nature of further evaluation and mitigation to be undertaken.

Consequently, officers will provide Members with an update in relation to the progress made since the completion of this report on the update sheet and whether the proposals would fulfil the requirements of both National and Local planning policy.

b) Land Contamination

i) Relevant Planning Policy

As with air quality, Paragraph 109 of the Framework also refers to contamination. Paragraph 120 expands upon this and suggests that “to prevent unacceptable risks from pollution and land instability, planning policies and decisions should ensure that new development is appropriate for its location. The effects (including cumulative effects) of pollution on health, the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account. Where a site is affected by contamination or land stability issues, responsibility for securing a safe development rests with the developer and/or landowner.”

In addition Paragraph 121 states that planning decisions “should also ensure that:

- the site is suitable for its new use taking account of ground conditions and land instability, including from natural hazards or former activities such as mining, pollution arising from previous uses and any proposals for mitigation including land remediation or impacts on the natural environment arising from that remediation;
- after remediation, as a minimum, land should not be capable of being determined as contaminated land under Part IIA of the Environmental Protection Act 1990; and
- adequate site investigation information, prepared by a competent person, is presented.”

In terms of Local Plan policies, given the location of the site, Policy LP16 directly refers to the requirements of development in relation to contaminated land.

ii) Assessment of the Implications of the Proposals

The application is not supported by a report into contamination but the applicant has been in dialogue with the Council’s Scientific Officer and is hoping to reduce the need for the pre-commencement planning conditions requested. If it is ultimately not possible to address this matter prior to the Planning Committee, officers will clarify at the meeting whether planning conditions would need to be imposed.

c) Site Drainage

Policy LP14 of the Local Plan deals with foul and surface water disposal. This links closely to the Framework, which deals with flooding at Paragraph 103.

Whilst it is noted that the Drainage Board objects to this application, the response from Lincolnshire County Council (as Lead Local Flood Authority) has confirmed that the surface water drainage principles for the site would be acceptable subject to the final design of the scheme to serve the site. Furthermore, as part of the Phase 1 application, Anglian Water suggested that it would be necessary to provide further details to satisfy them regarding the surface water drainage scheme for the development. Meanwhile, in terms of foul drainage they were content that there is capacity within the current system to accept the flows from the development. The comments of Anglian Water in relation to this phase are awaited and if they report any changes in circumstances with regard to the response provided to the first phase, this will be reported to Members on the update sheet.

In addition, as with Phase 1 of the development, the direct mitigation within the site to address the potential implications from surface water flooding upon the use of the buildings would be proportionate.

6) Planning Balance

Paragraph 14 of the Framework sets out a presumption in favour of sustainable development which for decision taking means that where relevant policies of the development plan are out-of-date planning permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against policies in the Framework, taken as a whole; or specific Framework policies indicate development should be restricted. There are no restrictive policies that would lead to the proposals not being sustainable. However, a conclusion whether a development is sustainable is a decision that has to be taken in the round having regard to all of the dimensions that go to constitute sustainable development.

In this case, officers consider that the principle of the development of the use proposed within the existing and proposed buildings within the site would be acceptable and the development would deliver economic and social sustainability directly through the construction of the development and the uses proposed therein; and indirectly through the occupation of the buildings. Moreover, the provision of additional purpose-built student bed spaces available in a location relatively close to both universities in the city should hopefully reduce the dependency further upon houses in multiple occupation, which would in turn also improve environmental sustainability.

It is clear from the main body of the report that the proposed building would have some impacts upon amenities of the occupants of One the Brayford but these are not considered to be so harmful to warrant refusal of the application or to unnecessarily restrict development. As such, at the time of writing this report, with suitable schemes to deal with drainage, contamination, noise, air quality, site lighting and particularly archaeology, the development would be environmentally sustainable.

In this instance, in light of all of the above, officers would advise Members that the planning balance should fall in favour of the proposals as long term enhancement would be brought to the conservation area, as well as potential stimulus to the wider enhancement of historic townscape. This is particularly important given that Newland and Carholme Road are key routes into the heart of the city.

Thus, assessing the development as a whole in relation to its economic, social and environmental dimensions and benefits, officers are satisfied that the benefits of developing this site would, in the long-term, be more important than the potential impacts of not doing so. As such, it is considered that the proposal could be sustainable development and would accord with the Local Plan and Framework, sufficient for the recommendation of officers to be that planning permission should be granted subject to planning conditions.

Application Negotiated either at Pre-Application or during Process of Application

Yes, in respect of numerous matters as referred to in the application.

Financial Implications

The proposals would offer benefits to economic and social sustainability through spend by new and existing students, jobs created/sustained through construction and the operation of the development respectively.

Legal Implications

None.

Equality Implications

None.

Conclusion

The presumption in favour of sustainable development required by the National Planning Policy Framework would apply to the proposals as there would not be conflict with the

Framework in respect of sustainability that would apply to development as set out in the planning balance. It is the conclusion of officers and therefore the recommendation to Members that there would not be harm caused by approving the development so the application should benefit from planning permission for the reasons identified in the report and be subject to the conditions outlined below.

However, if any new material planning considerations have been raised within correspondence received following the writing of this report which would lead to a different conclusion being reached or which would require further consideration and/or planning conditions, officers will provide members with a detailed response on the Update Sheet. This will have regard to any further consultation responses received in the timeframe from the agenda being published and the date of the planning committee, or these will be reported directly at the planning committee if appropriate.

Application Determined within Target Date

Yes.

Recommendation

As the overall public consultation period for the application does not expire until 23 June 2018, it is the recommendation of officers that authority is delegated to the Planning Manager to issue planning permission subject to the planning conditions listed below. However, should there be any further material planning considerations raised (within correspondence received following the Planning Committee agenda being published) that have not already been considered in this report or that could not be addressed by existing or additional planning conditions, the application will be referred back to the next available Planning Committee for the consideration of Members.

Standard Conditions

Timeframe of the planning permission
Approved Plans

Conditions to be discharged before commencement of works

Materials Schedule and Detailed Plans (Windows etc.)
Contaminated Land Remediation
Archaeology
Site Drainage
Air Quality and Noise Mitigation (including design of plant and machinery)

Conditions to be discharged before use is implemented

Closing of Accesses to Newland and Carholme Road
Building-wide Management Plan
Scheme of Landscaping
Refuse Storage and Servicing

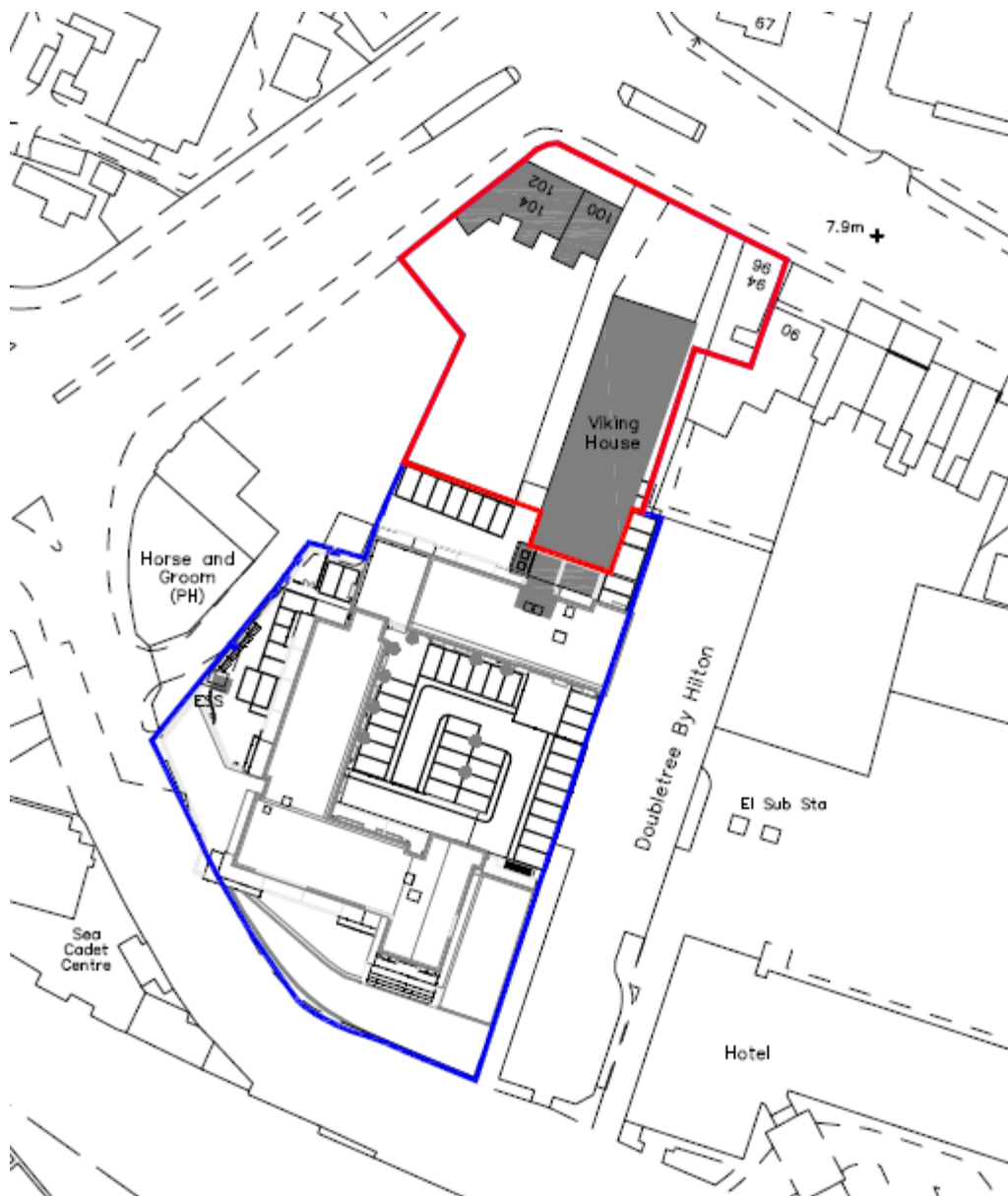
Conditions to be adhered to at all times

Construction Working Hours and Deliveries

Scheme of External Site Lighting

Report by Planning Manager

Plans



Site Location Plan



Ground Floor Plan in Context



Second Floor Plan in Context



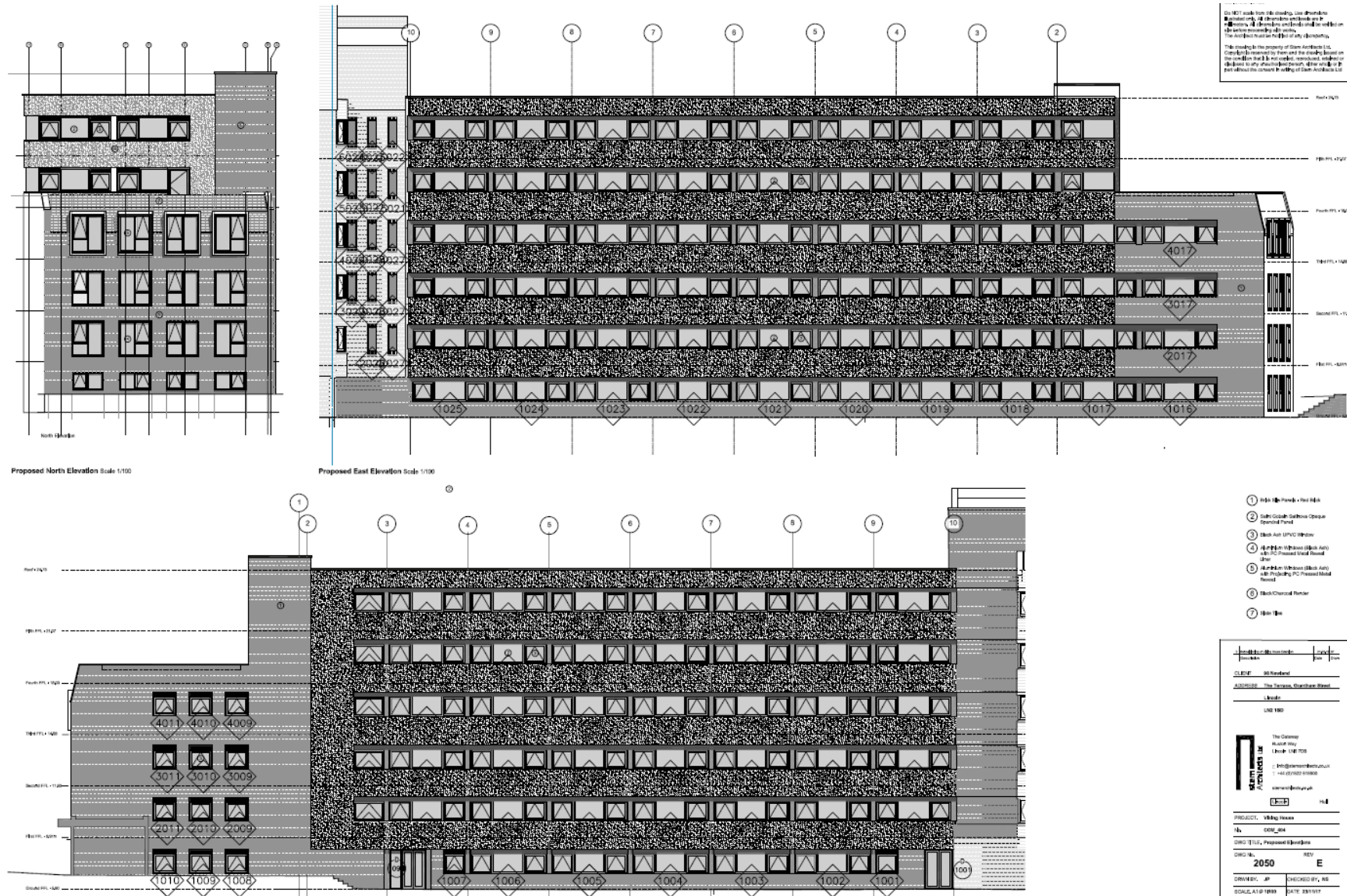
Fifth Floor Plan in Context



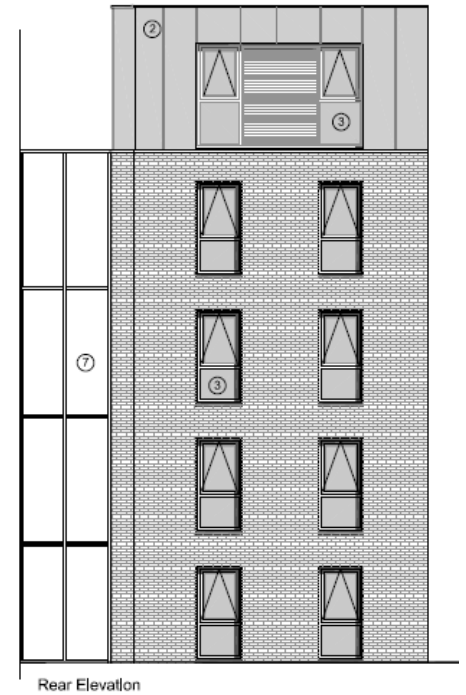
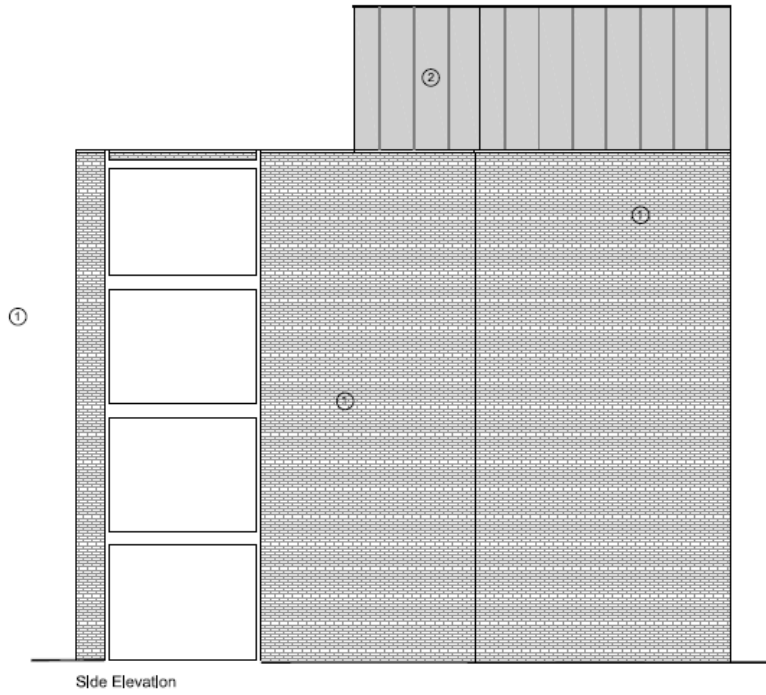
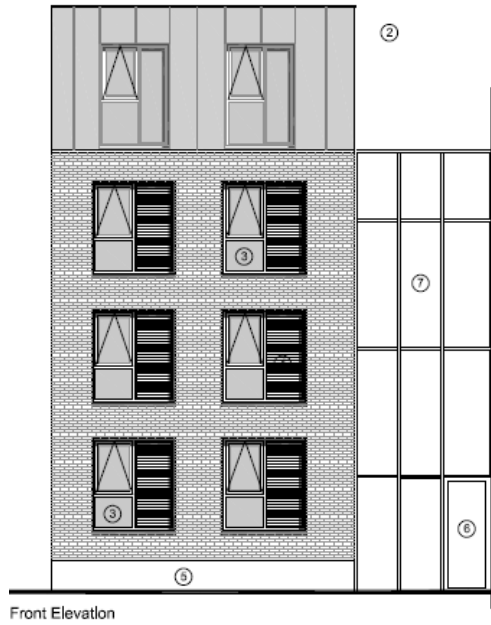
Extension to Rear of Former CAD Building (100-104 Newland)



Newland Frontages in Context of the Street



Viking House Elevations



New Building on Site of Taste of Marrakesh Restaurant

Site Photographs









Appendix A: Letter from the Applicant to Owners / Occupiers of Residents of One The Brayford



1st May 2018

Dear Resident,

Following the recent vacation of Viking House and given the success of One The Brayford, we have been excited to deliver a second site of high quality apartments as Phase Two. Plans for this had been drawn and approved by the City, however the opportunity to purchase surrounding buildings, including the former CAD offices and the Marrakesh restaurant, caused us to reconsider the scheme and what is best for the site. As a result, the business has decided to separate One The Brayford and the former Viking House to provide a stand-alone student scheme for the University.

As the new scheme will be completely separate and have no shared access we believe that this decision will benefit residents of One The Brayford by reducing footfall through the common areas. The reduction in apartment numbers will also improve the service offered by concierge while strengthening overall apartment values.

Residents of Ninety-Eight Newland (former Viking House) will access via Newland (opposite the county council) and there will be no connection through the existing shared carpark, which will also aid security at One The Brayford. Ninety-Eight Newland will be managed directly by the University with 24 hour on site presence, this will prevent any potential issues around noise or anti-social behaviour, that said given the separation any occurrences should not directly impact residents of One The Brayford.

The revised scheme will be delivered in two phases, the first being the renovation and front extension of Viking House, to be completed in September. The second will be a new build extension to the former CAD building, to be completed in September 2019. While there will be construction works during this time we will endeavour to keep any disruption to a minimum.

We would invite any residents with concerns to make contact and make an appointment to discuss issues directly by calling Alice Hall-Jones on 01522 534438 or via ah@jjdl.co.uk.

We are also keen to arrange a general residents meeting to get feedback on any improvements that could be made to the management of the block and will be circulating a date for this in the coming weeks.

Kind Regards,

J & J Team

Consultee Responses

Lincolnshire Police

Thank you for your correspondence and opportunity to comment on the proposed development. I would request that you consider the following points that if adhered to would help reduce the opportunity for crime and increase the safety and sustainability of the development.

Historically Student Accommodation can become vulnerable to crime and anti-social behaviour therefore it is important that the best security arrangements and provision are planned for such premises.

The safety, security and general well being of student should be of paramount importance when considering the detail of this application. The site is centrally located and has an entrance that exits onto a busy area of Lincoln, within the centre of Lincoln nightlife? The following aspects of security should be rigorously applied to this building.

Lincolnshire Police has no formal objections to the planning application in principle but would recommend that the initial advisory recommendations are implemented.

External doors and windows

The potential for unwanted guests will be considerable at this location and therefore robust measures should be installed to ensure the security and safety of student residents. Access may be gained via either of the shown entrances and the risk of 'follow through' entry gained. I would recommend that an air-lock style entrance vestibule is incorporated into the design (to help prevent unauthorised follow through access) commensurate with an access control system, with an electronic door release, and visitor door entry system that provides colour images, and clear audio communications linked to each individual unit. Under no circumstances should a trade person release button or similar uncontrolled access method be used.

An Industry standard approved CCTV system should be installed covering all communal points of entry and lobby areas. This system must be able to capture and record all persons using the entry system.

The secured by design requirement for all dwelling external doors is PAS 24:2016 or Bespoke equivalent (doors of an enhanced Security) or WCL 1 (WCL 1 is the reference number for PAS 23/24 and is published by Warrington Certification Laboratories).

All ground floor windows and doors and those that are easily accessible from the ground must conform to improved security standard PAS24: 2016. **All ground floor windows should have window restrainers and effective locking systems.**

Access to Places of Height

It is important that access to places of height (prevention of suicide) is secured on all levels and should include the provision of substantial windows and locking systems together with fixed and secured 'window restraining' devices. Any points of access to the roof area or other place of height should be secured by way of 'appropriate' fire compliant locking systems.

I would recommend that all ground floor and easily accessible windows have at least one pane of laminated glass.

Individual Flat or Unit Doors.

Flat entrance door-sets should meet the same physical requirements as the 'main front door' i.e. PAS24:2016. The locking hardware should be operable from both sides of an unlocked door without the use of the key (utilising a roller latch or latch operable from both sides of the door-set by a handle). If the door-set is certified to either PAS24:2016 or STS 201 Issue 4:2012 then it must be classified as DKT.

Homes of Multi-Occupancy / Student Accommodation – Communal Areas & Mail Delivery

Where communal mail delivery facilities are proposed and are to be encouraged with other security and safety measures to reduce the need for access to the premises communal letter boxes should comply to the following criteria.

- Located at the main entrance within an internal area or lobby (vestibule) covered by CCTV or located within an 'airlock style' entrance hall.
- Be of a robust construction (Federation Technical Specification 009 (TS009)
- Have anti-fishing properties where advised and appropriate.
- Installed to the manufacturers specifications.
- Through wall mail delivery can be a suitable and secure method.

Lighting

Lighting should be designed to cover the external doors and be controlled by *photoelectric cell* (dusk to dawn) with a manual override. The use of low consumption lamps with an efficacy of greater than 40 lumens per circuit watt is required; it is recommended that they be positioned to prevent possible attack.

Cycle Storage Structure (if to be included)

Generally pedestrian access doors-sets to commercial units should be certified to LPS 1175 security rating 2. The access controlled door should be designed in such a way that the hinges and door-sets are of a non-lift nature and non-tamper proof. The door locks must be operable by way of a thumb screw turn to avoid any person being accidentally locked in the cycle storage area.

Lighting within cycle storage area; automatically activated passive infra-red lighting should be considered rather than permanent lighting to which other users become accustomed and therefore activation would not draw any attention. Lighting units should be vandal resistant energy efficient light fittings.

Bin Storage

Internal communal bin and bicycle stores within blocks of flats must have no windows and be fitted with a secure door set that meets the same physical specification as 'front door' and specifically Section 2, paragraphs 21.1 to 21.6 and 21.8 to 21.13.

This will ensure that such stores are only accessible to residents. The locking system must be operable from the inner face by use of a thumb turn to ensure that residents are not accidentally locked in by another person. A bicycle store must also be provided with stands with secure anchor points or secure cycle stands.

External bins stores and home composting containers (supplied to meet 'Code for Sustainable Homes' 'Was 3') should be sited in such a way that they cannot be used as a climbing aid to commit crime.

Please do not hesitate to contact me should you need further information or clarification.

Please refer to *Commercial Guide 2015 & New Homes 2016* which can be located on www.securedbydesign.com

Crime prevention advice is given free without the intention of creating a contract. Neither the Home Office nor the Police Service takes any legal responsibility for the advice given. However, if the advice is implemented it will reduce the opportunity for crimes to be committed.

Yours sincerely,
John Manuel MA BA (Hons) PGCE Dip Bus.

Force Designing Out Crime Officer

Lincolnshire County Council (as Highway and Lead Local Flood Authorities)

Note to Case Officer:

Please note that this application has been assessed alongside Phase B 2018/0533/FUL and the comments below have been duplicated on both responses as they refer to the site as a whole.

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Requests that any permission given by the Local Planning Authority shall include the conditions below.

CONDITIONS (INCLUDING REASONS)

HP12 - Within seven days of the new access being brought into use, the existing access onto Newland shall be permanently closed in accordance with drawing number 170415-WDK-00-SI-DR-C-33001 dated May 2017.

Reason: To remove vehicle access points in the public highway that are not required and no longer serve their intended use.

HP33 - No development shall take place until a surface water drainage scheme for the site, based on sustainable urban drainage principles and an assessment of the hydrological and hydrogeological context of the development has been submitted to and approved in writing by the Local Planning Authority. The scheme shall:

a) Provide details of how run-off will be safely conveyed and attenuated during storms up to and including the 1 in 100 year critical storm event, with an allowance for climate change, from all hard surfaced areas within the development into the existing local drainage infrastructure and watercourse system without exceeding the run-off rate for the undeveloped site;

b) Provide attenuation details and discharge rates which shall be restricted to 5 litres per second, as per drawing number 170415-WDK-00-SI-DR-C-33000 dated May 2017;

c) Provide details of the timetable for and any phasing of implementation for the drainage scheme; and

d) Provide details of how the scheme shall be maintained and managed over the lifetime of the development, including any arrangements for adoption by any public body or Statutory Undertaker and any other arrangements required to secure the operation of the drainage system throughout its lifetime. The development shall be carried out in accordance with the approved drainage scheme and no dwelling shall be occupied until the approved scheme has been completed or provided on the site in accordance with the approved phasing. The approved scheme shall be retained and maintained in full in accordance with the approved details.

INFORMATIVES:

HI03 - Prior to the submission of details for works within the public highway, as referred to in the above condition, you must contact the Head of Highways - on 01522 782070 for application, specification and construction information.

HI08 - Please contact Lincolnshire County Council Streetworks & Permitting team on 01522 782070 to discuss any proposed statutory utility connections and any other works which will be required in the public highway in association with this application. This will enable Lincolnshire County Council to assist you in the coordination and timings of such works.

H100 - Could you please make the applicant aware that should any of their works affect any items of street furniture within the public highway, or require that they be moved, that they will need to arrange this with the necessary party at their own expense.

Thank you for the opportunity to comment on the above application. The site is within the Upper Witham Internal Drainage Board district.

Witham & Humber Internal Drainage Boards

Although a Flood Risk Assessment (FRA) and Drainage Strategy is now included, the Board **Objects** to the application because the Flood Risk Assessment does not addresses surface water as below.

The new build section is shown to be potentially at risk from surface water flooding on the Environment Agency Surface Water Flood Maps, with water flowing down the hill and over the road into the site. This is doubly an issue because the proposals show this as 'partial subterranean' with sleeping accommodation. The FFL of 5.9 is also very close to the design flood level on the nearby Brayford Pool.

No development should be commenced until the Local Planning Authority, in consultation with the Lead Local Flood Authority has approved a scheme for the provision, implementation and future maintenance of a surface water drainage system.

All drainage routes through the Site should be maintained both during the works on Site and after completion of the works. Provisions should be made to ensure that upstream and downstream riparian owners and those areas that are presently served by any drainage routes passing through or adjacent to the Site are not adversely affected by the development.

Drainage routes shall include all methods by which water may be transferred through the Site and shall include such systems as "ridge and furrow" and "overland flows". The effect of raising Site levels on adjacent property must be carefully considered and measures taken to negate influences must be approved by the Local Planning Authority.

Regards,

Richard Wright

Engineering Services Technician
Witham & Humber Internal Drainage Boards,

Application Number:	2018/0626/RM
Site Address:	Grantham Street Car Park, Grantham Street, Lincoln
Target Date:	10th August 2018
Agent Name:	Globe Consultants Ltd
Applicant Name:	Jackson & Jackson Developments Limited
Proposal:	Submission of Reserved Matters including access, appearance, landscaping, layout and scale for the erection of a six-storey building incorporating student accommodation and car parking as required by outline planning permission 2017/0721/OUT

Background - Site Location and Description

Site Location

The application site is situated at the south-western corner of the junction of Grantham Street with Flaxengate but also adjoins Swan Street to the west. In general terms, it is situated to the east of the High Street.

The application site is irregular but roughly square in shape and is currently utilised as a surface car park. It is adjacent to commercial uses within The Terrace, to the north, and with frontages to Clasketgate, to the south; there are residential apartments to the west and northwest on Swan Street and Grantham Street respectively; and student accommodation to the east in the Danesgate House building. Meanwhile, the County Council occupy a building across Flaxengate and the theatre is to the southwest.

The northern and southern boundaries are currently delineated by trees.

Description of Development

The application is for the submission of all Matters that were reserved following the grant of outline planning permission (ref: 2017/0721/OUT). These include access, appearance, landscaping, layout and scale for the erection of a six-storey building incorporating student accommodation and car parking.

The car parking would be at lower ground floor level and accessed from Flaxengate. It would accommodate 26 spaces, two of which would be DDA compliant. The student accommodation would be for ten clusters of bedrooms with a shared living room and kitchen over five floors, i.e. two clusters to each floor (clusters of 7 and 14 at ground floor; clusters of 9 and 16 on floors 1-3; and clusters of 9 and 13 on the fourth floor). There will be four DDA compliant rooms for students.

Site History

As alluded to in the description of development, only the principle of development was established by the outline planning permission parking along with one or a mixture of the following uses:

- Residential Units (C3);
- Student Accommodation;
- Offices (B1); and / or
- Hotel Accommodation (C1).

Notwithstanding this, maximum scale parameters were also set for the proposed development within which the reserved matters would be brought forward. These include the maximum footprint (including the resultant floor areas) and height of the building. The indicative floor plans submitted showed a student accommodation use.

Site History

Reference:	Description	Status	Decision Date:
2017/0721/OUT	Erection of a building to include 2 levels of car parking and 4 storeys above to provide either residential units (use class C3); and/or student accommodation; and/or office (use class B1); and/or Hotel (use class C1) (Outline) (REVISED PLANS AND DESCRIPTION)	Granted Conditionally	5th April 2018

Case Officer Site Visit

Undertaken on 11th May 2018.

Policies Referred to

- Policy LP1 A Presumption in Favour of Sustainable Development
- Policy LP2 The Spatial Strategy and Settlement Hierarchy
- Policy LP3 Level and Distribution of Growth
- Policy LP5 Delivering Prosperity and Jobs
- Policy LP6 Retail and Town Centres in Central Lincolnshire
- Policy LP7 A Sustainable Visitor Economy
- Policy LP9 Health and Wellbeing
- Policy LP11 Affordable Housing
- Policy LP12 Infrastructure to Support Growth
- Policy LP13 Accessibility and Transport
- Policy LP14 Managing Water Resources and Flood Risk
- Policy LP16 Development on Land Affected by Contamination
- Policy LP17 Landscape, Townscape and Views
- Policy LP21 Biodiversity and Geodiversity
- Policy LP24 Creation of New Open Space, Sports and Recreation Facilities
- Policy LP25 The Historic Environment
- Policy LP26 Design and Amenity
- Policy LP29 Protecting Lincoln's Setting and Character
- Policy LP31 Lincoln's Economy
- Policy LP33 Lincoln's City Centre Primary Shopping Area and Central Mixed Use Area
- Policy LP36 Access and Movement within the Lincoln Area
- Policy LP37 Sub-division and multi-occupation of dwellings within Lincoln

- National Planning Policy Framework

Issues

As alluded to above, the principle of the erection of development encompassing student accommodation was agreed through the approval of outline planning permission for the site. In light of this, it would not be possible to revisit the principle of this form of development. Furthermore, the maximum scale parameters of the building were also agreed at this point, including the overall footprint and height of the development. However, the details of the access, appearance of the building, landscaping and layout are for consideration. The main issues referred to below therefore need to be considered as part of this application:

1. The Impact of the Design of the Proposals;
2. The Implications of the Proposals upon Amenity;
3. Sustainable Access and Highway Safety;
4. Archaeology;
5. Matters Controlled by Planning Conditions on the Outline Planning Permission;
6. Other Matters; and
7. The Planning Balance.

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Monks Road Neighbourhood Initiative	Response Awaited
Lincolnshire Police	Comments
Lincoln Civic Trust	Object
Education Planning Manager, Lincolnshire County Council	No request for S106 due to the development being for student accommodation
Historic England	No Comments
Highways & Planning	Response Awaited

Public Consultation Responses

Name	Address
Richard Tibenham	Greenlite Energy Assessors
Mr Thomas Foley	7 Swan Street Lincoln LN2 1LF

Consideration

1) The Impact of the Design of the Proposals

a) *Relevant Planning Policy*

So far as this issue is concerned, as alluded to above, the proposals must achieve sustainable development and it is the social dimension of sustainability that relates to design. Moreover, Paragraph 7 of the National Planning Policy Framework (the Framework) requires the creation of high quality built environment. In addition, the policy principles outlined in Paragraphs 17, 58, 60, 61 and 64 of the Framework also apply. Moreover, the Framework states that good design is a key aspect of sustainable development and is indivisible from good planning. Design is to contribute positively to making places better for people (para. 56). To accomplish this development is to establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live and responding to local character and history (para. 58). It is also proper to seek to promote or reinforce local distinctiveness (para. 60).

At the local level, the Council, in partnership with English Heritage, have undertaken the Lincoln Townscape Appraisal (the LTA), which has resulted in the systematic identification of 105 separate “character areas” within the City. The application site lies within the High Street Character Area. Policy LP29 refers to the LTA and requires that developments should “protect the dominance and approach views of Lincoln Cathedral, Lincoln Castle and uphill Lincoln on the skyline”. This policy is supported by Policy LP17, which is relevant to the protection of views and suggests that:-

“All development proposals should take account of views in to, out of and within development areas: schemes should be designed (through considerate development, layout and design) to preserve or enhance key local views and vistas, and create new public views where possible. Particular consideration should be given to views of significant buildings and views within landscapes which are more sensitive to change due to their open, exposed nature and extensive intervisibility from various viewpoints.”

Policy LP26 refers to design in wider terms and requires that “all development, including extensions and alterations to existing buildings, must achieve high quality sustainable design that contributes positively to local character, landscape and townscape, and supports diversity, equality and access for all.” The policy includes 12 detailed and diverse principles which should be assessed. This policy is supported by Policy LP5 which also refers to the impact on the character and appearance of the area; and Policy LP31, which refers to the protection and enhancement of the character of the city.

In terms of the wider impacts upon built heritage, Policy LP29 also requires that “proposals within, adjoining or affecting the setting of the 11 Conservation Areas and 3 historic parks and gardens within the built up area of Lincoln, should preserve and enhance their special character, setting, appearance and respecting their special historic and architectural

context”; and “protect, conserve and, where appropriate, enhance heritage assets, key landmarks and their settings and their contribution to local distinctiveness and sense of place, including through sensitive development and environmental improvements”.

Meanwhile, conservation is enshrined in the Core Planning Principles of the Framework (Paragraph 17) as planning is expected to “conserve heritage assets in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of this and future generations”. In addition, Section 12 of the Framework also refers to the impacts of development upon designated heritage assets and is supported by Policy LP25 also applies as it specifically refers to the impacts of developments upon these assets. In terms of conservation areas, the policy requires that development should either enhance or reinforce features that contribute positively to the area’s character, appearance and setting. Meanwhile, proposals also need to have regard to the setting of other designated assets, including listed buildings.

b) Assessment of the Implications of the Proposals

i) The Site Context and Submissions

The application site is contained within the Cathedral and City Centre Conservation Area City Centre and is considered to have the potential to affect views into and within the Area. As such, the visual implications of the proposals for the site are key to the assimilation of development into its context and the creation of high quality built environment.

In terms of the scale of the development, officers have worked with the applicant on both applications in order to ensure that this would not have a detrimental impact upon townscape. Crucially, Members should note that the height of the building has reduced even further from that submitted as part of the outline planning application. This is referred to in further detail below.

ii) The Impact of the Development in its Wider Context

As the height of the building has been reduced again from the maximum parameters referred to in the outline planning permission, the impact upon views within and into the Conservation Area would be less than it was previously with that application. Moreover, the impacts would not be harmful in the context of the townscape as the building would either be sat against the backdrop of existing townscape or would be no higher than that townscape.

iii) Implications of the Development in its Immediate Context

As Members will appreciate, the details shown in the outline planning application were only indicative and it is the current application that shapes the appearance of the building and how it would assimilate within its context.

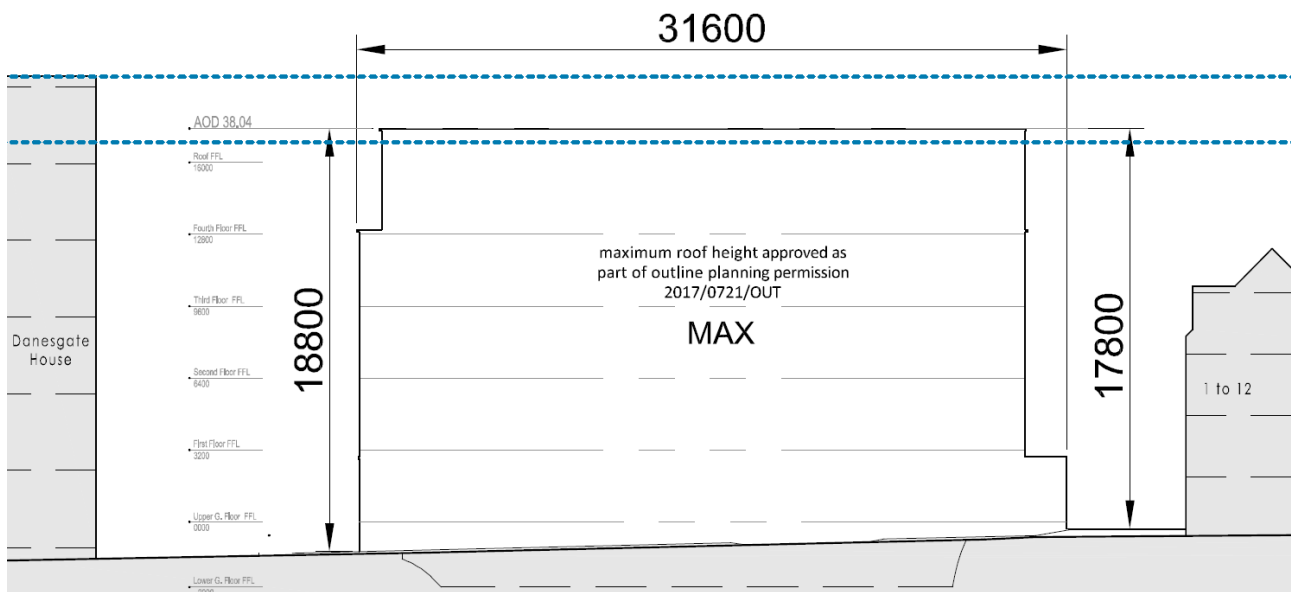
It is noted that the Civic Trust have raised concerns regarding the content of the application submission including the materials to be used in the construction of the development. However, the plans submitted for the development are clear and include specific references to the type of materials proposed for the building. Nonetheless, the report details the design in further detail below.

However, prior to this, it is first important to refer to the scale of the building. As alluded to in the report for the outline planning application, it is clear that there are tall buildings within

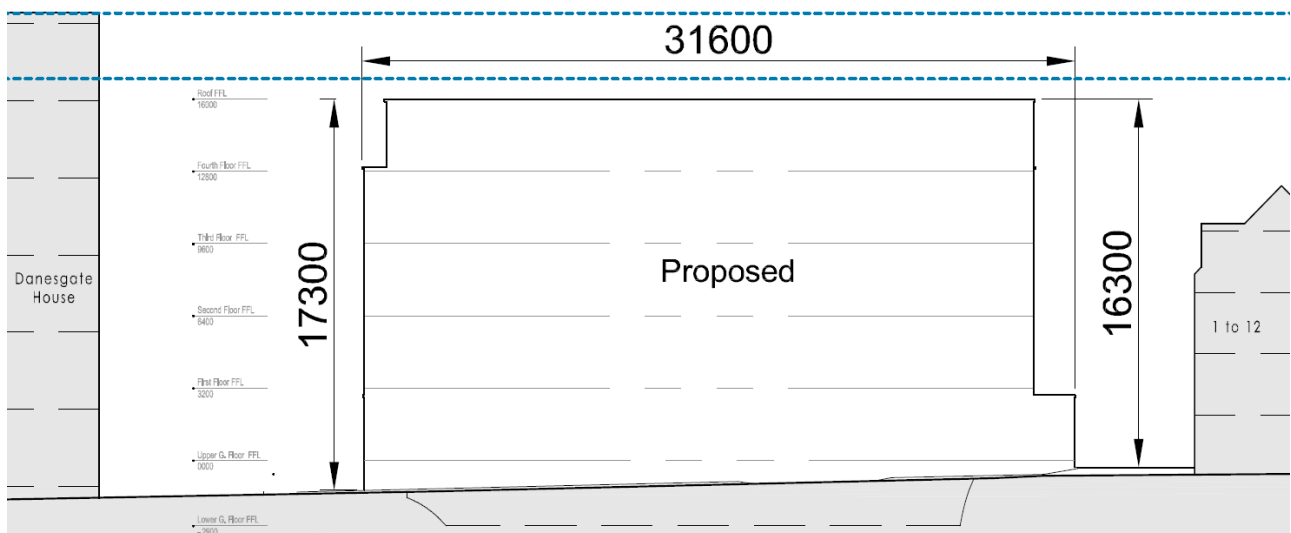
Architectural elevation drawing of a building facade. The drawing includes the following dimensions and labels:

- Overall Width:** 25900 (indicated by a dimension line at the top).
- Overall Height:** 21100 (indicated by a vertical dimension line on the left side).
- Roof Height:** 18500 (indicated by a vertical dimension line on the right side, from the ground level to the roofline).
- Roof Profile:** The roof is shown with a gabled section on the right and a flat section on the left. The gabled section is labeled "The Terrace".
- Height of Surrounding Buildings:** Indicated by a dashed blue line at the top right.
- Maximum Roof Height:** "maximum roof height approved as part of outline planning permission 2017/0721/OUT" with "MAX" written below it.
- Floor Levels (FFL):**
 - AOD 38.04
 - Roof FFL 16000
 - Fourth Floor FFL 12000
 - Third Floor FFL 9600
 - Second Floor FFL 6400
 - First Floor FFL 3200
 - Upper G. Floor FFL 0000
 - Lower G. Floor FFL -2800

This architectural elevation drawing illustrates a proposed building and its context. On the left, a vertical scale indicates floor levels: Lower G. Floor FFL (-2500), First Floor FFL (3000), Second Floor FFL (6400), Third Floor FFL (9600), Fourth Floor FFL (12800), and Roof FFL (16000). The proposed building is shown with a height of 19600. To the right, an existing building labeled 'The Terrace' is shown with a height of 17000. A dashed blue line at the top indicates the 'Height of Surrounding Buildings' at AOD 36.54.



The aforementioned 1.5m reduction in height from the outline parameters is shown in the images above and below for this east/west cross section, with Swan Street to the right and Flaxengate to the left of the images.



In addition to the reduction in the height of the building, the form and appearance of the building, including the overall proposed materials palette, have also been utilised carefully to ensure that the building would have a suitable modern appearance that would enhance the character and appearance of the Conservation Area.

Due to the intended end use of the building, the principal elements of the façades of the building will include a consistent layout of window apertures set within a wider buff brick frame. The choice of this lighter material would align with other similar bricks used elsewhere in the locality but would be more appropriate in the context of the architecture of the building. The frame would also share some similarities with the recladding undertaken of Danesgate House.

Within the window apertures themselves, the solid recessed elements will be of a different buff brick to distinguish them from the main façade. Meanwhile, the frames for the windows will be metal (bronze in colour).

All buildings have components that are more functional, such as stairwells but where possible the applicant has sought to provide an architectural solution. In particular, at the corner of Swan Street and Grantham Street, the stairwell and lift shafts have been incorporated in a contrasting reconstituted stone clad feature which wraps around from above the entrance on Swan Street to Grantham Street. This would incorporate a recessed section at the corner of solid bronze coloured aluminium framed curtain walling.

Meanwhile, in terms of the Flaxengate frontage, a key change from the original indicative outline scheme is that the car park would only be served by one access/egress from Flaxengate, which would be positioned centrally to the elevation to provide balance to that façade and framed in the same stone as the feature corner to Swan Street/Grantham Street. Furthermore, the reduction in the number of the entrances to the car park would ensure that there is minimal disruption to the façades of the building and would assist in drawing activity from Grantham Street around to the entrance on Flaxengate.

In a similar manner, the plant and machinery to serve the building is proposed to be accommodated internally on the ground floor behind the bin store and covered by louvered screens. As such, there would not be visual implications associated with these. Nonetheless, there was also a planning condition included on the outline planning permission to ensure that there would be suitable mitigation measures in place to protect residents from noise.

As with the outline planning application, the top floor of the building remains recessed back along the majority of the Flaxengate and Grantham Street frontages, in order to further reduce the perceived scale of the building. The external materials of these sections, as well as most of the west facing elevation of the fourth floor, will be bronze standing seam cladding.

Finally, whilst there would be limited opportunities within the site for landscaping, the applicant has included details of landscaping for the roof of the internal courtyard of the building and in certain locations at the site perimeter, as shown below:



c) *Summary in Relation to this Issue*

Officers are satisfied that the proposals would result in a tall modern building that will assimilate well within its context, particularly the façade treatments, which address the street edge in a similar way to other buildings within the vicinity and are sufficiently broken down into component parts in order to reduce the perceived mass of the building. Moreover, the proposals offer the opportunity to regenerate this important area with a high quality development that is suitably scaled to appropriately integrate with the surrounding townscape that contributes to the valued character and appearance of the conservation area. Therefore, it is considered that the proposal satisfies the duty contained within section 72 (1) of the Planning (Listed Buildings and Conservation Areas Act) 1990 'In the exercise, with respect to any buildings or other land in a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area'. Furthermore, the proposal is in accordance with the guidance contained within paragraph 137 of the Framework which advises that Local Planning Authorities should look for new development within a Conservation Area and within the setting of heritage assets to reveal or better enhance significance.

2) Implications of the Proposals upon Amenity

a) *Relevant Planning Policy*

In terms of national policy, the NPPF suggests that development that results in poor design and/or impacts upon the quality of peoples' lives would not amount to sustainable development. Consequently, the implications of both are key to the consideration of the acceptability of the principle of development within a given site. Moreover, the Framework (Paragraph 9) sees "seeking positive improvements in the quality of the built, natural and historic environment, as well as in people's quality of life" as being important to the delivery of sustainable development, through "replacing poor design with better design" and "improving the conditions in which people live" amongst others. Furthermore, the core principles of the Framework (Paragraph 17) indicate that "planning should...always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings".

Policy LP26 of the Plan deals with design and amenity. The latter refers to the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy and suggests that these must not be unduly harmed by, or as a result of, the development. There are nine specific criteria which must be considered. The policy is in line with the policy principles outlined in Paragraphs 17, 59 and 123 of the NPPF. Indeed, Paragraph 123 of the Framework suggests that "decisions should aim to...avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development".

b) *Assessment of the Implications of the Proposals*

i) *Impacts of the Scale of the Building*

As alluded to in the first section above, contrary to the assertion made by a resident, Members will note that the height of the proposed building is actually presented as being lower than the maximum parameters agreed through the outline planning application. Moreover, the height has been reduced by 1.5metres, which means that there would be an

improvement as a result of the actual scale of the building in relation to its impacts upon outlook, overshadowing and loss of light.

Furthermore, the perception of the scale of development has also been attended to through the architectural design of the building. In particular, as alluded to in the assessment of the design of the building, its façades would not be stark or oppressive. Conversely, officers would advise Members that the combination of the movement back and forth across the plane of the façades of the building, as well as the use of lighter coloured high quality materials, would provide sufficient visual interest to the façades to ensure that they would not be harmful to the outlook of the neighboring properties, and thereby the amenities that they would expect to enjoy.

ii) Overlooking and Loss of Privacy

Members will note from the outline planning application process that a number of concerns were raised from the occupants of neighbouring properties regarding the potential for overlooking or a direct loss of privacy resulting from the development.

However, following the grant of that permission, officers have worked with the applicant in order to ensure that the design of the west façade, adjacent to Swan Street, would ensure that habitable spaces are served by windows angled south, i.e. the occupants of these rooms would only be able to look south down the street and not toward the residential apartments opposite. Furthermore, whilst the southern aspect of the building seeks to maximise the opportunities for a greater degree of glazing, particularly to shared spaces, the glazing will also be obscured to lower elements to reduce the possibilities for loss of privacy in either direction in this elevation to and from other development.

In terms of other relationships to the north between the proposals and apartments and commercial premises in the Terrace across Grantham Street, officers are satisfied that the window to window relationship presented would be similar to that already found within the immediate context, e.g. between the apartments in the Sparkhouse and Swan Street. Therefore officers are satisfied that the relationship presented would not have a detrimental impact upon the occupants of existing properties from the perspective of overlooking or loss of privacy

iii) Noise and Disturbance

Whilst a resident has again raised concerns regarding the potential for noise and disturbance from the proposed use, there has not been a material change in circumstances from the outline to the current application to lead to a different conclusion being made with respect to the impacts of noise and disturbance from the proposals. Nonetheless, given that the proposed end use and the internal layout of the building is now fixed, including access and egress from the building, it would be reasonable to ensure that the management of the building is through an appropriate level building management plan, such as a 24 hour concierge serving the main entrance/reception.

c) The Planning Balance

Taking all the above in to account, it is considered that the proposed development of the site could be accommodated in the future in a manner that would not cause unacceptable harm in respect of most matters relevant to the protection of amenity. Moreover, with satisfactory controls over the mitigation employed in relation to the future management of

the building, the proposals would be socially and environmentally sustainable in the context of the Framework and would accord with the policies in the Local Plan.

3) Sustainable Access and Highway Safety

a) *Relevant Planning Policies*

The impacts of growth are enshrined in the Core Planning Principles of the Framework (Paragraph 17), which expects planning to actively manage this growth “to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable”. As such, Paragraph 35 requires that: “developments should be located and designed where practical to [amongst other things] give priority to pedestrian and cycle movements, and have access to high quality public transport facilities; and should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones”.

A number of Local Plan Policies are relevant to the access, parking and highway design of proposals. In particular, the key points of Policy LP13 are that “all developments should demonstrate, where appropriate, that they have had regard to the following criteria:

- a) Located where travel can be minimised and the use of sustainable transport modes maximised;
- b) Minimise additional travel demand through the use of measures such as travel planning, safe and convenient public transport, walking and cycling links and integration with existing infrastructure;
- c) Should provide well designed, safe and convenient access for all, giving priority to the needs of pedestrians, cyclists, people with impaired mobility and users of public transport by providing a network of pedestrian and cycle routes and green corridors, linking to existing routes where opportunities exist, that give easy access and permeability to adjacent areas”

There are also transport measures referred to in Policy LP36, which more specifically refers to development in the ‘Lincoln Area’, the key measures add to and reinforce the criteria within Policies LP5 and LP13. As such, they are intended to reduce the impact upon the local highway network and improve opportunities for modal shift away from the private car. In particular, development should support the East West Link in order to reduce congestion, improve air quality and encourage regeneration; and improve connectivity by means of transport other than the car. Similarly, Policy LP33 also requires that developments do not result in “levels of traffic or on-street parking which would cause either road safety or amenity problems.” Moreover, the policy also highlights the importance of providing appropriate parking for vehicles and cycles for all users within developments; and that walking and cycling links are maintained and promoted.

Paragraph 32 of the Framework suggests that the residual cumulative impacts of the development would need to be severe for proposals to warrant refusal. This is reinforced by Policy LP13 of the Local Plan which suggests that only proposals that would have “severe transport implications will not be granted planning permission unless deliverable mitigation measures have been identified, and arrangements secured for their implementation, which will make the development acceptable in transport terms.”

b) Assessment of the Implications of the Proposals

A resident has suggested that the application is not complete as the details of access are not sufficiently detailed. However, the layout plans for the application are sufficient for officers and statutory consultee (the Highway Authority) to be able to take a view on the appropriateness of the development. However, as with all applications, the precise design of the intersection and alignment with the highway would need to be agreed at a later date with the Highway Authority.

Moreover, whilst the formal response to the application is awaited from the Highway Authority, in informal discussions with their officer, there would not be any concerns in relation to the proposals, including the reduction upon existing car parking within the site by four spaces (from 30 to 26). However, their officer is in discussions with the applicant regarding some technical matters. Nonetheless, as alluded to above, it would be necessary for the applicant to carry out highway works as part of the application, including closing up the existing accesses on Grantham Street and Swan Street and agree works to the footpath and highway in connection with the proposed access to Flaxengate.

Consequently, it is the advice of officers that it would be difficult to argue that there would be a harmful impact upon highway safety. In particular, the inclusion of only one access / egress from Flaxengate for the car park is a positive outcome as it would result in:

- a reduction in the number of access points to one in Flaxengate which is inherently wider than Grantham Street and Swan Street;
- there being only one void in the ground floor elevations of the building; and
- the scale of the building reducing, as parking would no longer be required on two levels of the building.

In light of this, officers consider that it would be difficult to raise concerns regarding the development.

4) Archaeology

a) Relevant Planning Policy

The Framework and Planning Practice Guide as well as good practice advice notes produced by Historic England on behalf of the Historic Environment Forum including *Managing Significance in Decision-Taking in the Historic Environment* and *The Setting of Heritage Assets* are relevant to the consideration of Planning Applications.

Indeed, heritage is referred to within the core principles of the Framework (Paragraph 17) and Paragraph 128 of the Framework states that “in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.”

Paragraph 141 of the Framework states that LPAs should ‘require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible.’

Policy LP25 in the Central Lincolnshire Local Plan requires that development does not lead to significant detrimental impacts on heritage assets. This issue is directed in relation to archaeology that could be non-designated heritage assets.

b) Assessment of the Implications of the Proposals

Several planning conditions were imposed on the outline planning permission to require the provision of further information informative to reserved matters. The applicant has carried out further evaluation and provided the details of a foundation design, which is currently being negotiated with officers. At present, the building has been designed to ensure that the formation level of the development (and thereby the floor level of the car park) would be above Roman remains, which would be a part of a preservation strategy. However, officers are still in discussions with the applicant in relation to the final design of the foundations for the building, as this element needs to inform the design of the building.

Consequently, officers will provide Members with an update in relation to the progress made since the completion of this report on the update sheet.

5) Matters Controlled by Planning Conditions on the Outline Planning Permission

Unless indicated otherwise below, these matters will be controlled by the conditions included on the Outline Planning Permission:

a) Ecology, Biodiversity and Arboriculture

i) Relevant Planning Policies

Paragraph 118 of the NPPF requires LPAs to conserve and enhance biodiversity by refusing planning permission where significant harm resulting from a development cannot be avoided, mitigated or compensated for. Meanwhile, Policy LP21 refers to biodiversity and requires development proposals to “protect, manage and enhance the network of habitats, species and sites of international, national and local importance (statutory and non-statutory), including sites that meet the criteria for selection as a Local Site; minimise impacts on biodiversity and geodiversity; and seek to deliver a net gain in biodiversity and geodiversity.” The policy then goes on to consider the implications of any harm associated with development and how this should be mitigated.

ii) Assessment of the Impact of the Development

Members may recall that the approval of Outline Planning Permission enabled the developer to remove trees that would suppress the implementation of the development. However, a planning condition imposed on that permission ensures that the trees are only removed from the site once an appropriately designed scheme has been approved and a contractor has been appointed to develop the site. Until such time as both points have been satisfied the trees would remain protected in the conservation area.

Notwithstanding this, the scheme is now known and this would remove those trees, as such, it would be reasonable and proportionate to request the details of bird boxes for the building.

b) *Site Drainage*

In accordance with the requirements of Policy LP14 of the Plan and Paragraph 103 of the Framework, the design of schemes to deal with foul and surface water disposal were agreed by planning condition. As such, there would not be in conflict with the environmental dimension of sustainability outlined in Paragraph 7 of the Framework.

c) *Land Contamination*

In accordance with the requirements of Policy LP16 of the Plan and Paragraphs 109, 120 and 121 of the Framework, planning conditions were imposed on the outline planning permission to deal with land contamination prior to development being undertaken.

d) *Air Quality*

In accordance with the requirements of Policy LP13 of the Plan and Paragraphs 109, 120 and 124 of the Framework, a planning condition was imposed on the outline planning permission to request the developer to provide a scheme to include one or more charging points for electric vehicles within the development.

e) *Cycle Storage / Parking*

The outline planning application established that the site is sustainably located in the heart of the city, close to the facilities, services and employment opportunities that would support students; as well as the Universities which are accessible by cycle and walking routes.

Consequently, the proposed ground floor plan has therefore been updated to include provision of Sheffield Cycle Stands to provide secure cycle storage. This format of storage is recommended by the National Cycling Association and would offer a secure storage option for future residents.

f) *Impacts of Construction*

A planning condition was imposed on the outline planning permission which committed the applicant to providing details of the proposed parking for construction vehicles; the layout and location of the compound; and construction working and delivery hours.

g) *External Lighting*

In addition, the applicant is also committed through a further condition to providing details of any functional or architectural external lighting of the building or its curtilage, in order to avoid a detrimental impact upon neighbouring properties. It is therefore recommended that an appropriate scheme of lighting is controlled by planning condition.

h) *CCTV Cameras and Safety*

Officers of the Council, including the Council's CCTV Team Leader, have been in discussions with the applicant to ensure that CCTV coverage of the area would not be adversely affected by the development. The applicant has provisionally agreed to CCTV

Cameras being installed on the building and Members will note that one location has been indicated in the plans accompanying this report. However, it is anticipated that the precise location would almost certainly require further discussions so a condition would be required for these details to be agreed. This would ensure that existing monitoring of public areas for safety reasons would not be adversely impacted upon.

6) Other Matters

A resident has raised concerns regarding the number of proposed student rooms within the building and has compared the scheme presented at outline with that now proposed. As Members will appreciate, the scheme at outline was indicative and there is nothing to suggest that the applicant should provide a certain number of units of accommodation within the building. What is more, Members will note from other similar schemes in the city that the preference is for a clustering of bedrooms as this has been found to be a more appropriate social experience for students.

In addition, a further party has made comment in relation to the thermal properties of the building, including overheating but there are no policies within the Local Plan to insist that developments include measures to address the implications of solar gain. However, the applicant would be able to consider these matters in terms of the design of the plant and machinery proposed to serve the building.

7) Planning Balance

Paragraph 14 of the Framework sets out a presumption in favour of sustainable development which for decision taking means that where relevant policies of the development plan are out-of-date planning permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against policies in the Framework, taken as a whole; or specific Framework policies indicate development should be restricted. There are no restrictive policies that would lead to the proposals not being sustainable. However, a conclusion whether a development is sustainable is a decision that has to be taken in the round having regard to all of the dimensions that go to constitute sustainable development.

In this case, the principle of the development of the use proposed was established through the outline planning permission and this would deliver economic and social sustainability directly through the construction of the development and the use proposed therein; and indirectly through the occupation of the building. Moreover, as alluded to through that application, the provision of additional purpose-built student bed spaces available in a location relatively close to both universities in the city should hopefully reduce the dependency further upon houses in multiple occupation. This would also improve environmental sustainability.

It is clear from the main body of the report that the proposed building would be smaller than that approved at outline planning and the building has been designed to avoid overlooking or a loss of privacy, which were concerns identified with the outline scheme. It is therefore considered that the proposed building would not lead to any harm to amenity subject to the existing planning conditions on the outline planning permission and those recommended below.

There is no evidence to suggest matters of congestion or road safety would be harmful due to the social or environmental sustainability of the development. Furthermore, the

implications upon the character of the area and the residential amenities of near neighbours would not have negative sustainability implications for the local community, as they would lead to a development that would be socially sustainable. As such, with compliance with existing planning conditions and those recommended here, the development would be environmentally sustainable.

Subject to the foundation design of the proposed development being a suitable means of preservation for buried archaeological remains, in this instance officers would advise Members that the planning balance should fall in favour of the proposals due to the long term implications of the enhancement that would be brought to the conservation area, as well as the potential stimulus that the proposals could be for further wider enhancement of the historic townscape. This is particularly important given the proximity of Grantham Street to the High Street.

Thus, assessing the development as a whole in relation to its economic, social and environmental dimensions and benefits, officers are satisfied that the benefits of developing this site would, in the long-term, be more important than the potential impacts of not doing so. As such, it is considered that, in the round, this proposal could be considered as sustainable development and would accord with the Local Plan and Framework, sufficient for the recommendation of officers to be that suitable planning permission should be granted subject to planning conditions.

Application Negotiated either at Pre-Application or during Process of Application

Yes additional information provided and the scheme revised following officer feedback.

Financial Implications

The proposals would offer benefits to economic and social sustainability through spend by new and existing residents and visitors, jobs created/sustained through construction and the operation of the development respectively.

Legal Implications

None.

Equality Implications

None.

Conclusion

The presumption in favour of sustainable development required by the National Planning Policy Framework would apply to the proposals as there would not be conflict with the three strands of sustainability that would apply to development as set out in the planning balance. Therefore, there would not be harm caused by approving the development. As such, it is considered that the application should benefit from planning permission for the reasons identified in the report and subject to the conditions outlined below.

Application Determined within Target Date

Yes.

Recommendation

That the application is approved, with authority delegated to the Planning Manager to formulate Planning Conditions covering the matters referred to below:-

- Schedule of materials;
- Scheme for the inclusion of bird boxes on or as part of the fabric of the building;
- Scheme for Future Management of the Building; and
- Scheme for CCTV Cameras to Replace the Existing Provision.

Report by Planning Manager

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Site Photos



Views in either direction along Grantham Street



View from Flaxengate across the site towards Grantham Street & Swan Street



View from Flaxengate across the site towards its southern boundary



Views north along Flaxengate and Swan Street Respectively

Plans





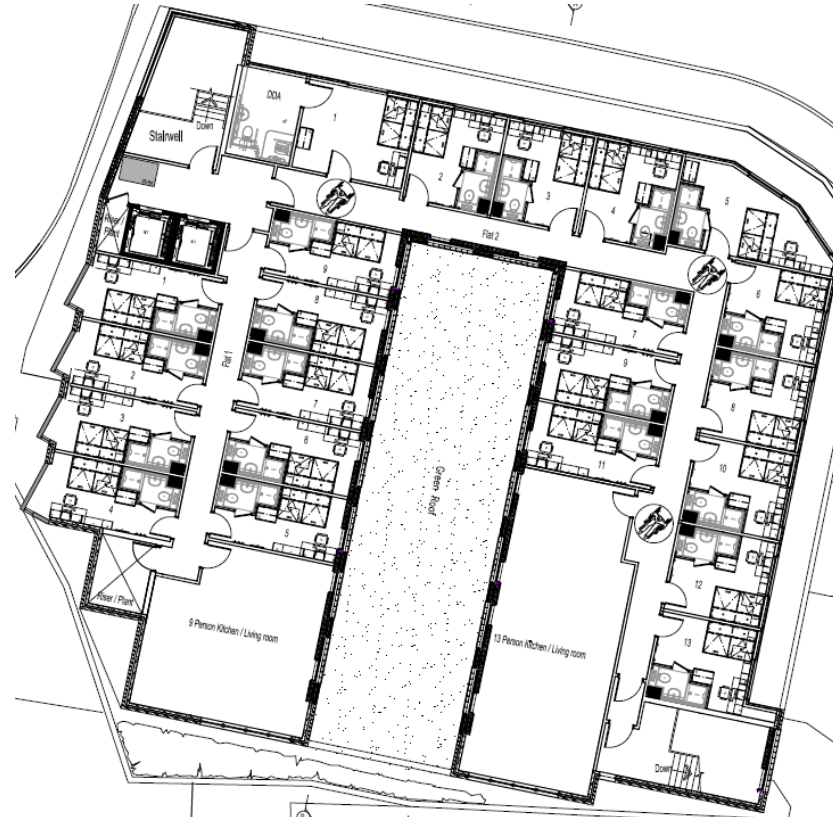
Car Park Plan (Lower Ground Floor)

Ground Floor Plan





First, Second and Third Floor Layout



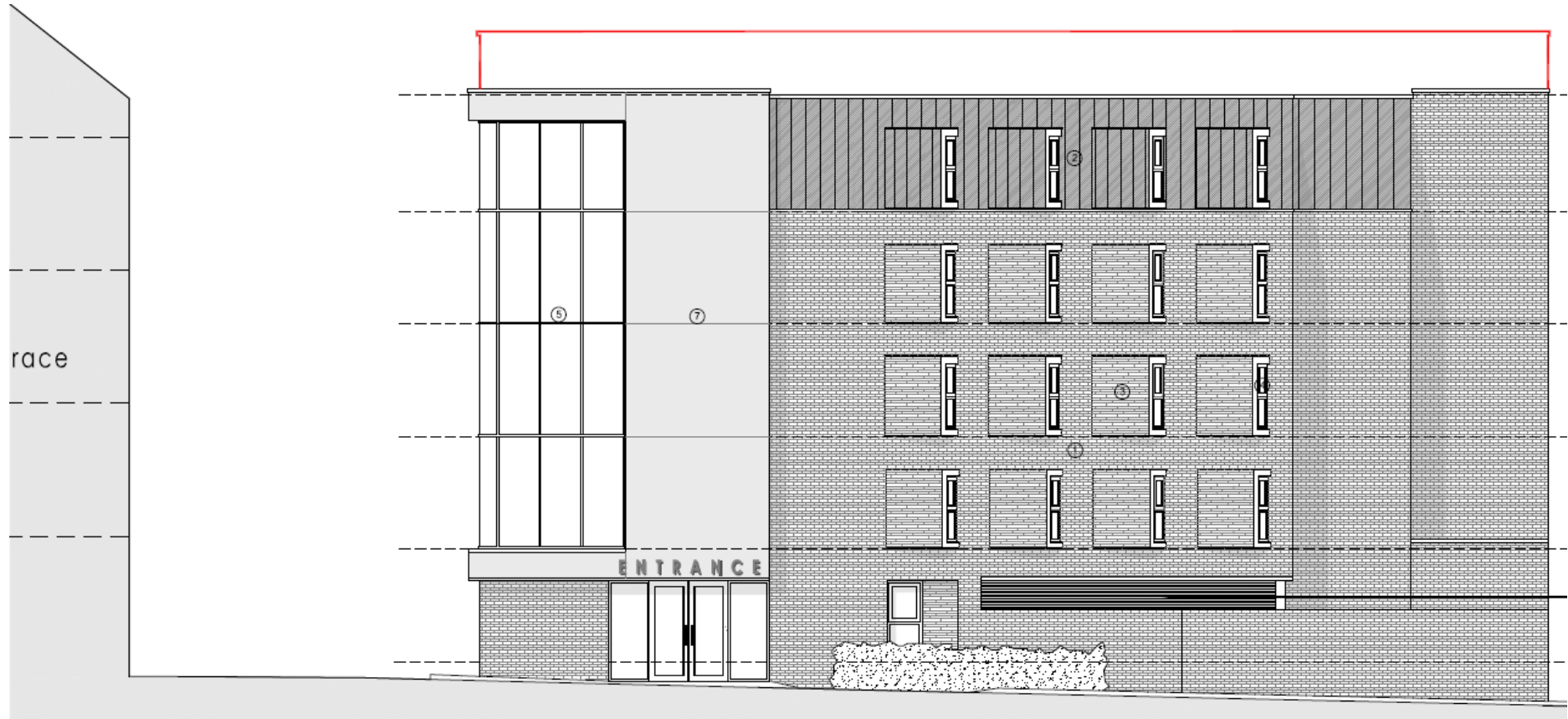
Fourth Floor Layout

Elevations

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Proposed East Elevation



Proposed West Elevation



Proposed North Elevation



Proposed South Elevation

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Consultee Comments

Historic England

Thank you for your letter of 11 May 2018 regarding the above application for planning permission. On the basis of the information available to date, we do not wish to offer any comments. We suggest that you seek the views of your specialist conservation and archaeological advisers, as relevant.

It is not necessary for us to be consulted on this application again, unless there are material changes to the proposals. However, if you would like detailed advice from us, please contact us to explain your request.

Lincoln Civic Trust

OBJECTION - We wish to object as it was felt that there was insufficient detail in the application as to the detailed design, the materials and an explanation as to the decision to reduce the number of car parking spaces to be provided.

Lincolnshire County Council, as Education Authority

The County Council has no comments to make on the current reserved matters application for this student accommodation scheme and notes that this also resolves condition 7 of planning permission 2017/0721/OUT in relation to education and residential housing (use class C3) on the site.

Kind regards

Simon Challis
Strategic Development Officer
Corporate Property

Lincolnshire Police

Thank you for your correspondence and opportunity to comment on the proposed development.

There are no further observations beyond the report which was submitted at the time of the application for outline planning permission. I have included an amended copy of that report for your convenience and the information of the developer.

Historically Student Accommodation can become vulnerable to crime and anti-social behaviour therefore it is important that the best security arrangements and provision are planned for such premises, this is particularly so when such a development is very central to a bustling city centre.

The safety, security and general well being of student should be of paramount importance when considering the detail of this application. The site is centrally located and has an entrance that exits onto a busy area of Lincoln, within the centre of Lincoln nightlife? The following aspects of security should be rigorously applied to this building.

Lincolnshire Police has no formal objections to the planning application in principle but would recommend that the initial advisory recommendations are implemented.

External doors and windows

The potential for unwanted guests will be considerable at this location and therefore robust measures should be installed to ensure the security and safety of student residents. I have some concerns that access may be easily gained via either of the shown entrances and the risk of 'follow through' entry gained. I would recommend that an air-lock style entrance vestibule is incorporated into the design (to help prevent unauthorised follow through access) commensurate with an access control system, with an electronic door release, and visitor door entry system that provides colour images, and clear audio communications linked to each individual unit. Under no circumstances should a trade person release button or similar uncontrolled access method be used.

An Industry standard approved CCTV system should be installed covering all communal points of entry and lobby areas. This system must be able to capture and record all persons using the entry system.

The secured by design requirement for all dwelling external doors is PAS 24:2016 or Bespoke equivalent (doors of an enhanced Security) or WCL 1 (WCL 1 is the reference number for PAS 23/24 and is published by Warrington Certification Laboratories).

All ground floor windows and doors and those that are easily accessible from the ground must conform to improved security standard PAS24: 2016. **All ground floor windows should have window restrainers and effective locking systems.**

I would recommend that all ground floor and easily accessible windows have at least one pane of laminated glass.

Access to Places of Height

It is important that access to places of height (prevention of suicide) is secured on all levels and should include the provision of substantial windows and locking systems together with fixed and secured 'window restraining' devices. Any points of access to the roof area or other place of height should be secured by way of 'appropriate' fire compliant locking systems.

Car Park (Lower Levels)

Whilst not clear in the plans I would ask that there is no opportunity for uncontrolled or unrestricted access from the car parking area to the accommodation levels of this development. I would also recommend that access control for both pedestrians and vehicles is such that the underground (lower level) parking areas do not become a focal point for anti-social or criminal activity due to the possible ease of access.

Effective lighting and monitored CCTV should form part of the overall safety and security of this development and as such measures to ensure effective policing of this development are important to ensure the safety of residents and users.

Individual Flat or Unit Doors.

Flat entrance door-sets should meet the same physical requirements as the 'main front door' i.e. PAS24:2016. The locking hardware should be operable from both sides of an unlocked door without the use of the key (utilising a roller latch or latch operable from both sides of the door-set by a handle). If the door-set is certified to either PAS24:2016 or STS 201 Issue 4:2012 then it must be classified as DKT.

Homes of Multi-Occupancy / Student Accommodation – Communal Areas & Mail Delivery

Where communal mail delivery facilities are proposed and are to be encouraged with other security and safety measures to reduce the need for access to the premises communal letter boxes should comply to the following criteria.

- Located at the main entrance within an internal area or lobby (vestibule) covered by CCTV or located within an 'airlock style' entrance hall.
- Be of a robust construction (Federation Technical Specification 009 (TS009)
- Have anti-fishing properties where advised and appropriate.
- Installed to the manufacturers specifications.
- Through wall mail delivery can be a suitable and secure method.

Lighting

Lighting should be designed to cover the external doors and be controlled by *photoelectric cell* (dusk to dawn) with a manual override. The use of low consumption lamps with an efficacy of greater than 40 lumens per circuit watt is required; it is recommended that they be positioned to prevent possible attack.

Cycle Storage Structure (if to be included)

Generally pedestrian access doors-sets to commercial units should be certified to LPS 1175 security rating 2. The access controlled door should be designed in such a way that the hinges and door-sets are of a non-lift nature and non-tamper proof. The door locks must be operable by way of a thumb screw turn to avoid any person being accidentally locked in the cycle storage area.

Lighting within cycle storage area; automatically activated passive infra-red lighting should be considered rather than permanent lighting to which other users become accustomed and therefore activation would not draw any attention. Lighting units should be vandal resistant energy efficient light fittings.

Bin Storage (not seen)

Internal communal bin and bicycle stores within blocks of flats must have no windows and be fitted with a secure door set that meets the same physical specification as 'front door' and specifically Section 2, paragraphs 21.1 to 21.6 and 21.8 to 21.13.

This will ensure that such stores are only accessible to residents. The locking system must be operable from the inner face by use of a thumb turn to ensure that residents

are not accidentally locked in by another person. A bicycle store must also be provided with stands with secure anchor points or secure cycle stands.

External bins stores and home composting containers (supplied to meet 'Code for Sustainable Homes' 'Was 3') should be sited in such a way that they cannot be used as a climbing aid to commit crime.

Please do not hesitate to contact me should you need further information or clarification.

Please refer to *Commercial Guide 2015 & New Homes 2016* which can be located on www.securedbydesign.com

Crime prevention advice is given free without the intention of creating a contract. Neither the Home Office nor the Police Service takes any legal responsibility for the advice given. However, if the advice is implemented it will reduce the opportunity for crimes to be committed.

Yours sincerely,

John Manuel
Force Designing Out Crime Officer

Neighbour Comments

Mr. T. Foley (7 Swan Street)

I am responding to your correspondence dated, 11th May 2013, relating to an Application for Approval of Reserved Matters - both my father and I oppose the application. For the sake of brevity, I adopt the arguments contained within my prior correspondence dated, 26th July 2017 and 28th February 2018 - we also oppose the application based upon the following:

a) Layout

A review of the recently submitted Planning Application documents reveals that a six-storey building incorporating student accommodation and car parking will be developed at the subject site. It is interesting to note that nowhere in the developers submitted documents does it confirm the development of a student building. Rather deceptively, the only documents that make reference to this fact are the Council's 11th May 2018 and Lincolnshire Police's 15th May 2018 correspondence.

The proposed Floor Plans (Ground Floor to Fourth Floor) have been altered and amended, they now depict 118 tiny, tightly packed/condensed single room student apartments and a reduction in the number of kitchens on each floor from three to two.

This is an increase of twenty apartments over the 98 apartments depicted in the Revised Application/Plans for Outline Planning Permission and more than the 105 apartments depicted in the Application for Outline Planning Permission. It is clear that increasing the number of student apartments and decreasing the number of kitchens is simply a way to maximize profits. This over-developed layout will destroy the peace, character and heart of the Cultural Quarter, at the expense of local residents. Such a large over-developed student building should not be permitted - **the developer should be directed to scale back the number of student apartments - no more than fifty should be approved;**

b) *Scale and Height*

The developer's Application for Approval of Reserved Matters reveal no change in the developments scale and height, however, it's still far too large and dominating. It will overlook the Swan Street Apartment building located directly opposite (see the Proposed North and East Elevation Visual). There is also no change to the building footprint/circumference, every inch of the site is occupied by the proposed development, overcrowding other nearby buildings. The application reveals that there will be twenty-six car park spaces at the Lower Ground Floor level, however, one change is the elimination of car park spaces at the Ground Floor level. Student apartments will now replace those missing car park spaces, thereby, maximizing profits for the developer. **The application clearly depicts no reduction in scale and height.** The proposed development needs to be setback from the site boundary and significantly reduced in size and height - **no more than 3-4 storeys, including car park should be permitted;**

c) *External Appearance*

More detailed plans need to be submitted regarding external materials, colours and textures, etc. The Elevation Visuals only contain very basic details regarding external materials and some visuals do not contain any information, including the Roof Plan. In my prior correspondence opposing the applicant's Planning Application I requested that the proposed development be referred for a National Design Review - what is the status of that request?

d) *Means of Access*

Absent from the developer's application for Approval of Reserved Matters is information detailing: *"means of access to, and service roads for the development, including road widths, radii and sight lines, space for loading, unloading and maneuvering and turning of service vehicles and their parking; space for parking and maneuvering."* **Therefore, the application should be denied in its entirety as being insufficiently submitted.** Both Swan Street and Grantham Street are extremely narrow streets - a proposed development of such magnitude will cause major congestion, be extremely disruptive for local residents and a safety issue for pedestrians;

e) *Landscaping*

Other than details regarding the installation of a green roof (see Landscaping Details) no other details have been provided regarding a scheme of landscaping for the proposed development, including information regarding: *"surface treatments, walls, fences, or other means of enclosure, including, materials, indications of all existing trees and hedgerows on the land, and details of any to be retained, together with measures for their protection in the course of development."* **Therefore, the application should be denied in its entirety as being insufficiently submitted;**

f) *Effects on Trees*

Without a valid reason, none of the trees in the area of the proposed development should be removed. The applicant should be directed to incorporate the existing trees into the proposed development - if any trees do require removal, new tree plantings should then occur;

g) *The Reserved Matter for the scale of the development shall be for development no greater than the parameters shown in the drawings listed within Table A*

The application for Approval of Reserved Matters does not include updated drawings for Visual No. 0006 and 0007 as indicated in Table A. The applicant should be directed to provide these updated visuals;

h) *Archaeological Work/Foundation Design*

Has a programme of archaeological work been undertaken and a foundation design devised by the applicant in compliance with the National Planning Policy Framework? A report of the findings of the evaluation and subsequent assessment impact are not contained within the list of Planning Application Documents made part of the Application for Approval of Reserved Matters;

i) *Points of Extraction/Stationary External Plant or Machinery*

The Application for Approval of Reserved Matters does not include details of the space(s) and location(s) of any points of extraction and stationary external plant or machinery required to serve the building. The applicant should be directed to provide a detailed response, including a noise impact assessment for any and all stationary external plant or machinery;

j) *Safe and Secure Cycle Parking/Storage/Electric Vehicle Recharge Points*

The proposed Ground Floor Plan and Lower Car Park Plan Visuals do not depict details of safe and secure cycle parking/storage or electric vehicle recharge points within the building. The applicant should be directed to provide a detailed response regarding same;

k) *Written Scheme of Investigation*

Pursuant to §12 of the National Planning Policy Framework the applicant is required to secure the implementation of an appropriate programme of archaeological work undertaken by a competent person/organization in accordance with a Written Scheme of Investigation (WSI). Has the applicant submitted a WSI?

l) *Parking*

Has the applicant submitted proposals for the parking of construction vehicles; the layout of the site compound and construction working and delivery hours?

m) Site Contamination/Remediation Scheme/Scheme of Drainage

Has the applicant completed an investigation and risk assessment to assess the nature and extent of any contamination on the site, as well as, detailed remediation and drainage scheme?

n) External Lighting

Has the applicant submitted details of the design of external lighting for the proposed development, including an assessment of the offsite impact of the lighting?

o) Noise and Disturbance

Lincolnshire Police's correspondence dated, 15th May 2018 states that, "historically Student Accommodation can become vulnerable to crime and anti-social behavior." The applicant's increased building size to 118 student apartments located right on my doorstep will most certainly cause unwanted noise and disturbance at all hours of the night and day, preventing and disturbing sleep. The building's entrance still remains directly opposite my Swan Street apartment (see the proposed West Elevation Visual) - **this entrance needs to be relocated to Grantham Street or Flaxengate AWAY from the residential apartments located directly opposite.** In my prior correspondence opposing the applicant's Planning Application I requested that a Noise Action Plan be conducted for the proposed development - what is the status of this request?

The proposed student building will also attract crime to the Cultural Quarter area, including the Swan Street apartment building. This is confirmed by Lincolnshire Police's above-noted correspondence, which states that, "the potential for unwanted guests will be considerable at this location and therefore robust measures should be installed to ensure the security and safety of student residents." What measures will the applicant take towards other nearby residents, regarding crime prevention and noise and disturbance? Will the applicant pay for a new entrance, CCTV and soundproof windows at the Swan Street apartment building? For all of the above noted reasons, **student accommodation should not be permitted;**

p) Loss of Light

There will be a significant loss of light caused by the proposed development. The development will reduce the level of natural light entering the Swan Street apartment building, including my apartment, which will be cloaked in constant darkness (see the proposed West Elevation Visual). The proposed development is far too tall/large and too close to my apartment building. The proposed development needs to be reduced in height and set back from the site boundary;

q) Overlooking

The much taller/larger proposed developments living spaces will create views into my apartment, causing a lack of privacy and obtrusive light intrusion. **The windows of the proposed development need to be re-designed to minimize overlooking.**

The Application for Approval of Reserved Matters is severely deficient in many required areas and should be denied in its entirety. The applicant's sole interest is profit at the expense of local residents and the Cultural Quarter, of which it has shown no regard.

The development of a student building WILL destroy the Cultural Quarter. I feel that all parties involved in the granting of Outline Planning Permission have failed to truly envision what the Cultural Quarter could have become. Every city, including Lincoln, needs a cultural area where a creative industry/hub is located, including museums, art centres, galleries and artist studios, etc. Living in Chelsea, New York and also working in the arts industry, I know this only too well - Chelsea is a vibrant arts centre containing over 300 art galleries, museums and artists - attracting millions of tourists and investment every year. Lincoln had the opportunity to likewise create a similar area in the Cultural Quarter, attracting tourists and investment - the development site would be much better suited to a Lincoln Museum of Modern Art¹ or an arts center/art galleries. A large student building does not belong here. The Council should carefully rethink their approval before it's too late.

¹ This could be along the same lines as Nottingham Contemporary - funding from various sources and donated artwork could be arranged.

Richard Tibenham (Greenlite Energy Assessors)

Thank you for recent communication concerning the Consultation on application for Approval of Reserved Matters for the proposed development at Grantham Street, Lincoln.

With respect to my initial communication dated 25.03.18, I am delighted to see that either as a result of my comments, or due to other factors, a more conservative stance has been taken to glazed areas and resultant solar gains, as included in the latest architectural drawings. I believe this will have a significant benefit to the welfare of occupants, the long term energy demands of the building and the capital build costs.

I note from the current drawings that the building is proposed to be used for student accommodation. As such, I don't anticipate the inclusion of mechanical cooling and the resultant energy demands from such systems under this usage. Without the use of mechanical cooling, the potential for summertime overheating risks is increased.

As noted in my previous communication, whilst the building regulations include criteria to mitigate high solar gains, there is no policy within the building regulations to police overheating risks. The mitigation of overheating risks remains the responsibility of the *developer and design team*. Being high density student accommodation, with only single sided natural ventilation, the building falls under a high risk building type with respect to overheating risks. This occurs due to the high density of internal gains in the building, low external surface area to volume ratio, and more importantly, the limited ability to achieve cross-flow natural ventilation. In order to ensure that new infrastructure in the town is fit for purpose during periods of high external temperatures, such as those that we are currently experiencing, and under more demanding future climate change scenarios, I would advocate that a suitable performance standard of summertime thermal comfort is included within the design brief, such as that defined within CIBSE Technical Memorandum 59.

The London Plan already includes such policy, in the interests of ensuring that the local built environment serves the community well. Whilst I recognise that these issues are on the fringes of current planning policy, with the housing market in the state of disfunction it is in, consumers are often left with limited choices. For this reason, I believe that planning authorities or the building regulations should help ensure that buildings do not incur excessive summertime temperatures. With the building regulations currently not regulating this issue, it falls on the planning authority to help ensure that good standards are met if the developer is otherwise not interested in doing so.

These opportunities are normally only viable during the initial design and build stage of the building, or during deep refurbishment. Hence, once a building is built, this normally sets the energy demand and the thermal comfort performance of the building for several decades. With the forecast for future weather scenarios including higher summertime temperatures, I believe that building in resilience at the build stage is a sensible approach in all parties medium-long term interests.

I intend for these comments to be taken as my personal viewpoint and recommendation. I remain largely ambiguous to the development in principle. If you would like to engage further on the matter, I would be most interested in doing so.

Application Number:	2018/0547/FUL
Site Address:	129 Yarborough Road, Lincoln, Lincolnshire
Target Date:	22nd June 2018
Agent Name:	Evolution Design
Applicant Name:	Mr & Mrs C Appleton
Proposal:	Erection of a three storey dwelling.

Background - Site Location and Description

Site Location

129 Yarborough Road is located on the east side of Yarborough Road close to its junction with Carline Road. The plot of land is located to the rear of the host property and forms part of the rear garden. It is currently occupied by a single storey garage.

The development is located within Conservation Area No.8 - Carline.

Description of Development

Permission is sought to erect a dwelling to the rear of 129 Yarborough Road with access taken from Carline Road. The property would provide 4 bedrooms split over three floors and have off street parking and private external amenity space.

Site History

No relevant site history.

Case Officer Site Visit

Undertaken on various dates during the pre-application process. Photos taken on 08/06/2018.

Policies Referred to

- National Planning Policy Framework
- Central Lincolnshire Local Plan
 - Policy LP1 A Presumption in Favour of Sustainable Development
 - Policy LP25 The Historic Environment
 - Policy LP26 Design and Amenity

Issues

- National and Local Planning Policy
- The principle of the development
- Impact on the character and appearance of the conservation area
- Residential amenity
- Highways
- Loss of trees

Consultations

Consultations were carried out in accordance with the Statement of Community

Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Lincoln Civic Trust	Comments Received
Lincolnshire Police	Comments Received
West End Residents Association	No Response Received
Highways & Planning	Comments Received

Public Consultation Responses

Name	Address
Mr Matthew Corrigan	7 Carline Road Lincoln LN1 1HL
Mr Lee Clark	5 carline road Lincoln LN1 1HL
Mr And Mrs Came	9 Carline Road Lincoln LN1 1HL
Mr And Mrs Eve	11 Carline Road Lincoln LN1 1HL
Mr Jon Alexander	135 Yarborough Road Lincoln LN1 1HR
Mr Michael Cowling	13 Carline Road Lincoln LN1 1HL

A number of objections have been received from neighbours. The issues can be summarised below:

- Inappropriate back yard development
- It would add more vehicles to the road
- Lack of parking on Carline Road
- Overlooking to 135 Yarborough Road
- Loss of trees
- Design
- Impact on sunlight to 135 Yarborough Road

- Contrary with Local Plan Policy
- Air pollution
- Sets a precedent for further buildings on rear gardens fronting Carline Road
- Slope stability
- Impact on views down the hillside

These issues will be considered within the main body of the report.

Consideration

Impact of the Proposed New Development on the Character and Appearance of the Conservation Area

Whilst the proposed property would be located within the garden of 129 Yarborough Road it would actually front Carline Road and would therefore need to take reference from the properties in this area in terms of relating to scale and design. This is supported in the NPPF which states that "Planning decisions should not attempt to impose architectural styles or particular tastes and they should not stifle innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles. It is, however, proper to seek to promote or reinforce local distinctiveness." Similarly Local Plan Policy LP26 requires the development to respect the existing topography, landscape character and identity, and relate well to the site and surroundings, particularly in relation to siting, height, scale, massing and form.

The applicants have considered the siting of the development and its relationship with Carline Road. Pre application advice on the scheme was sought from the Planning Authority and concern was raised at this time about the elevation fronting Carline Road. This issue has also been raised by neighbouring residents. To overcome this issue the applicants have retained the existing boundary wall to Carline Road but have added a gated entrance to act as an entrance to the site. When viewed from Carline Road, as shown on the sections, the ground floor of the property, where the entrance to the property would be expected to be found, is obscured by the boundary wall. A number of windows to the first floor have been added so as not to present as a blank elevation to the street. Neighbours have also raised concern about the positioning of the building in relation to Carline Road. However, it is common for properties on this side of the road to be built at the back edge of the pavement, with examples immediately to the east, and therefore the positioning of the proposal is appropriate.

The Planning Authority is therefore satisfied, that given the design alterations, and taking into account the site topography, the property would assimilate into the surroundings and would not have a harmful visual impact.

Whilst of seemingly modern design the proposed dwelling has taken reference from nearby properties, windows are of the correct proportions and the property has used traditional corbelled brick detailing to reference nearby properties. The proposal does not copy the architecture found on Carline Road but there is no requirement for developments to replicate previous designs in accordance with the NPPF.

Taking into account the prominence of the site it is essential that the applicant use appropriate, high quality materials which reinforce or enhance local distinctiveness, with consideration given to texture, colour, pattern and durability. The applicants originally proposed a modern palette of materials, however it was considered that whilst the design

had some modern elements it would be more appropriate, given the location within a Conservation Area, for the proposal to be constructed of good quality materials which reference the surrounding context. Therefore the applicants have suggested an Ibstock Leicester Red Stock brick, natural slate roof tiles, grey aluminium windows and doors and black rainwater goods. The Planning Authority is comfortable that these materials would assimilate well into the surrounding area.

Due to the topography of the site, the properties on Carline road to the north east sit higher on the hillside than the proposed property. It is therefore considered that the scale of a two storey property would be appropriate and the overall massing of the development would be acceptable. The property does sit over three floors but this is due to the slope of the site, the additional height to the rear of the site relates to the properties fronting Yarborough Road.

Residential Amenity

Policy LP26 of the Central Lincolnshire Local Plan states that "the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy must not be unduly harmed by or as a result of development."

The garden to No.129 is substantial and slopes considerably. It is considered that even with the erection of a dwelling in the garden there would be sufficient amenity space for both the host dwelling and the proposed dwelling without compromising the character of the area. A 1.8metre high perimeter fence would separate the new dwelling from the existing. The proposed dwelling has been aligned with Carline Road which means that the property sits at an oblique angle to the properties on Yarborough Road, this would avoid any direct overlooking. As well as this there is substantial landscaping along the south and north boundaries to further prevent overlooking with the neighbours.

Comments have been received from neighbours at 135 Yarborough Road about overlooking and loss of sunlight. It is considered, that given the positioning of the proposed dwelling, the dwelling in between the two sites and the distance from 135 Yarborough Road, in excess of 25metres away, the property would not have an adverse impact on privacy or light.

Although this is a relatively small development, due to the close proximity to neighbouring sensitive uses, there is potential for problems due to noise from the construction phase of the development, particularly during the noise sensitive hours. Therefore a condition should be attached to any consent granted to restrict construction to appropriate hours.

Loss of Trees

The proposal would result in the loss of a pear tree which would be replaced elsewhere within the site. No further changes to the soft landscaping are proposed as a result of this development and it is not considered that the removal of a single tree, to be replaced, would be harmful.

Land Stability

Paragraph 120 of the NPPF states that "To prevent unacceptable risks from pollution and land instability, planning policies and decisions should ensure that new development is appropriate for its location. The effects (including cumulative effects) of pollution on health,

the natural environment or general amenity, and the potential sensitivity of the area or proposed development to adverse effects from pollution, should be taken into account. Where a site is affected by contamination or land stability issues, responsibility for securing a safe development rests with the developer and/or landowner."

The site is in an area where slope stability has occurred, but with similar developments in the area piled retaining walls have been used to intercept potential slip planes and allow excavation and filling activities to be undertaken whilst ensuring that an acceptable factor of safety against failure be maintained.

The applicants have submitted a structural engineer's report which considers that following further site investigation a suitable method for the construction of the substructure and foundations would be possible. Further detail could be secured by condition to give the planning authority comfort that, structurally, the property can be built.

Highways

The Highways Authority have raised no objections to the proposed scheme.

A number of objections have been received which cite the issue of parking on Carline Road. The proposed dwelling would come with off street parking with a garage and driveway. It should also be noted that permission has been granted for a new access to the front of 129 Yarborough Road which would serve the existing property and ensure that both the existing and the proposed property have dedicated off street parking provision.

Contaminated Land

A condition should be attached to ensure that any unexpected contamination is reported to the Local Planning Authority. This is to ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

Conclusion

It is considered that the applicants have addressed the constraints of the site and the proposal is appropriate in this location. The proposal is therefore in accordance with national and local planning policy.

Application Determined within Target Date

Yes.

Recommendation

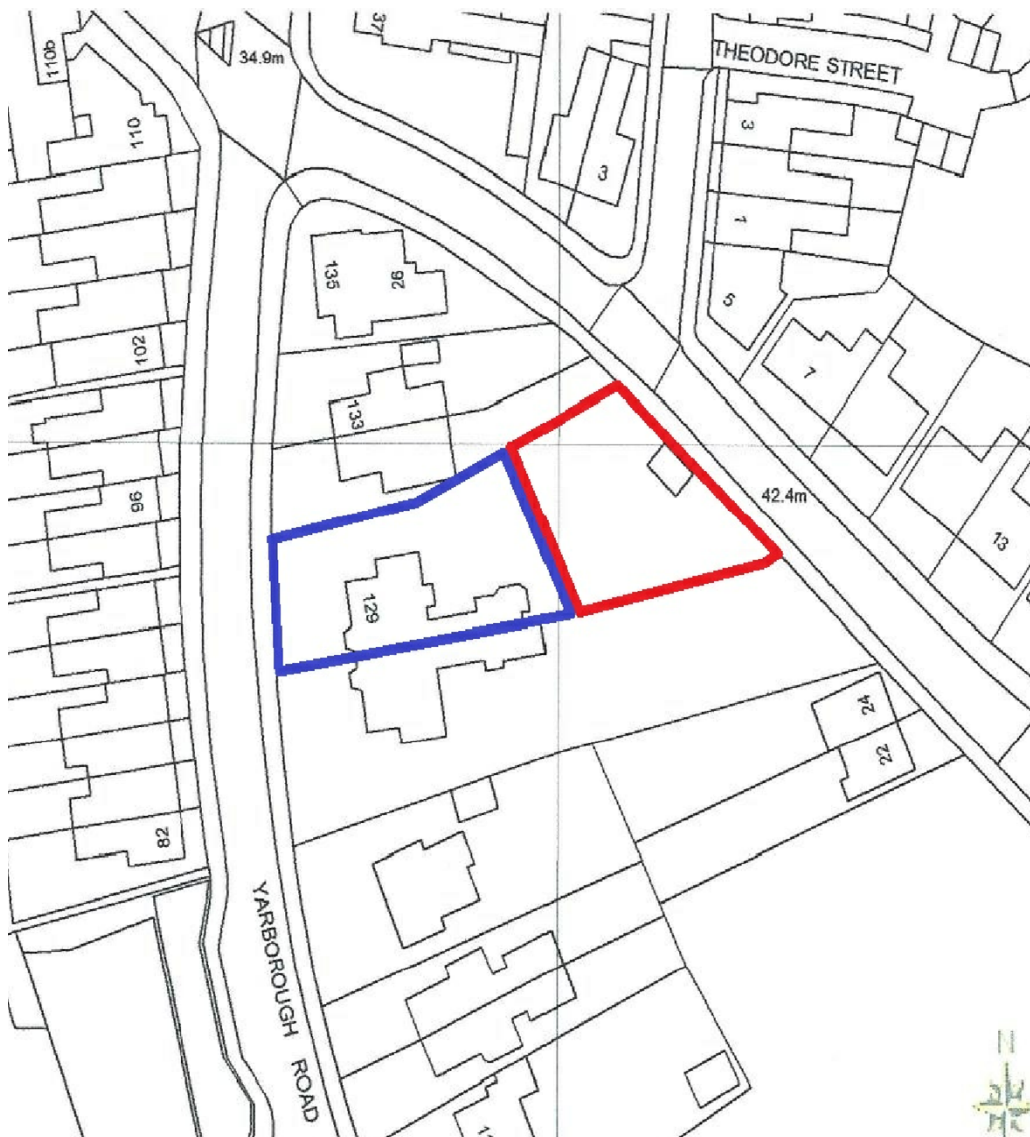
That the application is granted conditionally.

Conditions

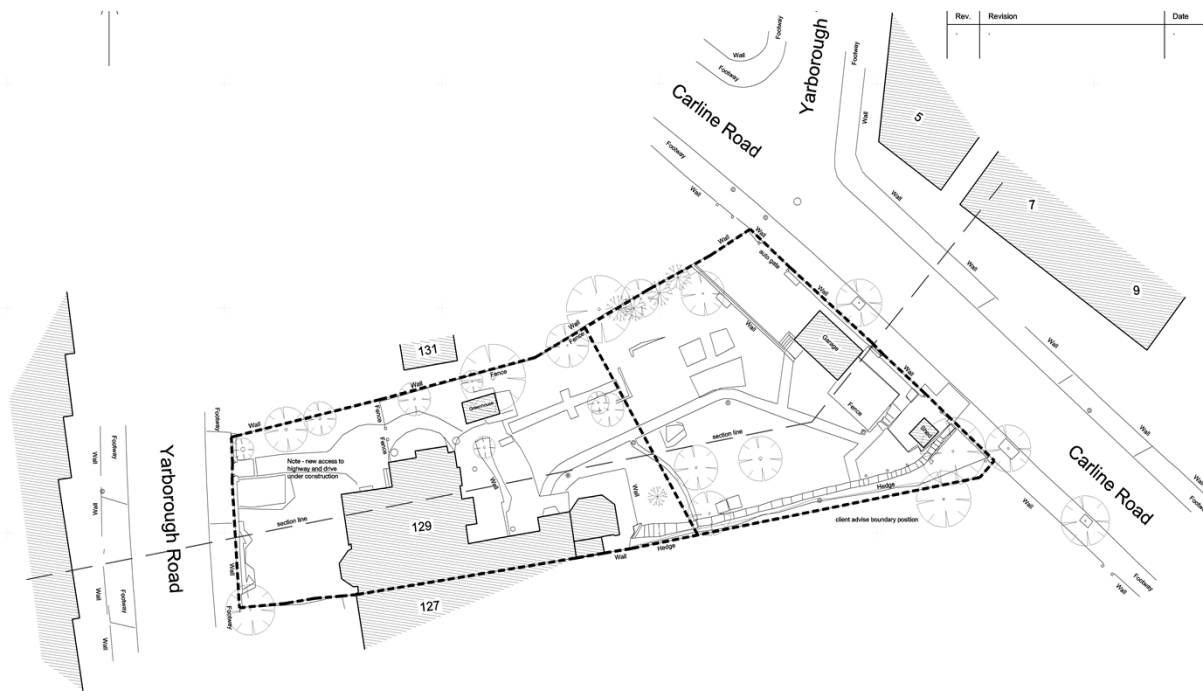
- Development to commence within 3 years
- Development to be carried out in accordance with the plans

- Land stability and foundation design details
- Surface water details
- Contaminated land
- Hours of working

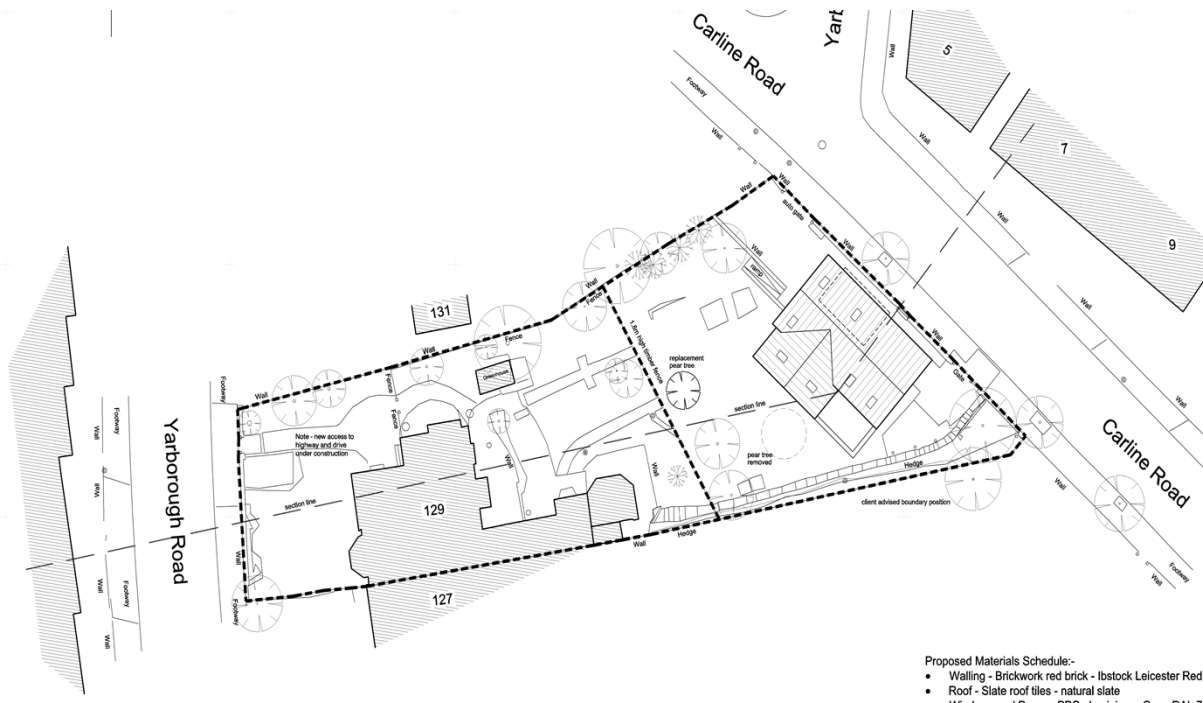
Site Location Plan



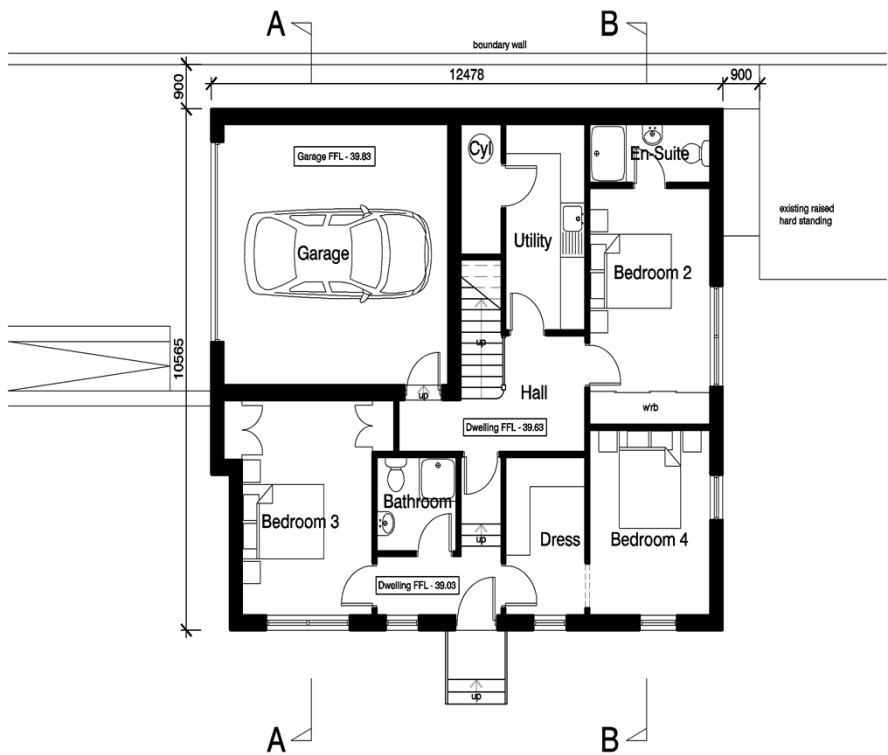
Existing Site Layout



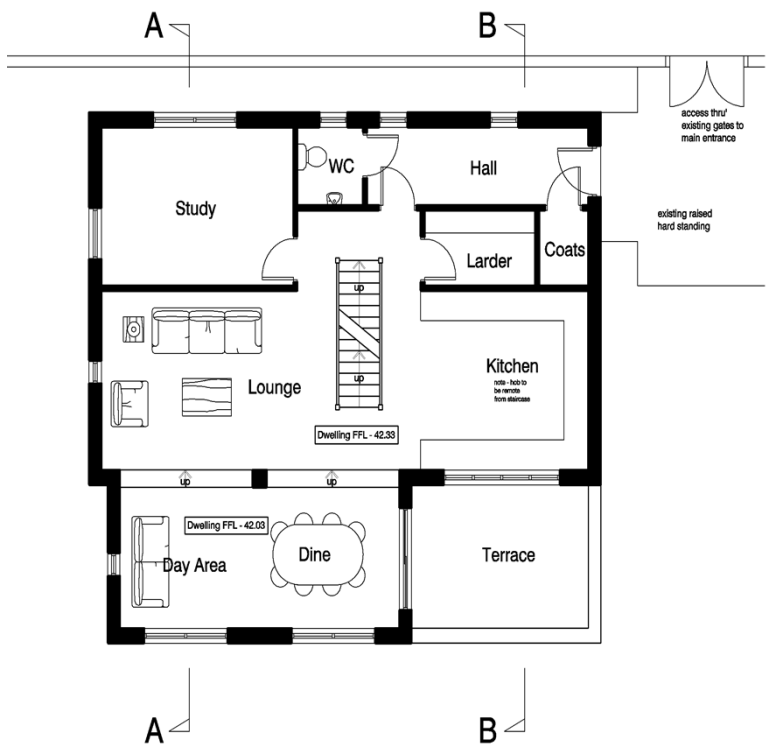
Proposed Site Layout



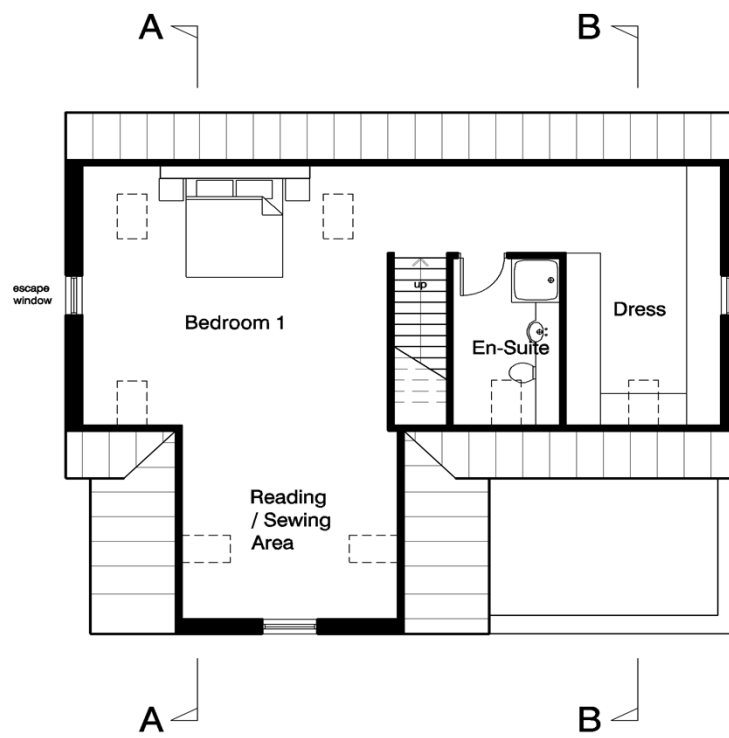
Proposed Ground Floor



Proposed First Floor



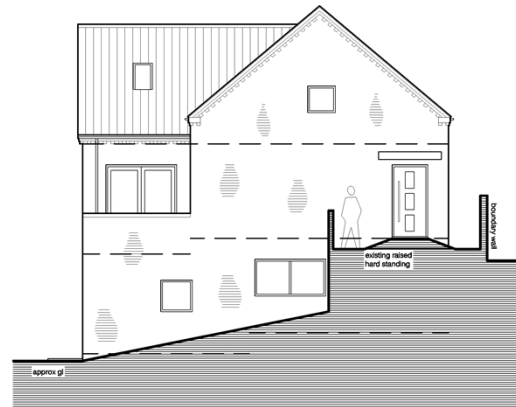
Proposed Second Floor



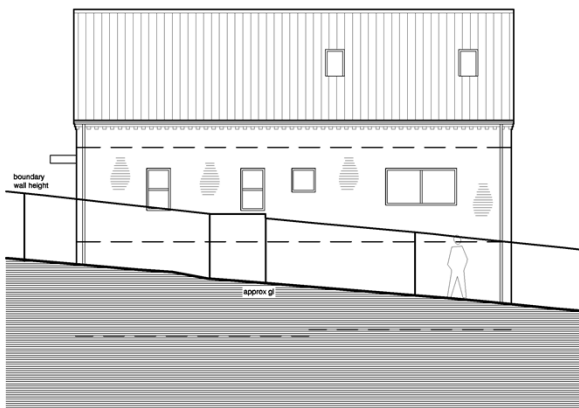
Proposed elevations



South West Elevation



South East Elevation



North East Elevation



North West Elevation

Site Photos



Looking to the rear of 129 Yarborough Road



Looking to the rear of 131 Yarborough Road



Within site looking at the auto gate



Within the site towards the rear boundary wall



Looking to the rear of 129 Yarborough Road



Looking across to 127 Yarborough Road



Eastern corner of the site



Existing garage on site



Outside site looking west on Carline Road



Outside site looking East on Carline Road



Standing on Carline Road, site on the left



Looking across Carline Road at the site



Looking east on Carline Road, site on the right.

Consultee Comments

Environment & Economy

Lancaster House
36 Orchard Street
Lincoln LN1 1XX
Tel: (01522) 782070
E-Mail: Highwaysdsupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2018/0547/FUL

With reference to this application dated 26 April 2018 relating to the following proposed development:

Address or location

129 Yarborough Road, Lincoln, Lincolnshire, LN1 1HR

Date application referred by the LPA
1 May 2018

Type of application: Outline/Full/RM/
FUL

Description of development

Erection of a three storey dwelling

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Does not wish to restrict the grant of permission.

CONDITIONS (INCLUDING REASONS)

NO OBS

Having given due regard to the appropriate local and national planning policy guidance (in particular the National Planning Policy Framework), Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) has concluded that the proposed development is acceptable. Accordingly, Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) does not wish to object to this planning application.

HI03

Prior to the submission of details for any access works within the public highway you must contact the Head of Highways - on 01522 782070 for application, specification and construction information.

HI08

Please contact Lincolnshire County Council Streetworks & Permitting team on 01522 782070 to discuss any proposed statutory utility connections and any other works which will be required in the public highway in association with this application. This will enable Lincolnshire County Council to assist you in the coordination and timings of such works.

Consultee Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Consultee Details

Name: Ms Catherine Waby

Address: St Mary's Guildhall, 385 High Street, Lincoln LN5 7SF

Email: lincolncivictrust@btconnect.com

On Behalf Of: Lincoln Civic Trust

Comments

OBJECTION - We have for sometime been concerned about back-yard development and have consistently 'objected' when applications have been made. We feel here that the new property is actually a new build on Carline Road and not an alteration to a property on Yarborough Road. This could set a precedent for others in the area to follow and apply to build at the rear of the existing properties. Carline Road is virtually, due to parking, a single-track road and this and other applications would add more vehicles to the road usage. We also note that from the plans that the application appears to require the removal of a tree which for obvious reasons should not be allowed.

Neighbour Comments

Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Jon Alexander

Address: 135 Yarborough Road Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: The proposed development will have a number of negative effects on Carline Road and my property in particular. We purchased our property in November 2017 and one of the reasons was the location, the neighbours, lack of properties overlooking our rear garden and our house, and the views from the upstairs of our property looking down Carline Road. The rear of our property backs onto Carline Road and access for our car is from Carline Road.

1. Carline Road is a very busy road regarding the parking of cars and on occasions we are unable to park within a 100 metres of our house. Another property within the immediate vicinity will not help this but only make the issue of parking more difficult.

2. A three storey property at the end of our garden is now going to overlook our property and will drastically reduce our garden privacy. We are a family who enjoy our outdoor space and this property will have a dramatic effect upon our quality of life.

3. I note from the planning application that a tree is to be felled as part of the building process. This has an effect on the environment as trees help reduce the CO2 in the atmosphere. We would rather look at a tree than a property at the end of our garden. Once spring comes to the UK it is a pleasure to hear the birds first thing in the morning and less trees will mean less birds.

4. The properties opposite the proposed development on Carline Road are all of a certain period and architecture and are a pleasure to look at. How will this property add to or improve that.

5. At present our rear garden receives direct sunlight from morning to early evening and the proposed development will probably impact upon our morning sunlight.

Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Jon Alexander

Address: 135 Yarborough Road Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: The proposed development will have a number of negative effects on Carline Road and my property in particular. We purchased our property in November 2017 and one of the reasons was the location, the neighbours, lack of properties overlooking our rear garden and our house, and the views from the upstairs of our property looking down Carline Road. The rear of our property backs onto Carline Road and access for our car is from Carline Road.

We park our car on Carline Road along with most of the residents and at times we have been unable to park within 100 metres of our house. Another property can only make the parking situation worse and cannot improve it.

I notice from the plans that a tree is to be felled as part of the development. Trees are a very important part of reducing the CO2 within our environment. Also once spring arrives in the UK there is the opportunity early in the morning to hear the "dawn chorus". Reducing the number of trees can only have a detrimental effect on this.

Currently there are no properties that directly overlook our rear garden. If this development is allowed to happen then at the end of our rear garden there will be a three-storey dwelling that will be able to look directly into our rear garden. We are a family that enjoy our outdoor space as much as the weather permits and a property overlooking our rear garden will make us feel very uncomfortable.

Currently our rear garden enjoys the daily sunshine from early morning to early evening. The building of this property will have a detrimental effect upon our morning sunshine.

Currently the properties opposite the proposed development on Carline Road were built in a certain period and the architecture is from that period. How will the new development fit in with its

historic surroundings.

The view from the rear upstairs of our property is down Carline Road looking at the afore mentioned properties. However if this development occurs that view will be dramatically impacted upon.

Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Lee Clark

Address: 5 Carline Rd Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: We are proposing to object the following planning permission on the following basis: __This planning submission does not meet the intended spirit, goals and objectives of The Central Lincolnshire Local Plan. This application does not uphold: __Policy LP17: Landscape, Townscape and Views _Policy LP21: Biodiversity and Geo diversity_Policy LP25: The Historic Environment Conservation Areas_Policy LP26: Design and Amenity_Policy LP29: Protecting Lincoln's Setting and Character __We believe the following areas require further clarity: __Scale & Height __It has not been established if this proposal has structural integrity, with no safety impacts to the surrounding excavation area. __Highway Safety & Congestion __No pre or post risk assessment has been retained within this application. It is our view that Carline road already has a number of safety & congestion controls in place (restricted parking access & permitted numbers of vehicles + a no right turn traffic flow control, 250m away from the proposed dwelling). By adding a multi occupancy dwelling this places greater risk on safety & infrastructure resources. __Air Pollution __Though this proposal in isolation is not the sole source of pollution it is the accumulative effect of ongoing policy to grant planning permission for properties in a city that is trying to meet it's obligations of part 6 of the Environment Act 1995. __Design / Appearance / Layout __As stated. __This application does not uphold or enter in the spirit of local policy: __Policy LP17_Policy LP21_Policy LP25_Policy LP26_Policy LP29 __

Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Lee Clark

Address: 5 carline road Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

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Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Michael Cowling

Address: 13 Carline Road Lincoln

Comment Details

Commenter Type: Member of the Public

Stance: Customer made comments neither objecting to or supporting the Planning Application

Comment Reasons:

Comment: Whist the address of this property is given as Yarborough Road, it will, according to the plans, be a property that has entrance/exit points on Carline Road.

As residents of Carline Road, we (my wife and I) are concerned about a number of issues which we wish to be seriously considered prior to any decisions being made.

* The idea of building another property in ones garden is a contentious issue and if permission is granted sets a massive precedent for similar houses in this area. This really needs careful consideration for any future development in this area.

* Houses built on the sides of slopes where piling is undertaken can affect the surrounding buildings - there are numerous examples to be cited in this area and the houses on the northern side of Carline Road are potentially at risk.

*The area currently has street parking issues - another property will only intensify this problem.

* The design quality and appearance of the proposed build needs to be in keeping with and complement what is a unique Edwardian terrace in Lincoln.

Thankyou

9 Carline Road
Lincoln LN1 1HL

Mr K Manning
Planning Manager
City of Lincoln Council
City Hall
Beaumont Fee
Lincoln LN1 1DF

20 May 2018

Proposed development at 129 Yarborough Road, Lincoln

Dear Mr Manning

Thank you for your letter of 1 May 2018 informing us of a proposed development at the above address on Yarborough Road. We are replying in order to state our objection to the proposal.

Carline Road is a major conservation feature of West Lincoln. Comprising entirely Edwardian houses, the lower end of Carline Road curves gracefully up from Yarborough Road towards The Lawn, the Castle, and the Cathedral. The Edwardian houses on the north-east side of Carline Road are perfectly balanced by, and spaced from, the back gardens of the Edwardian houses that front onto Yarborough Road and back onto Carline Road's south-west side (of which number 129 Yarborough Road is an example).

The proposed development would alter and diminish the character of the road in the following ways:

1. The proposed house would be the only house in the lower half of Carline Road that would be not Edwardian.
2. The proposed house is of mundane and un-aesthetic modern design with no appealing features or architectural qualities.
3. The proposed house would be the only house that would not be located and oriented in a manner in keeping with the existing houses in the area: its location would be such that it would not be spaced from the road as all other houses are; its orientation, with the back of the house facing Carline Road, would also be out of keeping with all the other houses.
4. In short, the proposed house would be an eyesore, damaging to the architectural quality of West Lincoln and significantly diminishing the local and tourist appeal of the City.

Yours sincerely

Peter Came

Comments for Planning Application 2018/0547/FUL

Application Summary

Application Number: 2018/0547/FUL

Address: 129 Yarborough Road Lincoln Lincolnshire LN1 1HR

Proposal: Erection of a three storey dwelling.

Case Officer: Lana Meddings

Customer Details

Name: Mr Matthew Corrigan

Address: 7 Carline Road Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: Mr K Manning

Planning Manager

City of Lincoln Council

City Hall

Beaumont Fee

Lincoln

LN1 1DF

23 May 2018

Dear Mr Manning

Proposed Development at 129 Yarborough Rd, Lincoln, LN1 1HR

We refer to your letter of 1 May 2018 regarding the proposed development at 129, Yarborough Rd, Lincoln.

We wish to object to the development based on the aesthetics and siting of the scheme in a conservation area of the city.

We feel that this development will compromise the Carline Road conservation area. The aesthetic of this part of the conservation area is a grand Edwardian terrace overlooking the city and the introduction of a modern building of a standard design, with its "back" facing onto the road will be highly detrimental.

The development is not in keeping with other properties in the street and the proposed building is close to the road where other properties are all set back, and it will create the feeling of a enclosed street not the open aspect it has currently.

The proposed development is built to exploit the views over the city at the expense of the views from, and of established Edwardian properties in the conservation area. The Introduction a modern building in this location will also badly impact distant views of the City by, for example, those approaching the city from the west along the A57.

The proposed design is ordinary and lacking any architectural features that are encompassed in the design of other properties in the conservation area, including the creative design of new buildings at "The Heights".

Yours sincerely

Matthew Corrigan Lynn Corrigan

7 Carline Road, Lincoln, LN1 1HL

Your Ref: 2018/0547/FUL

ELECTRONICALLY 23 May 2018

Mr K Manning
Planning Manager
City of Lincoln Council
City Hall
Beaumont Fee
Lincoln
LN1 1DF

23 May 2018

Dear Mr Manning

Proposed Development at 129 Yarborough Rd, Lincoln, LN1 1HR

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The proposed design is ordinary and lacking any architectural features that are encompassed in the design of other properties in the conservation area, including the creative design of new buildings at "The Heights".

Yours sincerely



Matthew Corrigan



Lynn Corrigan

7 Carline Road, Lincoln, LN1 1HL

Belmont
11 Carline Road
Lincoln
LN1 1HL



Development Team
City of Lincoln Council
City Hall
Lincoln LN1 1DF

21 May 2018

Dear Sir/Madam

Your ref 2018/0547/FUL 129 Yarborough Road, Lincoln LN1 1HR

We object to planning permission for the above.

We are concerned about the potential impacts of the construction process on our residential amenity and are particularly concerned about the impact of any piling operations, arising from noise and vibration, and the potential damaging impact on the highway Carline Road and other residential properties on Carline Road and Yarborough Terrace.

We would like the reassurance that should the development be approved, the contractor will be required to comply with the relevant British Standards with regard to piling. We would also like confirmation that pile driving will be by jacking and that planning conditions are applied which control the pile driving and construction working hours.

We have lived at 11 Carline Road since 1968 and have been aware and seen all the problems associated with landslide activity, including the recent landslip at Alexandra Terrace, above the development on Yarborough Road.

Over the years myself and other residents have had to pay out for repair work for cracks in walls, rebuilding of bay windows, drainage complications and much expensive remedial work has had to be carried out.

The approval of this planning application could further aggravate our problems.

Yours faithfully


Howard Eve (Mr)


Mavis Eve (Mrs)

Comments from the applicant

Planning Application Reference. No: 2018/0547/FUL

Following the end of the consultation period we would like to take the opportunity to respond to the objections raised.

We believe Carline Road to be a desirable road that deserves quality housing. We are aware Carline Road is a conservation area and have worked in consultation with the Lincoln City Planning Department to come up with a suitable design.

Running along the front of our proposed plot is a brick wall, this continues along all of our neighbouring properties. We believe this to be an important feature of the area and for this reason have resisted altering it.

Our proposed plot has approximately 30.5m (100 feet) of frontage onto Carline Road. We are proposing to build a house with a total width of 12.5 m. We are not a commercial company looking to exploit the site to its full potential, but wish to build a single detached quality home for ourselves, complemented by ample outside amenity space.

Due to the slope of the plot, our proposed property will be built 1 meter behind the front wall. All housing built on the western side of Carline Road, which is the majority of housing in the road, has the same slope issue and is typically either built abutting the pavement or within a few meters of it. Thus our close proximity to the pavement will not be an unusual look (as has been suggested).

The proposed site has a slope in two directions. To reduce the impact of the building and to work with the slope, the ground floor will be partial basement. This, combined with the 1.8m front wall, will result in the entire ground floor and some of the first floor being invisible when viewed from the road. Due to this limited visual height and the large space either side of our proposed property, we do not believe this will result in an enclosed feel for the road (as has been suggested).

Our plot is surrounded by red brick houses that predominantly have slate roofs. We are proposing using the same materials complemented by traditional eave and verge corbelled brick detailing to add character to the property.

Opposite our proposed build on the eastern side of Carline Road is an Edwardian terrace. The western side of Carline Road is a mixed vintage of houses with the nearest neighbours on our side of the lower end of Carline Road (no.22 & 24) also being non-Edwardian.

It has been suggested the house has its back to the road. We have worked in consultation with the Lincoln City Planning Department to design the street scene. The challenging slopes of the site and the desire not to alter the front wall, which is a feature of the road, means the back of the property is always going to appear taller and grander than the street view of the house. Additionally the drawings show the back door to be of the same design as the front door; this may not help with this perception.

We are not looking to lose any trees. We are requesting to remove one pear tree and replace it with another.

Concerns have been raised about the potential vibration impact of any piling work. It is anticipated that a targeted and comprehensive intrusive site investigation will be carried out by a suitably qualified and experienced Geotechnical Engineer. They should then be able to provide a practical methodology for construction of the substructure and foundations while providing an acceptable factor of safety against instability. The use of drilled rather than driven piling is most likely, assuming piling is necessary. It should be noted that piling has previously been carried out on the site when the retaining wall and garage were built in 2008.

We have positioned the proposed new build as far as reasonably possible from our northern boundary wall to minimise any impact of shadowing our neighbours' gardens; we are approximately 9m away from the boundary with 131 Yarborough Road. We note that Mr Alexander at 135 Yarborough Road has complained that we will be looking directly into his garden and blocking his morning sun. Given the distance between our properties we do not believe this to be the case.

The house has not been designed to exploit views over the city which are to the south (as has been suggested). The proposed property is to comprise a ground floor and first floor with a master bedroom in the roof space. As can be seen from Drawing 195-A-5c only the bedroom in the roof space is likely to see over the properties on Yarborough Road. From the first floor there will be partial distant westerly views between the houses on Yarborough Road.

There will be no additional parking load on Carline Road as a result of this proposed development. Currently parking for 129 Yarbrough Road is a driveway and single garage accessed off of Carline Road. A new driveway of approximately 100 m² is being added to the front of 129 Yarbrough Road making the existing driveway and garage available for the new property. The proposed property will see the single garage replaced with a double garage and 90 % of the driveway retained for access and parking. 129 Yarborough Rd is a 5/6 bedroom property, the new property is 4 bedroom. In addition to the current parking on Carline road, the site also has a hard-standing that has been used for parking. This is accessed via another set of gates close to the proposed front door and already has a dropped kerb in place. We are planning to use these gates as a pedestrian access to the front door, however this hard-standing could be brought back into use in the future if further onsite parking is needed. We only have one car and have no requirement for this.

Application Number:	2018/0559/FUL
Site Address:	35 Newark Road, Lincoln, Lincolnshire
Target Date:	23rd June 2018
Agent Name:	London Residential Architects Limited
Applicant Name:	Mr Modestas Kurpeikis
Proposal:	Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette (part retrospective).

Background - Site Location and Description

The application proposes the conversion of an existing dwellinghouse into a ground floor flat and first/second floor maisonette. The application property is 35 Newark Road, Lincoln a three storey mid-terrace property.

The property has been occupied as two flats for at least 7 years without the benefit of planning approval. This application seeks to regularise this use with changes to the existing floor plans.

Site History

No relevant site history.

Case Officer Site Visit

Undertaken on 7th June 2018.

Policies Referred to

- Policy LP1 A Presumption in Favour of Sustainable Development
- Policy LP10 Meeting Accommodation Needs
- Policy LP26 Design and Amenity
- Policy LP37 Sub-division and multi-occupation of dwellings within Lincoln 86
- National Planning Policy Framework

Issues

To assess the proposal with regard to:

- National and local planning policy
- Principle of the use
- Effect on residential amenity
- Effect on highway safety and parking

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Highways & Planning	Comments Received
Environmental Health	Comments Received
Lincoln Civic Trust	Comments Received
Lincolnshire Police	Comments Received

Public Consultation Responses

Name	Address
Mr Robert Dickinson	8 St Catherines Grove Lincoln Lincolnshire LN5 8NA
Mr David Harding	70 St Catherines Grove Lincoln LN5 8NA
Mrs Sara McNair	170 St Catherines Grove Lincoln LN5 8ND
Ms Michele Servaud	8 St Catherines Grove Lincoln Lincolnshire LN5 8NA
Mrs Shona Smith	5 St Catherines Grove Lincoln Lincolnshire LN5 8NA

Consideration

Local and National Planning Policy

Policy LP1 'A Presumption in Favour of Sustainable Development' of the Central Lincolnshire Local Plan (2017) states 'Planning applications that accord with the policies in this Local Plan will be approved without delay, unless material considerations indicate otherwise.'

Policy LP10 'Meeting accommodation needs' of the Central Lincolnshire Local Plan (2017) is relevant to the application site. The policy advises that 'new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.'

Policy LP26 'Design and Amenity' is permissive of alterations to existing buildings provided the siting, height, scale, massing and form relate well to the site and surroundings, and duly reflect or improve on the original architectural style of the local surroundings; and use appropriate high quality materials, which reinforce or enhance local distinctiveness, with consideration given to texture, colour, pattern and durability. In relation to both construction and life of the development, the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy must not be unduly harmed by or as a result of development.

Policy LP37 relates to the sub-division and multi-occupation of dwellings within Lincoln. It clarifies that the conversion or change of use of existing dwellings to self-contained flats will be supported where it can be demonstrated there is an established lack of demand for the single family use of the property concerned (b); and the development will not lead to or increase an existing over-concentration of such uses in the area (c). In addition, point (d) also refers to the provision of on-site parking and cycle storage unless it can be demonstrated that the site is sustainably located on a regular bus route or within walking distance of the City Centre.

Principle of the Use

In this instance, the proposals are for the dwelling to be split into two flats and within the provision of rooms proposed, officers are satisfied that there would still be potential for the building to be used by at least one family. As such, officers would not strictly apply Point (b) of the policy as this is inherently directed at greater subdivision. There is also no evidence that the occupation of the building as two flats would lead to an overconcentration of such uses. The application building is located within an area of predominantly residential development, the current and proposed uses fall within a residential use class and as such officers are satisfied that the use would be acceptable in this location.

A number of objections have been received from local residents. These issues principally relate to:

- Increased pressure on on-street parking
- Increased number of occupants
- Too many HMOs in the area
- Noise and other disturbance

Residential Amenity

Policy LP37 also outlines how the sub-division of dwellings within Lincoln should be dealt with in terms of amenity. Points (a) and (d) suggest that the dwelling should be capable of conversion without causing harm to the amenities of future occupants, neighbours and the wider area; and that adequate provision should be made for external communal areas and bin storage / collection. Policy LP26 also refers to amenity and is supported by the Framework.

The Council's records suggest the use has been operating since at least 2011 without complaint regarding noise and disturbance, parking or any other issues. The existing property has 4 bedrooms and there is no restriction on the number of people that can live within the property. The proposal would create 3 bedrooms and given the reduction in bedrooms it is not considered that the proposed use would constitute an over-intensive of use of the property.

The Council's Environmental Officer has raised no objections to the proposals in terms of contaminated land, air quality, noise or other environmental impact. There is no requirement therefore for any additional noise assessment or sound attenuation in relation to the proposal.

The occupation of the property by two independent households would not therefore result in a significant change in site circumstances to warrant the application being resisted upon the grounds of the noise and disturbance created by occupants and a residential use would be in keeping with the surrounding area. Similarly, the property would be served by sufficient amenities, both internally and externally for the proposed occupation by two households. Moreover, there is space for bin storage and for occupiers to enjoy quiet amenity space.

Visual Amenity

No external alterations are proposed as part of the application. Accordingly there would be no impact on visual amenity and therefore the character of the area would be preserved, in accordance with Policy LP26 of the Central Lincolnshire Local Plan.

Highways and Parking

The Lincolnshire County Council as Highway Authority has assessed the application and has raised no objections to the proposal. The potential number of cars is likely to be reduced in fairly central locations with pedestrian and frequent bus facilities in the area. There are double yellow lines along Newark Road therefore parking outside the property would not be available. Based on this advice it is considered that the proposal would not be detrimental to highway safety or traffic capacity.

A number of residents have raised the issue of on street parking and the increased demand the proposal would have on surrounding streets. However there is no evidence to suggest that the 3 bedrooms proposed would have any greater impact than the current 4 bedrooms within the property. Therefore it would be considered that the proposal is acceptable.

Conclusion

The proposal is in accordance with local plan policies LP37 and LP26 of the Central Lincolnshire Local Plan (2017) and the National Planning Policy Framework and does not cause harm to the amenity of neighbours or to the wider area.

Application Determined within Target Date

Yes.

Recommendation

That the application is granted conditionally.

Conditions

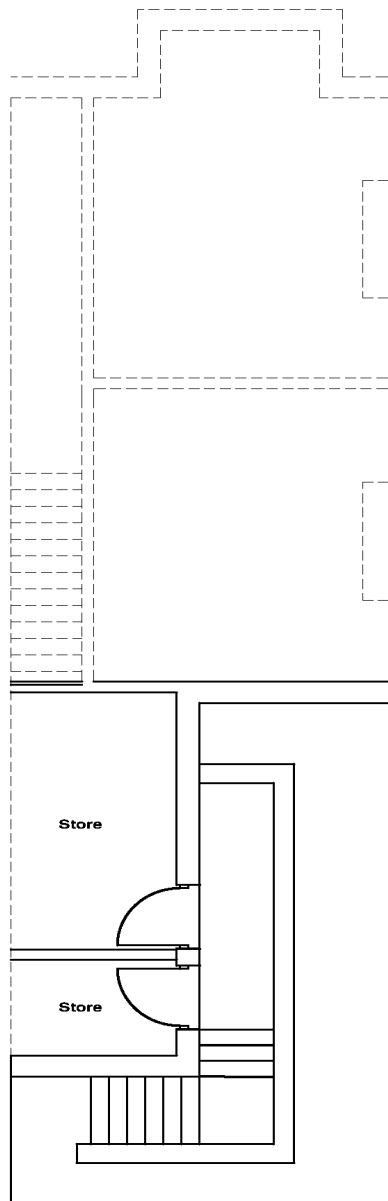
- Development to be carried out within 3 years
- Development to be carried out in accordance with the plans

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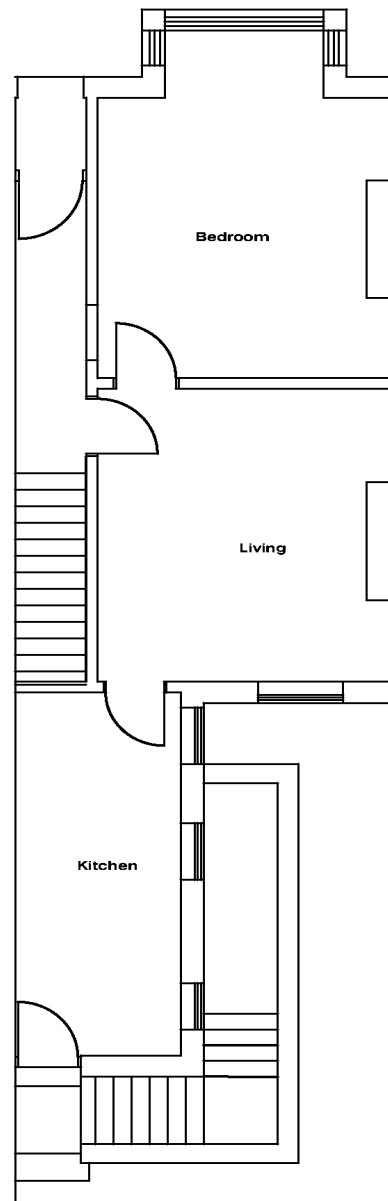
Site Plan



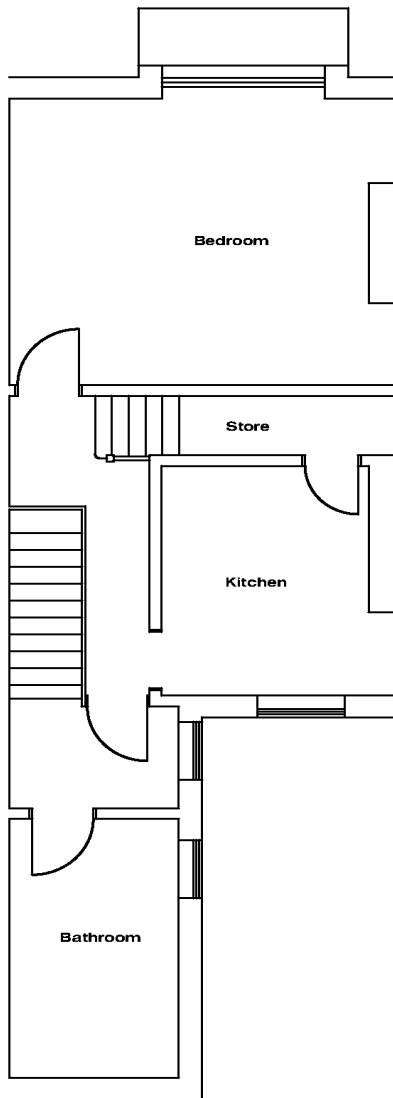
Existing Floor Plans



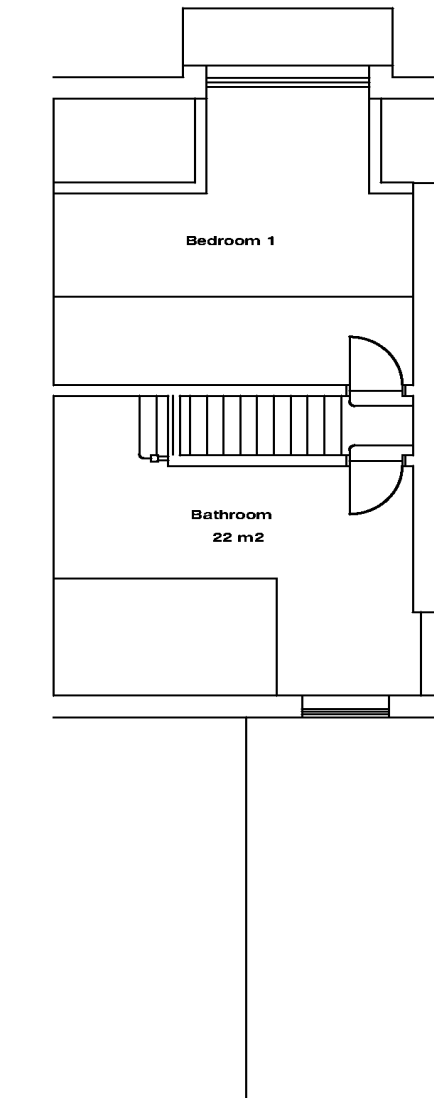
EXISTING BASEMENT PLAN



EXISTING GROUND FLOOR PLAN

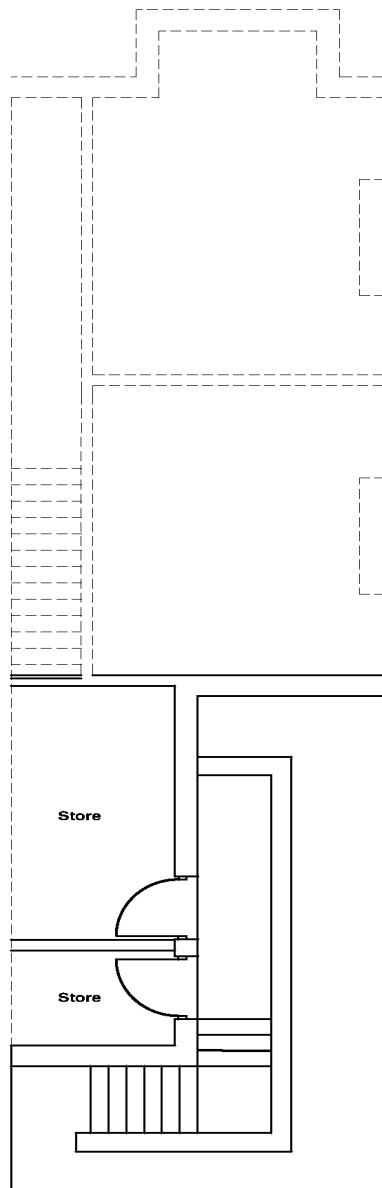


EXISTING FIRST FLOOR PLAN

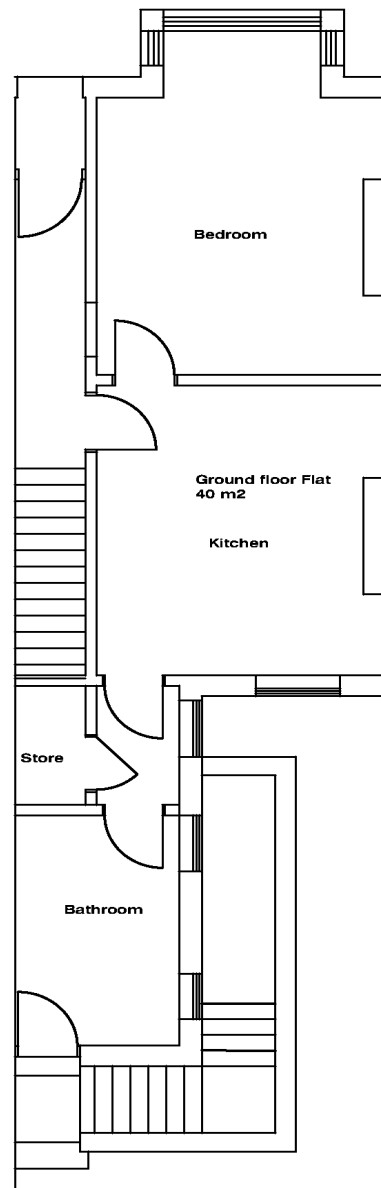


EXISTING SECOND FLOOR PLAN

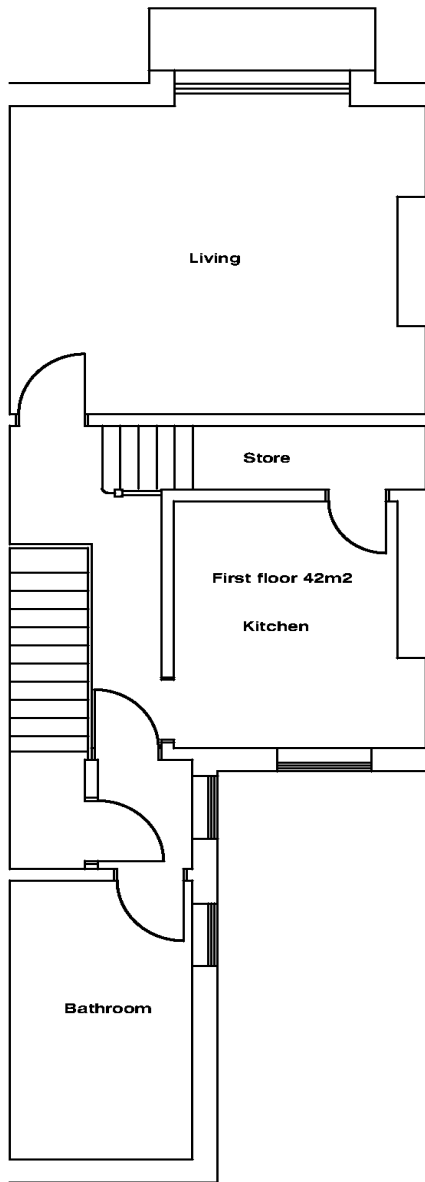
Proposed Floor Plans



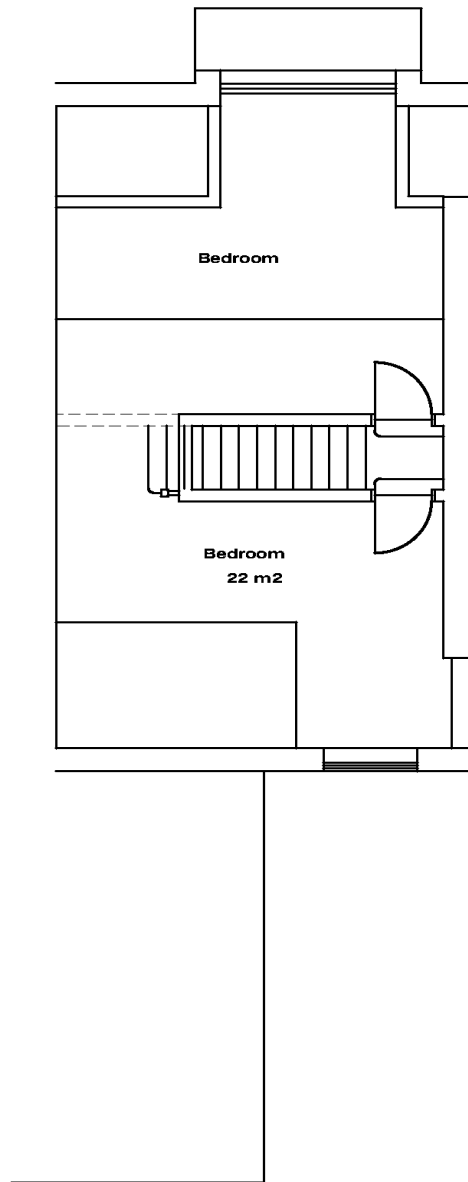
PROPOSED BASEMENT PLAN



PROPOSED GROUND FLOOR PLAN

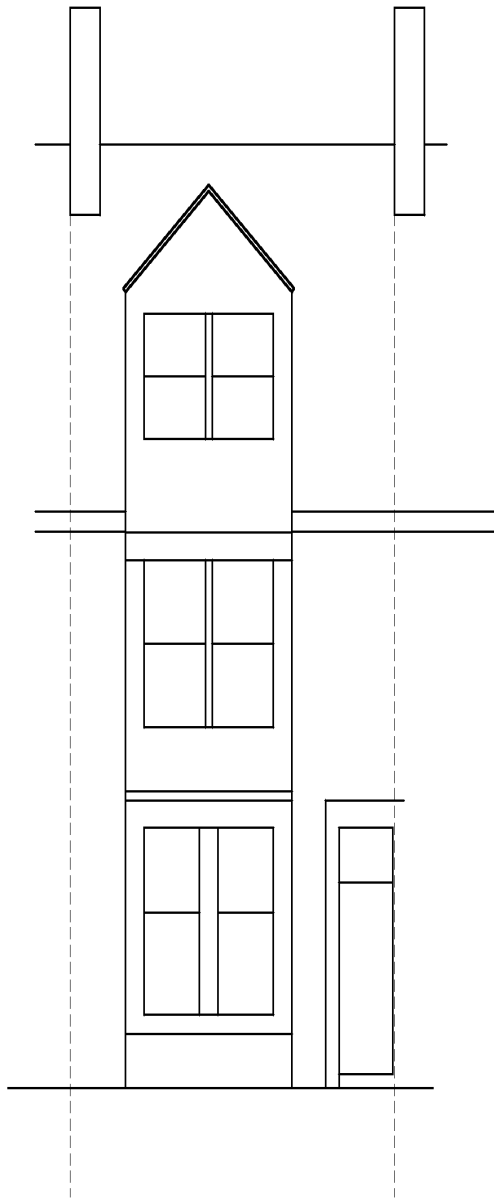


PROPOSED FIRST FLOOR PLAN

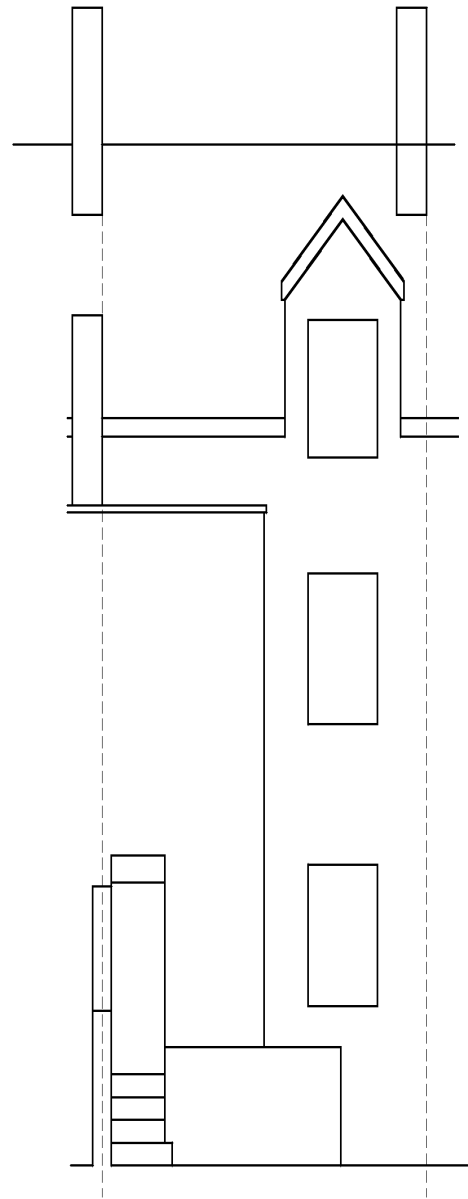


PROPOSED SECOND FLOOR PLAN

Existing and proposed front and rear elevations



PROPOSED/EXISTING FRONT ELEVATION



PROPOSED/EXISTING REAR ELEVATION

Photograph of front elevation



Neighbour Comments

Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Customer Details

Name: Mr David Harding

Address: 70 St Catherines Grove Lincoln

Comment Details

Commenter Type: Member of the Public

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to the proposed change of use of this property from a house to a flat and maisonette. Please take note of my objection even if it appears to have been submitted late. This is because the relevant part of your website was not working for several days last week. The grounds of my objection are as follows: 1. St Catherines Grove and the surrounding area have seen a huge increase in properties converted from family homes to two or more dwellings in recent years, to the detriment of the community. This problem is recognised by Article 4 of the National Planning Policy Framework 2012, which I believe applies here. 2. One of the most pressing problems is the lack of sufficient on-street parking as the number of households increases. This leads to double or inappropriate parking, which has on occasion prevented large vehicles getting through. In an emergency, this could have very serious consequences. On a day to day basis, the lack of parking causes great inconvenience to local residents, and the current application (if approved) would only add to the problem. 3. The increase in multi-occupancy homes on the scale seen recently has added to noise and other disturbance, and should not be allowed to continue, as it adversely affects the close community feel of the area. 4. This increase has to some extent gone unchallenged because the planning department has failed on more than one occasion to display the relevant notices, which would have allowed residents to object. In the future, every application must be clearly displayed, please. I would be grateful if you would consider my views when considering this application. Yours faithfully, David Harding

Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Customer Details

Name: Mrs shona smith

Address: 5 st catherines grove lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: i object to the planning permission for number 35 newark road for the following reasons

1. parking is a huge issue and considering newark road has restricted parking where are the residents going to park? they will attempt to use St Catherines Grove as their passage backs onto our street.

2. The passage alone is a problem with parties from newark road spilling out onto the passage and into our road with smoking and drinking waking the street up, something the police have been called to before, this is something that will only increase.

3. The percentage of multiple occupancy housing in the street and surrounding area is above what should be legally allowed, St Catherines grove alone is number 1,3,7,9,11,17 and the list goes on.

4. The occupants are obviously not vedette as already someone who admitted moving into a house on newark road strolled from the passageway asking for money two days ago, they have no respect for their neighbours.

5. The passage itself is not maintained, currently housing a shopping trolley food bags and i hate to think what else.. its not a skip, id be surprised if we dont have rats!

I get people want to make money from student/multiple housing but what about allowing the community to keep a friendly warm and inviting family street, just for a change!

Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Customer Details

Name: Mrs Sara McNair

Address: 170 St Catherines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to the new development at 35 Newark Road on these grounds:

1. Parking is already problematic, my husband is only home fri night to Monday morning & we struggle to park on our side of the grove & many times there has been no parking on either side.
2. I believe that allowing these properties to be split into flats will also significantly affect the value of the surrounding properties.

Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Customer Details

Name: Ms Michele Servaud

Address: 8 St Cathrines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: To whom it may concern,

To whom it may concern,

I object to a change of use from a house into two flats at No 35 Newark Road, Lincoln.

I would like to underline that because we already have a significant number of HMOs surrounding us on St Catherines Grove: No 1, No 2, No 3, No 4, No 7, No 13, No 15, No 16, No 17 and No 19; this new development will create more strain on the parking resources on our street. This cluster of HMOs which has been granted by the planning department over the years is already far in excess of what Article 4 recommends; adding two flats at the end of St Catherines Grove is only going to increase the parking problems we already experience on a daily basis. As there is no possibility of parking on Newark road of which a great proportion of the properties are HMOs, the residents on this road are already using our street to park their car. This new development will only be creating even more problems in addition to what we are currently experiencing.

We deserve to have a certain quality of life, and being able to park on our street is an essential element of this. In recent policies like the National Planning Policy Framework of the 27 of March 2012, communities are at the heart of the central government preoccupations. For example in Article 4, which mentions that 'for communities in those areas where HMO growth has been marked, there are increasing concerns about the changing nature of their communities'. Since we moved onto our street, we have seen eight houses transformed from family dwellings into HMOs, and this, without clear display of notices placed on our street which has been acknowledged by the planning department as an error on their part. This has led to increased problems with parking and subsequent difficulties for large vehicles wishing to pass. In this context, for example, due to people parking on double yellow lines, on several occasions the passage of the council waste collection vehicle has been prevented. In the document of reference, which focus on HMOs but is also valid in this case, it is mentioned that: 'opportunities for HMO developments to share off-street

parking should be considered, particularly in those areas where on-street parking is limited' which is the case in our street.

Regards,

Michèle Servaud

Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Customer Details

Name: Mr Robert Dickinson

Address: 8 St Catherines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to the change of use and conversion of the dwelling house at 35 Newark Road into a a ground floor flat and two further first and second floor maisonettes. The basis of my objection is due to the pressure which will be placed upon on-street parking as a result of such a development and in particular a further concentration of such parking in neighbouring St Catherines Grove. It should be noted that currently there are major health and safety concerns related to the high density of parking on St Catherines Grove with vehicles from neighbouring properties both on St Catherines Grove and close-by parking on double yellow lines thus creating hazards for vehicles wishing to exit St Catherines into Newark Road. There is currently no assurance that parking will be provided in the proposed conversion. The authorities have been made aware of such concerns in the recent past and it is somewhat surprising that no concerns have been voiced by these in respect of the current application.

Consultee Comments for Planning Application 2018/0559/FUL

Application Summary

Application Number: 2018/0559/FUL

Address: 35 Newark Road Lincoln Lincolnshire

Proposal: Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette.

Case Officer: Gemma Till

Consultee Details

Name: Ms Catherine Waby

Address: St Mary's Guildhall, 385 High Street, Lincoln LN5 7SF

Email: lincolncivictrust@btconnect.com

On Behalf Of: Lincoln Civic Trust

Comments

No Objection

Consultee Comments

LINCOLNSHIRE POLICE



POLICE HEADQUARTERS

PO Box 999

LINCOLN LN5 7PH

Fax: (01522) 558128

DDI: (01522) 558292

email

john.manuel@lincs.pnn.police.uk

Your Ref: App. 2018/0559/FUL

8th May 2018

Our Ref: PG//

Development & Environmental Services

City Hall, Beaumont Fee

Lincoln, LN1 1DF

Re: 35 Newark Road, Lincoln, Lincolnshire, (Conversion)

Thank you for your correspondence and opportunity to comment on the proposed development.

Lincolnshire Police has no formal objection to this application.

Please do not hesitate to contact me should you need further information or clarification.

Crime prevention advice is given free without the intention of creating a contract. Neither the Home Office nor the Police Service takes any legal responsibility for the advice given. However, if the advice is implemented it will reduce the opportunity for crimes to be committed.

Yours sincerely,

John Manuel MA BA (Hons) PGCE Dip Bus.

Force Designing Out Crime Officer (DOCO)

Environment & Economy

Lancaster House
36 Orchard Street
Lincoln LN1 1XX
Tel: (01522) 782070
E-Mail: Highwaysudssupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2018/0559/FUL

With reference to this application dated 27 April 2018 relating to the following proposed development:

Address or location

35 Newark Road, Lincoln, Lincolnshire

Date application referred by the LPA
1 May 2018

Type of application: Outline/Full/RM/
FUL

Description of development

Conversion of dwellinghouse into a ground floor flat and first/second floor maisonette

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Does not wish to restrict the grant of permission.

CONDITIONS (INCLUDING REASONS) /REASONS FOR REFUSAL

NO OBS

Having given due regard to the appropriate local and national planning policy guidance (in particular the National Planning Policy Framework), Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) has concluded that the proposed development is acceptable. Accordingly, Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) does not wish to object to this planning application.

Case Officer:
Emily Stevenson
for Warren Peppard
Flood Risk & Development Manager

Date: 15.05.18

Application Number:	2018/0589/FUL
Site Address:	61 St Catherines, Lincoln, Lincolnshire
Target Date:	29th June 2018
Agent Name:	AM2 Architects
Applicant Name:	Mr Browne
Proposal:	Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Background - Site Location and Description

61 St Catherines is a semi-detached property located on the western side of St Catherines, a primarily residential street within the Conservation Area (No. 4) of the same name. There is a gravelled area to the rear for parking.

This application for planning permission proposes to change the use of the property from 9 bedroom house in multiple occupation to 5 self-contained flats. Planning permission was granted in 2015 (2015/0735/F) to change the use of the property to a 9 bedroom HMO.

The proposal is for 4no. one bedroom flats and 1no. two bedroom flats. The proposal would require minor internal and external alterations.

Site History

Prior to its use as a HMO the property was a care home for the elderly. It had permission for a care home from 1988 until its change to a HMO in 2015.

Case Officer Site Visit

Undertaken on 6th June 2018.

Policies Referred to

- National Planning Policy Framework
- Central Lincolnshire Local Plan
 - Policy LP25 The Historic Environment
 - Policy LP26 Design and Amenity

Issues

- Residential amenity
- Visual amenity
- Highways

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Highways & Planning	Comments Received

Public Consultation Responses

Name	Address
Mr David Harding	70 St Catherines Grove Lincoln LN5 8NA
Mrs Shona Smith	5 St Catherines Grove Lincoln LN6 0JT
Mrs Sara McNair	170 St Catherines Grove Lincoln LN5 8ND
Ms Michele Servaud	8 St Catherines Grove Lincoln LN5 8NA
Mr Robert Dickinson	8 St Catherines Grove Lincoln LN5 8NA

A number of objections have been received from local residents. These issues principally relate to:

- Increased pressure on on-street parking
- Increased number of occupants
- Too many HMOs in the area
- Noise and other disturbance

Consideration

Policy Context

The application building is located within an area of predominantly residential development, the current and proposed uses fall within a residential use class and as such it is considered that the use would be generally consistent with policies in the Central Lincolnshire Local Plan.

Residential Amenity

The property currently has permission to be used as a 9 bedroom house in multiple occupation. Therefore any issues with that use would have been addressed as part of the previous scheme and whether there are now too many HMOs in the area should not be addressed as part of this application. However it is appropriate to assess whether changing the use of the property to self-contained units would have a different impact on neighbouring residents and on future occupants of the property.

Policy LP26 of the Local Plan states that "The amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy must not be unduly harmed by or as a result of development."

The proposed internal arrangement of the property is such that corridors would remain on the adjoining wall with the neighbours where they currently exist which helps prevent the transfer of noise. The City Councils Pollution Control Officer has raised no concerns with regards noise or other environmental impacts.

The existing HMO has 9 bedrooms and there is no restriction on the number of people that can live within the property. The subdivision of the property to flats would create 6 bedrooms, again with no restriction on the number of people that can live in a flat. However given that the proposal is a reduction in bedrooms it is not considered that the proposed use would be an over concentration of use of the property.

The property is served by sufficient amenities for the occupation of the property by 5 households. There is space for bin storage to the rear of the property and a shared outdoor amenity space.

Visual Amenity

The proposed change of use would require minor changes to the external appearance of the building. A window would be added to the north elevation and an existing doorway blocked up and replaced with a window. These changes would have no material impact on the appearance of the building and subsequently would have no impact on the Conservation Area in accordance with local plan policy LP25. The property would retain living accommodation at ground floor to the front of the property which ensures an active fronting in keeping with the residential area.

Highways

The proposal would include for an additional parking space bringing the total number of off street parking spaces to 5. Whilst there are no formal parking standards employed by the City or County council it is considered that parking on a 1:1 basis within the City Centre is preferable. A number of residents have raised the issue of on street parking and the increased demand the proposal would have on surrounding streets. However there is no evidence to suggest that the 6 bedspaces proposed would have any greater impact than the current 9 bedspaces within the property.

The Highways Authority have raised no objections to the proposed scheme.

Financial Implications

None.

Legal Implications

None.

Equality Implications

None.

Conclusion

Having taken into account the current permitted use of the property it is not considered that the proposed subdivision would cause any additional harm to neighbouring residents or surrounding area. The proposal would therefore be in accordance with local plan policies LP25 and LP26 which seeks to protect the impact on residential and visual amenity.

Application Determined within Target Date

Yes.

Recommendation

That the application is Granted Conditionally

Conditions

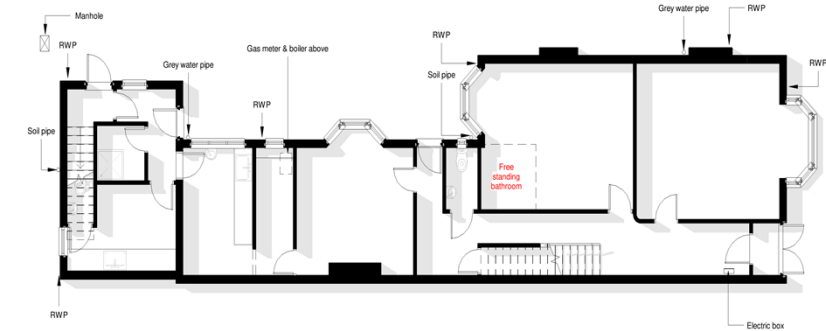
- Development to be carried out within 3 years
- Development to be carried out in accordance with the plans

Site Location Plan

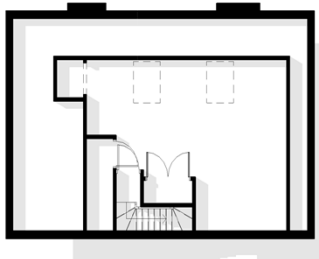


Existing Layout

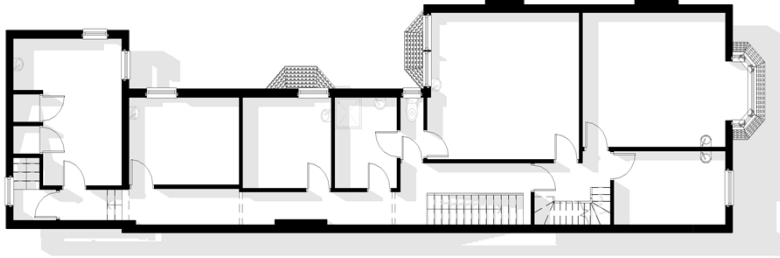
Ground Floor
1 : 100



Second Floor
1 : 100

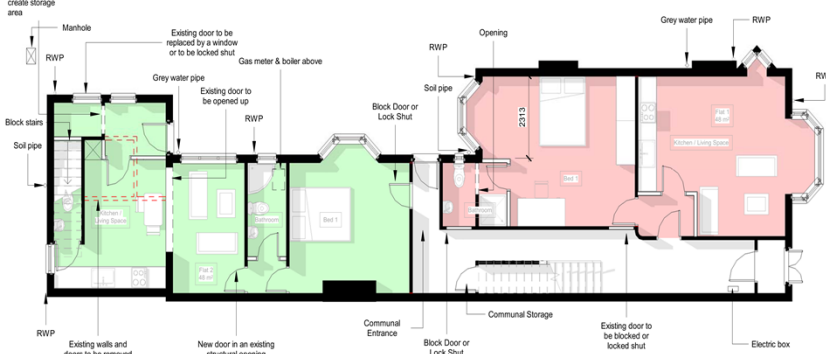


First Floor
1 : 100

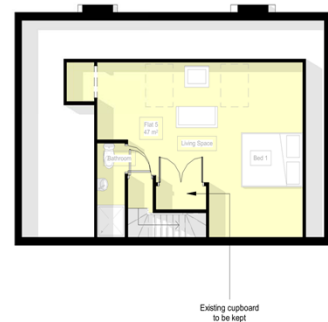


Proposed Layout

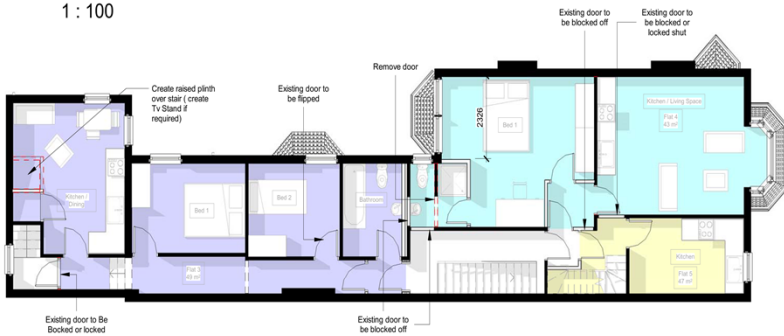
Ground Floor
1 : 100



Second Floor
1 : 100



First Floor
1 : 100



Ground Floor

- Flat 1: 1 Bed
- Flat 2: 1 Bed

Second Floor

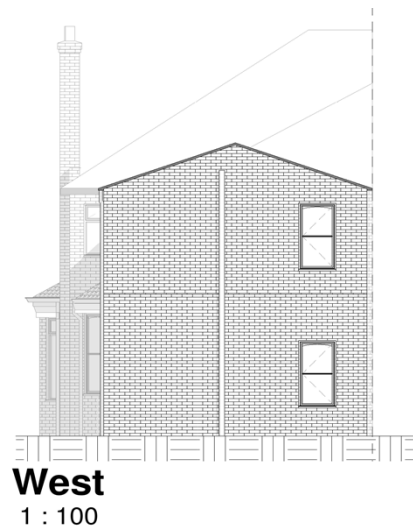
- Flat 5: 1 Bed

First Floor

- Flat 3: 2 Beds
- Flat 4: 1 Bed
- Flat 5: 1 Bed

- Existing walls
- New walls
- Existing walls to be demolished

Proposed elevations



Site Photos



Front elevation



Adjoining property



Rear elevation and existing parking area



Side elevation and existing vehicular access to the rear of the site.

Consultee Comments

Environment & Economy

Lancaster House
36 Orchard Street
Lincoln LN1 1XX
Tel: (01522) 782070
E-Mail: Highwayssudssupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2018/0589/FUL

With reference to this application dated 3 May 2018 relating to the following proposed development:

Address or location

61 St Catherines, Lincoln, Lincolnshire, LN5 8LR

Date application referred by the LPA
16 May 2018

Type of application: Outline/Full/RM/
FUL

Description of development

Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3)

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Does not wish to restrict the grant of permission.

CONDITIONS (INCLUDING REASONS)

HI08

Please contact Lincolnshire County Council Streetworks & Permitting team on 01522 782070 to discuss any proposed statutory utility connections and any other works which will be required in the public highway in association with this application. This will enable Lincolnshire County Council to assist you in the coordination and timings of such works.

NO OBS

Having given due regard to the appropriate local and national planning policy guidance (in particular the National Planning Policy Framework), Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) has concluded that the proposed development is acceptable. Accordingly, Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) does not wish to object to this planning application.

Case Officer:

Date: 4 June 2018

Becky Melhuish

for Warren Peppard
Flood Risk & Development Manager

Neighbour Comments

Comments for Planning Application 2018/0589/FUL

Application Summary

Application Number: 2018/0589/FUL

Address: 61 St Catherines Lincoln Lincolnshire LN5 8LR

Proposal: Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Case Officer: Lana Meddings

Customer Details

Name: Mr Robert Dickinson

Address: 8 St Catherines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to a change of use from a 9 bed house into five flats at No 61 St Catherines, Lincoln.

The change of use will further increase the pressure placed upon resident parking in the area and in particular in St Catherines Grove. The primary concern is that there is no provision being made by the developer to accommodate increased off-street parking for the flat tenants. The planning department has already granted permission for a cluster of HMOs in the local area which on examination is already in excess of what Article 4 recommends. I am concerned that planning guidelines are not being followed in this respect. From the same document it is noted that 'opportunities for HMO developments to share off-street parking should be considered, particularly in those areas where on-street parking is limited'. It is clearly evidenced that this is the case in respect of the local area. It is of further concern that the creation of five flats will result in an increased number of occupants at number 61 from the current nine bedrooms and the conversion of the property from a nine bedroom house into five flats could be seen as a way of bypassing HMO regulations by the developer. I am concerned that the development will not only impact parking in a negative manner (and infringe planning guidelines as noted) but also have additional environmental impacts such as pressure on sewage services, waste disposal, noise pollution and possible anti-social behaviour. In the latter respect there have been a recorded number of such behaviour by the local Police associated with properties on both Newark road and St Catherines Grove which is ongoing and current.

Comments for Planning Application 2018/0589/FUL

Application Summary

Application Number: 2018/0589/FUL

Address: 61 St Catherines Lincoln Lincolnshire LN5 8LR

Proposal: Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Case Officer: Lana Meddings

Customer Details

Name: Ms Michele Servaud

Address: 8 St Catherines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: To whom it may concern,

I object to a change of use from a 9 bed house into five flats at No 61 St Catherines, Lincoln.

I would like to underline that because we already have a significant number of HMOs surrounding us on St Catherines Grove: No 1, No 2, No 3, No 4, No 7, No 13, No 15, No 16, No 17 and No 19; this new development will create more strain on the parking resources on our street. This cluster of HMOs which has been granted by the planning department over the years is already far in excess of what Article 4 recommends; adding five flats at the end of St Catherines Grove is only going to increase the parking problems we already experience on a daily basis. As there is no possibility of parking on Newark road of which a great proportion of the properties are HMOs, the residents on this road are already using our street to park their car. The only way we could accept such a development would be if the developer could guaranty that enough parking place is left for cars to park on the land available at the back of the property. Currently, there are only three parking places provided at the back of No 61 St Catherines. This is not a sufficient amount of parking space for the number of flats planned and the only option these residents will have to park their car will be on St Catherines Grove, creating even more problems in addition to what is currently happening.

We deserve to have a quality of life, and being able to park on our street is an essential element of this. In recent policies like the National Planning Policy Framework of the 27 of March 2012, communities are at the heart of the central government preoccupations. For example in Article 4, which mentions that 'for communities in those areas where HMO growth has been marked, there are increasing concerns about the changing nature of their communities'. Since we moved onto our street, we have seen eight houses transformed from family dwellings into HMOs, and this, without clear display of notices placed on our street which has been acknowledged by the planning department as an error on their part. This has led to increased problems with parking and subsequent difficulties for large vehicles wishing to pass. In this context, for example, due to

people parking on double yellow lines, on several occasions the passage of the council waste collection vehicle has been prevented. In the document of reference, which focus on HMOs but is also valid in this case, it is mentioned that: 'opportunities for HMO developments to share off-street parking should be considered, particularly in those areas where on-street parking is limited' which is the case in our street.

Regards,

Michèle Servaud

Comments for Planning Application 2018/0589/FUL

Application Summary

Application Number: 2018/0589/FUL

Address: 61 St Catherines Lincoln Lincolnshire LN5 8LR

Proposal: Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Case Officer: Lana Meddings

Customer Details

Name: Mr David Harding

Address: 70 St Catherines Grove Lincoln

Comment Details

Commenter Type: Member of the Public

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to the proposed change of use of this property from a 9-bedroom HMO to 5 flats. Please take note of my objection even if it appears to have been submitted late. This is because the relevant part of your website was not working for several days last week.

The grounds of my objection are as follows:

1. St Catherines Grove and the surrounding area have seen a huge increase in multi-occupancy properties in recent years, to the detriment of the community. This problem is recognised by Article 4 of the National Planning Policy Framework 2012, which I believe applies here.
2. One of the most pressing problems is the lack of sufficient on-street parking as the number of households increases. This leads to double or inappropriate parking, which has on occasion prevented large vehicles getting through. In an emergency, this could have very serious consequences. On a day to day basis, the lack of parking causes great inconvenience to local residents, and the current application (if approved) would only add to the problem, as 61 St Catherine's only has 3 off-street parking spaces - clearly not enough for 5 flats.
3. The increase in multi-occupancy homes on the scale seen recently has added to noise and other disturbance, and should not be allowed to continue, as it adversely affects the close community feel of the area.
4. This increase has to some extent gone unchallenged because the planning department has failed on more than one occasion to display the relevant notices, which would have allowed residents to object. In the future, every application must be clearly displayed, please.

I would be grateful if you would consider my views when considering this application.

Yours faithfully,

David Harding

Comments for Planning Application 2018/0589/FUL

Application Summary

Application Number: 2018/0589/FUL

Address: 61 St Catherines Lincoln Lincolnshire LN5 8LR

Proposal: Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Case Officer: Lana Meddings

Customer Details

Name: Mrs shona smith

Address: 5 st catherines grove lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I write to object to the above application for the following reasons.

I moved to the area three years ago as the street gave a nice warm family orientated feeling. Little did I know how many multiple occupancy housing I was surrounded by, far more than legally should have been allowed. The house in question already has a busy parking area which can be seen and heard from St Catherines grove. The noise on a weekend can be intolerable. Parking is a huge issue on St Catherines Grove bad parking, no where to park, scrapes and bumps and having to park streets away, coming home at 11pm from a shift at work and walking with your shopping is no joke. The parking at 61 can not take the strain of 9 vehicles never mind their visitors so where are they going to park? and therefore allowing another multiple occupancy will add to the strain we already have. Litter is a problem and the extent of weed smoking and general smoking with cigarette ends being chucked on the floor is ridiculous....i get the housing is advertised no smoking and if you do smoke outside but why should we have to put up with the smell and mess. There are many landlords who don't wish to vet their occupants and we have this situation on our street, noise levels at unreasonable hours, occupants falling out and arguing in the street.....we all need to sleep to be able to function for work. We wish our street and local area to remain a warm family orientated supportive community not for more students and multi housing where they have no respect for others.

Comments for Planning Application 2018/0589/FUL

Application Summary

Application Number: 2018/0589/FUL

Address: 61 St Catherines Lincoln Lincolnshire LN5 8LR

Proposal: Change of use from 9-bedroom HMO (Use Class Sui Generis) to 5 flats (Use Class C3).

Case Officer: Lana Meddings

Customer Details

Name: Mrs Sara McNair

Address: 170 St Catherines Grove Lincoln

Comment Details

Commenter Type: Neighbour

Stance: Customer objects to the Planning Application

Comment Reasons:

Comment: I object to the new development at 61 St Catherine on these grounds:

1. Parking is already problematic, my husband is only home fri night to Monday morning & we struggle to park on our side of the grove & many times there has been no parking on either side.
2. I believe that allowing these properties to be split into flats will also significantly affect the value of the surrounding properties.

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Application Number:	2018/0591/LBC
Site Address:	40 - 42 Michaelgate, Lincoln, Lincolnshire
Target Date:	29th June 2018
Agent Name:	City Of Lincoln Council
Applicant Name:	Mrs Angela Andrews
Proposal:	Works to repair structural damage including taking down and reinstating a section of the north wall, reinstating meters and services, repairing damaged plasterwork and other finishes and redecoration to certain rooms.

Background - Site Location and Description

The application relates to 40-42 Michaelgate, which are conjoined Grade II listed buildings within the City Council's ownership. The two storey, brick built property was constructed in the mid to late 18th Century with a late 18th century shop window to the left. The property is located on the east side of Michaelgate, close to the junction with Steep Hill and Bailgate, and is also within the Cathedral and City Centre Conservation Area.

The listed building consent application is for structural repairs to resolve damage, both internal and external caused by a vehicular impact to the north elevation, to the portion of the property represented as 42 Michaelgate. The application advises that the impact significantly damaged the low level single brick thick north elevation of the property, puncturing through the elevation beneath a large ground floor window adjacent to the party wall with The Harlequin and internally through the ground to first floor staircase. The timber casing of the electricity and gas meters was also damaged along with plasterwork in the ground floor rooms.

The application is being presented to Members of the Planning Committee as the City Council is the applicant.

Site History

Reference:	Description	Status	Decision Date:
98/260/LBC	Internal alterations to kitchen. Removal of dwarf wall in the dining room and installation of new external light. Reroof porch with pantiles (Amendment in accordance with DP memo of 20.7.98)	Granted Conditionally	8th June 1998

Case Officer Site Visit

Undertaken on 7th February 2018.

Policies Referred to

- Policy LP25 The Historic Environment
- National Planning Policy Framework

Issues

- Impact on the building as a designated heritage asset

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Highways & Planning	Comments Received
Principal Conservation Officer	Comments Received
Lincoln Civic Trust	No Response Received
West End Residents Association	No Response Received
Environmental Health	Comments Received

Public Consultation Responses

No responses received

Consideration

Impact on the Building as a Designated Heritage Asset

The City Council's Principal Conservation Officer has visited the property and considered the application, including the 'Schedule of Works & Specification for Repairs' report. She notes that the proposals have been informed by advice from a structural engineer who is on the Conservation Accreditation Register for Engineers and as such is considered to employ sound conservation principles and practices.

The Conservation Officer advises that, with regard to the external works, the engineers report identifies that the vehicular impact has caused the north wall to become structurally unsound necessitating a degree of dismantling and reconstruction. The recording of the brick bonding and other architectural detailing, including analysis of the existing mortar and windows to be refurbished, is welcomed to ensure that the proposed reconstruction is accurate. Attached to no. 42, The Harlequin, also Grade II listed, is anticipated to have also suffered from the impact in terms of its timber frame and plaster infill panels. A sequential approach is proposed whereby the preference is for the repair to the timber frame if possible, or else a replacement as necessary. This approach is supported in terms

of minimising loss of historic fabric and its integral significance. It is also important to note that the methodology of the proposed works will safeguard the structural integrity of The Harlequin.

In terms of the internal works the Conservation Officer advises that localised areas of loss to the internal lime plaster has been caused by the impact and will be reinstated on a like for like basis. Dismantling of the floor boards and staircase to allow for the works to the north wall is unfortunate, however, their prior recording will ensure accurate reinstatement.

It is concluded that the proposed works will address structural issues which currently threaten the integrity of the building. The proposals have been carefully considered to achieve the desired outcome with minimal intervention, ensuring an authentic reinstatement of materials and returning the building to its former appearance using appropriate materials.

Therefore, the Conservation Officer considers that the proposal is in accordance with the duty contained within section 16(2) of the Planning (Listed Buildings and Conservation Areas Act) 1990 which requires 'In considering whether to grant listed building consent for any works the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses'. She also considers that the proposals are in accordance with the guidance contained within paragraph 131 of the National Planning Policy Framework (NPPF) which requires that the Local Planning Authorities in determining applications should take account of the desirability of sustaining and enhancing the significance of heritage assets, putting them to viable uses consistent with their conservation. The proposal would also be in accordance with paragraph 132 which requires that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation.

Officers accordingly would concur with the Conservation Officer's conclusions that the proposals will address structural issues, returning the building to its previous appearance with the use of minimal intervention and appropriate materials. Conditions will require samples and detail of materials, such as a brick sample panel and lime plaster, as well as methodologies for the refurbishment of the windows and timber framing. Subject to the approval of these details it is considered that the proposals will preserve the listed building and would not be prejudicial to its special architectural or historic interest, in accordance with Central Lincolnshire Local Plan (CLLP) Policy LP25 and guidance within the NPPF.

Conclusion

The proposed works will address structural issues caused by a vehicular impact which currently threaten the integrity of the building. The proposals have been carefully considered to achieve the desired outcome with minimal intervention, ensuring an authentic reinstatement of materials and returning the building to its former appearance. The works will therefore preserve the building and would not be prejudicial to its special architectural or historic interest, in accordance with CLLP Policy LP25 and guidance within the NPPF.

Application Determined within Target Date

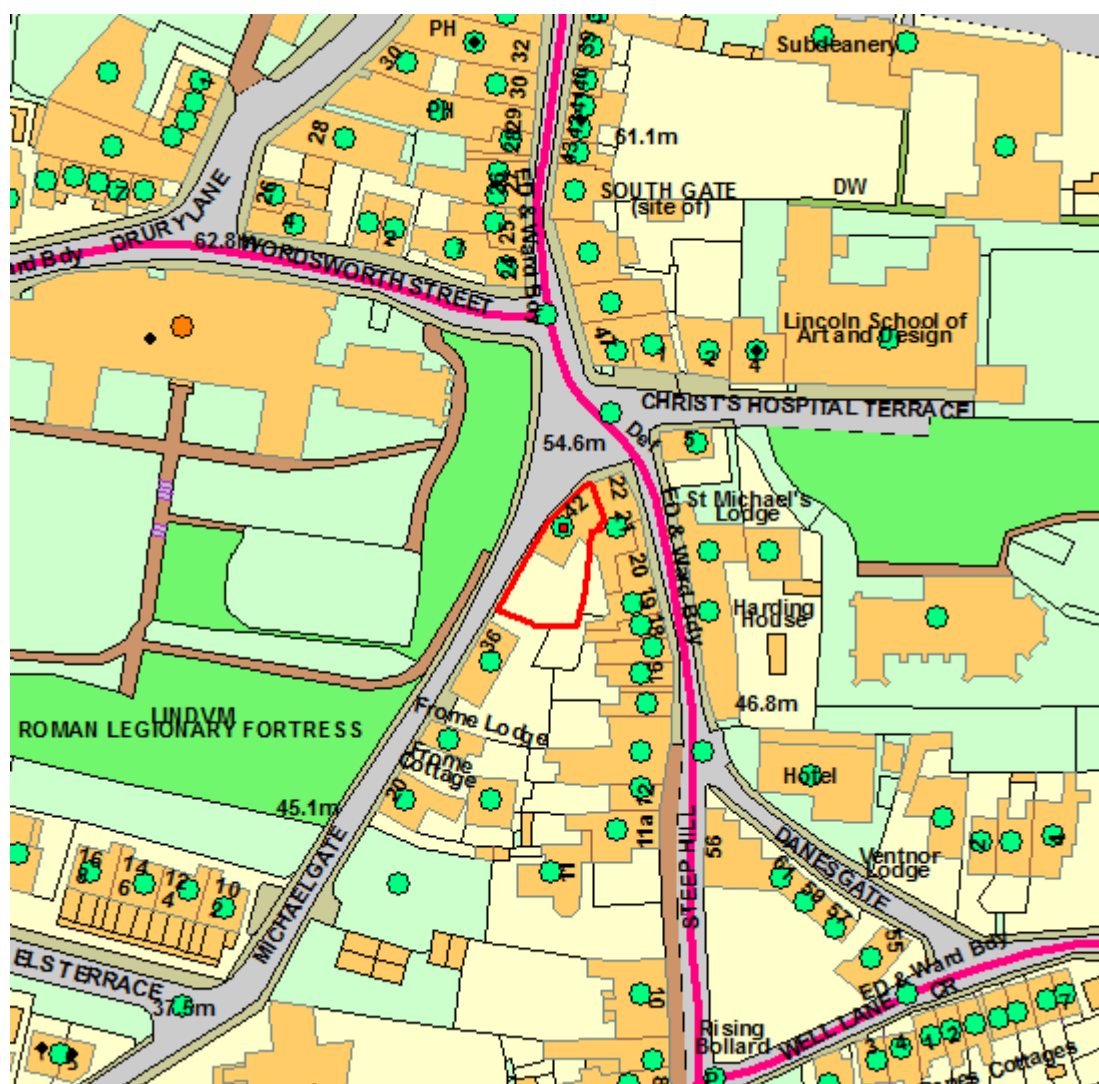
Yes.

Recommendation

That the listed building consent be Granted Conditionally subject to the following conditions:

- Time limit of the permission
- Development in accordance with approved plans
- Brick sample panel including lime mortar and bonding to match existing
- Sample of replacement bricks
- Sample of lintels
- Specification and sample of lime plaster
- Samples and surface treatment of timber for replacement of timber frame
- Methodology for refurbishment of windows
- Methodology for repair to timber framing

40-42 Michaelgate: Plans, photos and response



Site location plan



Existing front, north elevation

Photos showing external and internal damage







Environment & Economy

Lancaster House
36 Orchard Street
Lincoln LN1 1XX
Tel: (01522) 782070
E-Mail: Highwayssupport@lincolnshire.gov.uk



To: Lincoln City Council

Application Ref: 2018/0591/LBC

With reference to this application dated 3 May 2018 relating to the following proposed development:

Address or location

40 - 42 Michaelgate, Lincoln, Lincolnshire, LN1 3BT

Date application referred by the LPA
16 May 2018

Type of application: Outline/Full/RM/
LBC

Description of development

Taking down and reinstating a section of the north wall. Reinstating meters and services attending to damaged plasterwork and other finishes and redecoration to certain rooms

Notice is hereby given that the County Council as Local Highway and Lead Local Flood Authority:

Does not wish to restrict the grant of permission.

CONDITIONS (INCLUDING REASONS) /REASONS FOR REFUSAL

HI08

Please contact Lincolnshire County Council Streetworks & Permitting team on 01522 782070 to discuss any proposed statutory utility connections and any other works which will be required in the public highway in association with this application. This will enable Lincolnshire County Council to assist you in the coordination and timings of such works.

NO OBS

Having given due regard to the appropriate local and national planning policy guidance (in particular the National Planning Policy Framework), Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) has concluded that the proposed development is acceptable. Accordingly, Lincolnshire County Council (as Highway Authority and Lead Local Flood Authority) does not wish to object to this planning application.

Case Officer:

Date: 30.05.18

Emily Stevenson

for Warren Peppard

Flood Risk & Development Manager

Application Number:	2018/0557/HOU
Site Address:	69 Greetwell Road, Lincoln, Lincolnshire
Target Date:	22nd June 2018
Agent Name:	CDM Design Solutions
Applicant Name:	Mr Ian Wicks
Proposal:	Erection of a single storey rear/side extension.

Background - Site Location and Description

The application proposes the erection of a single storey rear and side extension at 69 Greetwell Road. The property is a two storey semi-detached dwelling.

The application is brought before Planning Committee as the applicant is an employee of the City of Lincoln Council.

Site History

No relevant site history.

Case Officer Site Visit

Undertaken on 15th May 2018.

Policies Referred to

- Policy LP26 Design and Amenity
- National Planning Policy Framework

Issues

To assess the proposal with regard to:

- National and local planning policy
- Effect on residential amenity
- Effect on visual amenity
- Effect on highway safety

Consultations

Consultations were carried out in accordance with the Statement of Community Involvement, adopted May 2014.

Statutory Consultation Responses

Consultee	Comment
Highways & Planning	Comments Received

Public Consultation Responses

No responses received.

Consideration

Local and National Planning Policy

The National Planning Policy Framework states that applicants should take the opportunities available for improving the character and quality of an area and the way it functions. Buildings and extensions should promote high levels of sustainability through good design and weight will be given to outstanding or innovative designs which help raise the standard of design more generally in the area.

Policy LP26 'Design and Amenity' of the Central Lincolnshire Local Plan (2017) is permissive of extensions to existing buildings provided the siting, height, scale, massing and form relate well to the site and surroundings, and duly reflect or improve on the original architectural style of the local surroundings; and use appropriate high quality materials, which reinforce or enhance local distinctiveness, with consideration given to texture, colour, pattern and durability. In relation to both the construction and life of the development, the amenities which all existing and future occupants of neighbouring land and buildings may reasonably expect to enjoy must not be unduly harmed by or as a result of development.

Impact on Residential Amenity

In terms of the impact of the extension on residential amenity, the extension would be positioned approximately 0.7 metres from side boundary with No. 67 Greetwell Road which is defined by an approximately 1.8 metre high timber fence. A door is proposed in the elevation facing No. 67, given the existing boundary treatment it is considered there would be no issues of loss of privacy. The extension would have a roof sloping away from the boundary and given the extensions single storey nature, it is considered that the extension would not be overbearing or cause an unacceptable degree of loss of light when viewed from this neighbouring property. As such, the proposals would not unacceptably harm the living conditions of the residents of that property and therefore would be in accordance with the amenity requirements of Policy LP26 of the Central Lincolnshire Local Plan.

With regard to impact on No. 71 Greetwell Road the extension would project 1.3 metres from the original rear elevation. No. 71 has a conservatory projecting approximately 2.5 metres adjacent to the boundary. The extension would have a roof sloping away from No. 71 given its single storey nature and No. 71's existing conservatory, it is considered that the extension would not be overbearing or cause an unacceptable degree of loss of light when viewed from this neighbouring property. There are no windows in the side elevation facing No. 71 and therefore privacy would be maintained between the two properties. As such, the proposals would not unacceptably harm the living conditions of the residents of that property and therefore would be in accordance with the amenity requirements of Policy LP26 of the Central Lincolnshire Local Plan.

Beyond the other boundary is the hospital. The hospital is set some distance away from the application site. The extension will therefore have no impact on the hospital buildings. The proposed extension will be located over 18 metres from the boundary that adjoins the hospital grounds.

There are no other properties in the vicinity which would be affected by the proposal and

officers are therefore satisfied that the development would not cause undue harm to the amenities which occupiers of neighbouring properties may reasonably expect to enjoy, in accordance with CLLP Policy LP26.

No objections have been received either from the Hospital or the neighbouring properties.

Impact on Visual Amenity

The proposed extension is well designed, constructed with a hipped roof to reflect the roof design of the original dwellinghouse. The existing car port to the side of the property will screen views of the extension, however officers consider that the design is good, and that the extension will not detract from the visual amenity of the area it is therefore considered that the visual amenity of the wider area would not be harmed in accordance with Policy LP26 of the Central Lincolnshire Local Plan.

Effect on Highway Safety

Lincolnshire County Council as Highway Authority has assessed the application and has raised no objections to the proposal. Therefore based on this advice it is considered that the proposal would not be detrimental to highway safety or traffic capacity.

The Council's Environmental Officer has outlined no objections to the proposals in terms of contaminated land, air quality, noise or other environmental impact.

Lincoln Townscape Assessment

Much of the suburb is composed of single individual property build units constructed during the Late Victorian/Edwardian and Inter-war periods. The style and form of houses within the area varies considerably, resulting in a constantly changing character along many streets. However, there are a small number of larger development units, which are more coherent in character, along some cul-de-sacs. Properties are built towards the front of spacious rectangular plots orientated perpendicular to the road. The depth and width of plots often varies along streets, leading to an irregular pattern of houses within their plots. Boundaries to the front of plots are highly varied in style and material, and are frequently tall in height, leading to an increased sense of enclosure and inactivity. Houses are detached or semi-detached and almost entirely two storeys in height and between two to five bays in width. The majority are constructed of a red coloured brick laid in stretcher bond, although a handful of earlier buildings are constructed of stone. Houses have active frontages with doors and windows facing the street creating a feeling of vitality and security along streets.

Application Negotiated either at Pre-Application or during Process of Application

No.

Conclusion

It is considered that the proposed development would be in accordance with local plan policy LP26. The height and scale of the extension is in keeping with the existing and neighbouring properties and would be constructed of materials to match. The amenities which all existing and future occupants of neighbouring properties may reasonably expect to enjoy would not be unduly harmed by the proposed development and as such is

considered acceptable subject to the conditions set out below.

Application Determined within Target Date

Yes.

Recommendation

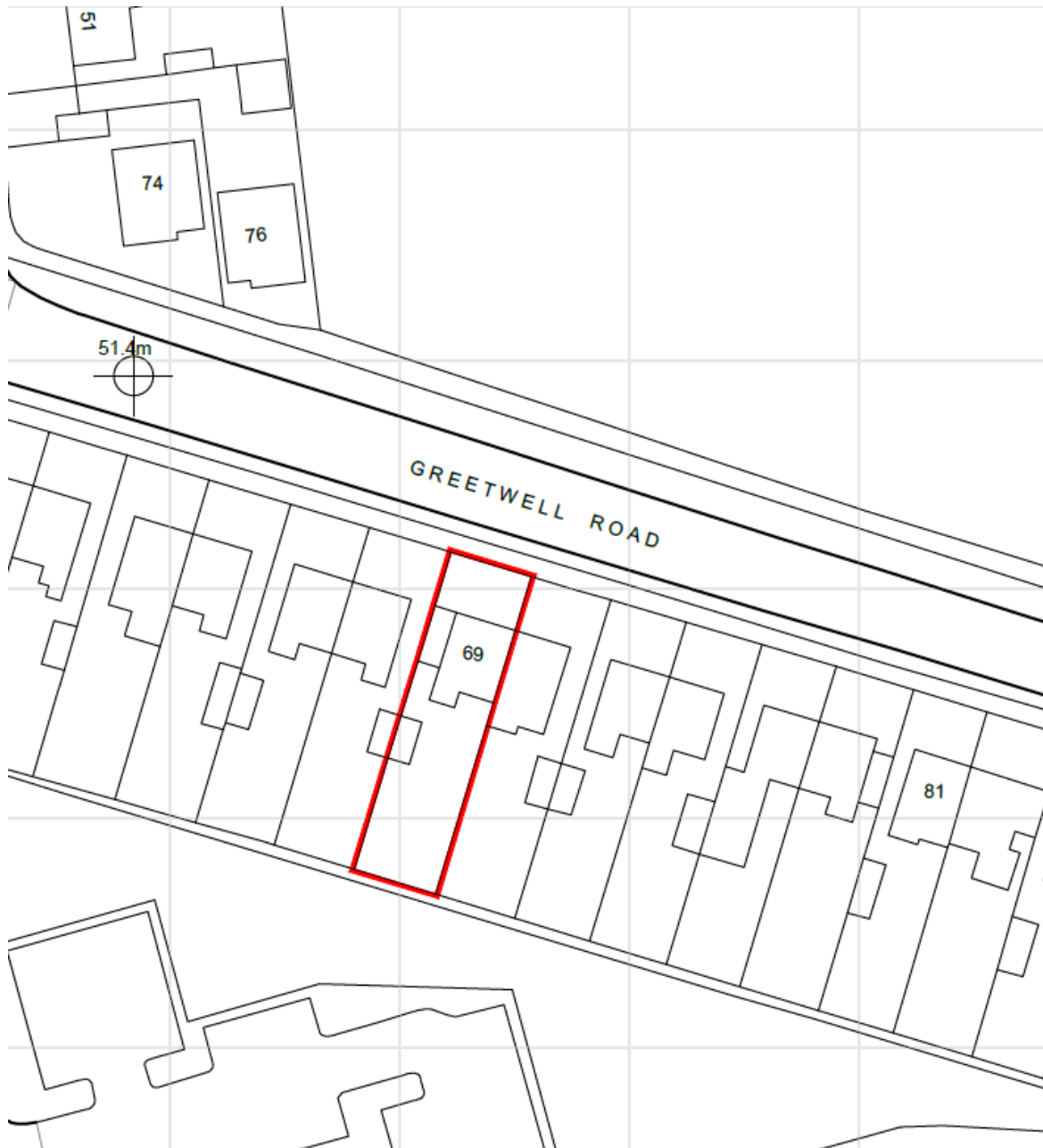
That the application is Granted Conditionally

Conditions

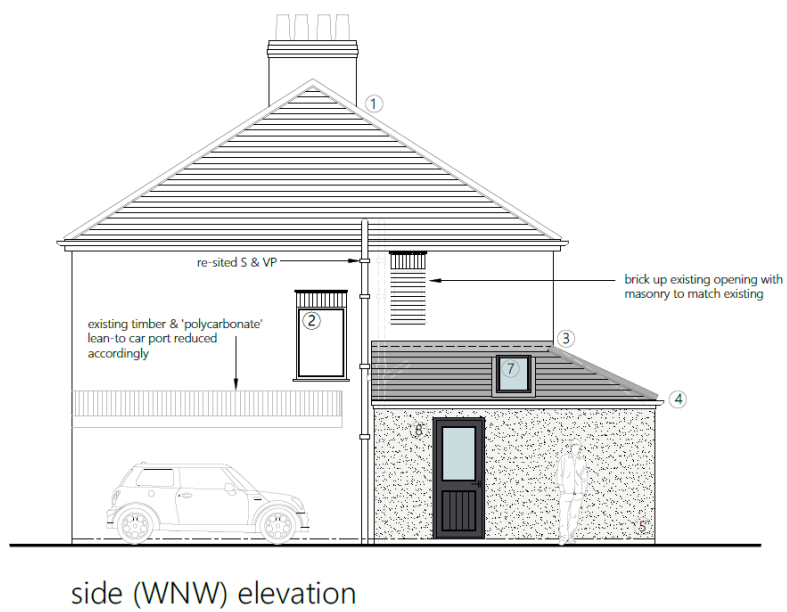
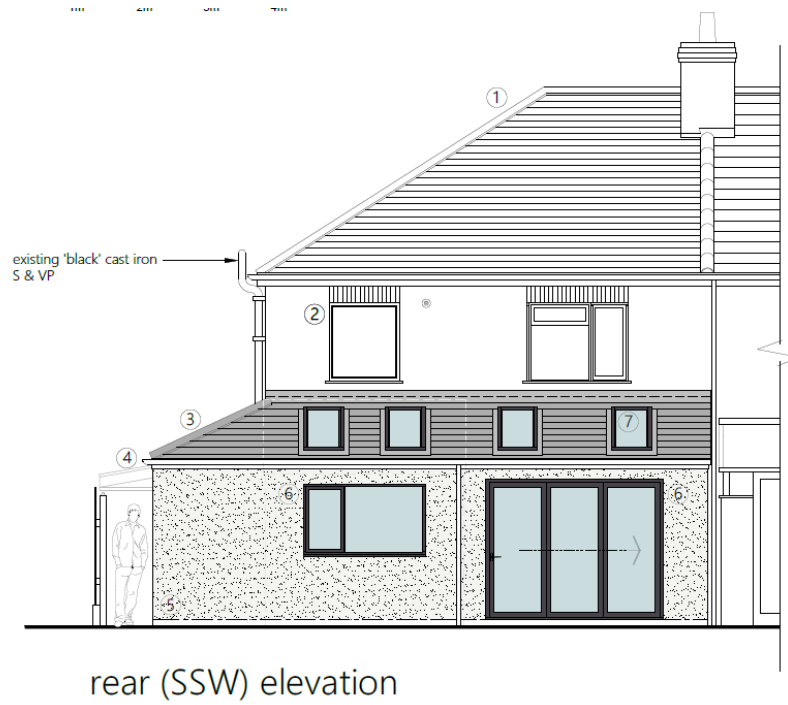
- Development to commence within 3 years
- Development to be carried out in accordance with the plans

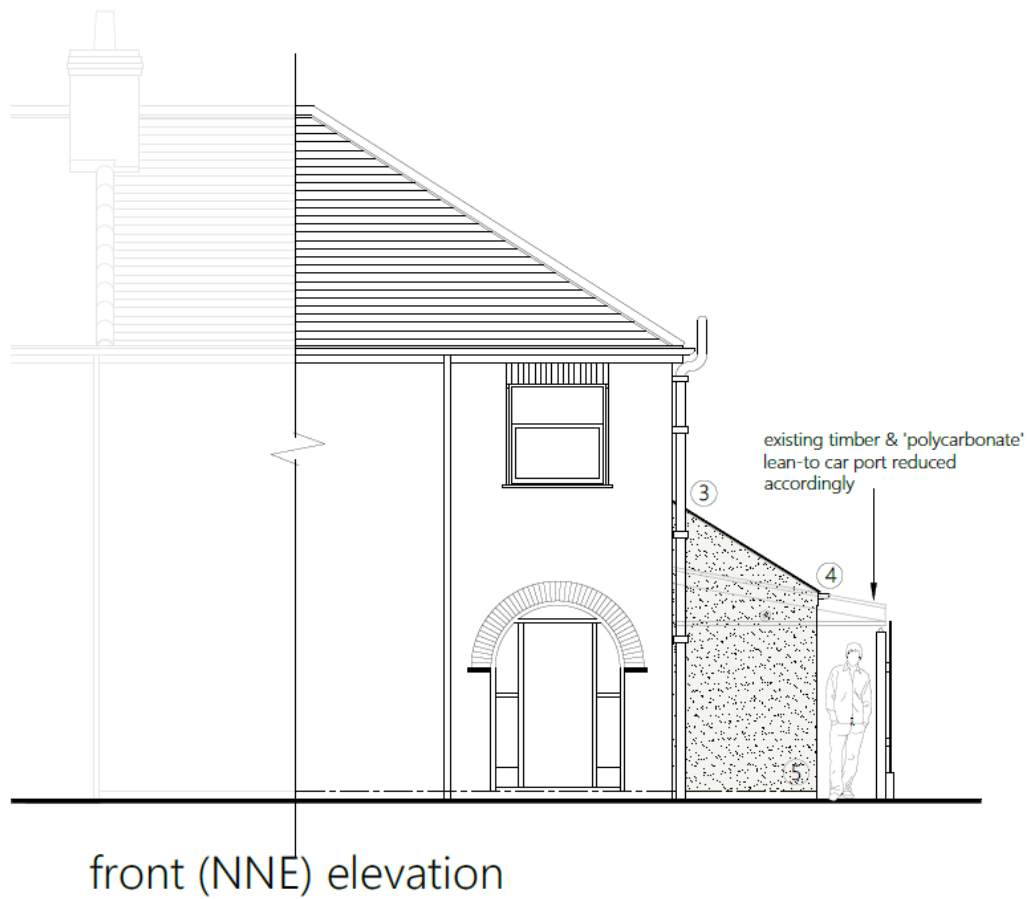
Plans

Site Location Plan

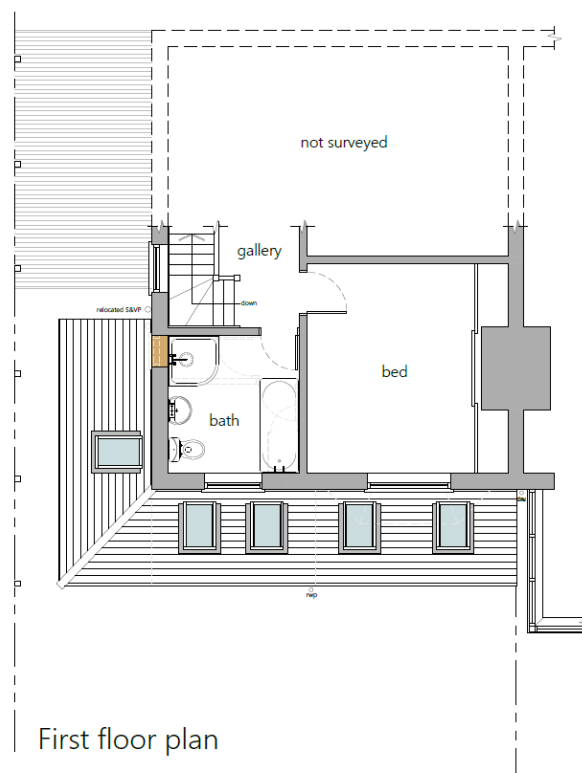
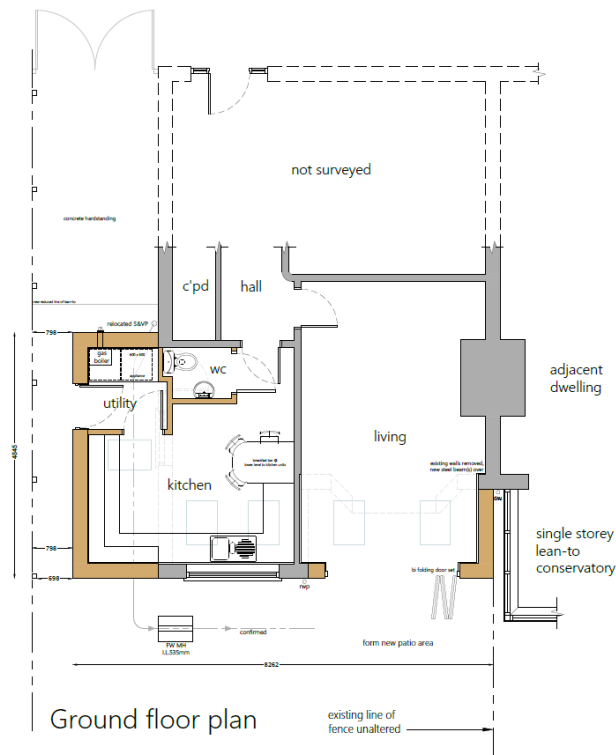


Proposed Elevations





Proposed Floor Plans



Site Photographs







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